

PUBLIC RECORD OFFICE

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TIGHTLY BOUND FOR ALL WORDS TO BE
REPRODUCED IN ENTIRETY**

Answers to

29¹

? 1903

Draft

100

5 Jan, 1902

Mr A. B. Hoyle

The Maas

sir,

Rhayader

Radnorshire

In reply to your

letter of 30th ultimo,

L. H.

I am directed by the
Marquess of Lansdowne
to say that no report
has yet been received
from Mr. Minister at
Peking on the subject
of your dismissal by
the British Railway

administration

a copy of your
letter will be sent

to Sir S Satow for his
information

India Office,

Whitehall,

3

London, S.W.

21st January 1903.

Confidential.

Sir,

I am directed to forward herewith, for the information of the Secretary of State for Foreign Affairs, a Military Report on Imperial Railways in China under the British Railway Administration.

I have the honour to be, Sir,

Your obedient Servant,

Hedware

Major General,

Military Secretary.

The Under Secretary of State,
Foreign Office.

Drake
Mr A. D. Drake
26 Jan 1903
[Rg'd in 30 sec'd]

Ms. A. D. Drake's Memorial
No Report has yet
been received. So the
will be forwarded to Peking

q.c.l. down

SECTION I - T'ANG-KU TO PEKING.

MAPS.

(I) General map of railway

PLANS.

(II) Map of T'ANG-KU to PEKING Section etc

- (1) T'ANG-KU and HSIN-HO.
- (2) CHUNG-KUA-CHUNG.
- (3) TIEN-TIN Station.
- (4) Country round TIENSIN Station.
- (5) YANG-TE'SUN.
- (6) PEI-TS'ANG.
- (7) LO-FA.
- (8) LANG-FANG and German Post. (2 Plans)
- (9) HUANG-TE'SUN.
- (10) AN-T'ING.
- (11) CHUN-LIANG-CH'ENG.
- (12) Country round FENG-T'AI Station.
- (13) FENG-T'AI Station and Post.
- (14) YUNG-TING-MEN etc.
- (15) CH'IEN-MEN and Post etc.
- (16) CH'IEN-WEN Station.
- (17) GU-ROU-CH'IAO extension.
- (18) SHUANG-CH'IAO.
- (19) Specimens "HALF-DUCK" "HALF-THROUGH" and "THROUGH" bridges.
- (20) TONG-SHAN.
- (21) Comparative diagrams of rates etc.
- (22) Plan of Tunnel through Hata Men Bastion

SECTION II T'ANG-KU TO SHAN-HAI-ZUAN.

MAPS.

(III) T'ANG-KU TO SHAN-HAI-ZUAN section with the CHIN-RANG-TAO extension.
PLANS.

- (1) T'ANG-KU and HSIN-HO.
- (2) PEI-T'ANG.
- (3) HSI-KU.
- (4) T'ANG-FANG.
- (5) TONG-SHAN.
- (6) HSIAO-LI.
- (7) HSU-YEH Station.
- (8) LAM-CHOU.
- (9) AN-CHAN.
- (10) LIU-SHOU-YING.
- (11) T'ANG-HO etc. (2 plans)
- (12) SHAN-HAI-ZUAN Station.

For general map of railway and specimens of bridges etc see plan T'ANG-KU to Peking section.

Schedule of fine
D 21 January 1903
Mr. J. C. S. Smith
Secretary
Ministry of Finance
in China under British Railways
Government

General Schedule

The distribution of our
force in the Chinese for
the next 2 years

Under command of
W. Morris is decided

Retain.

L. Hill.

as it would still be
desirable, to keep a
force at Wei-Hai-wei

to occupy the Railway

at once if necessary -

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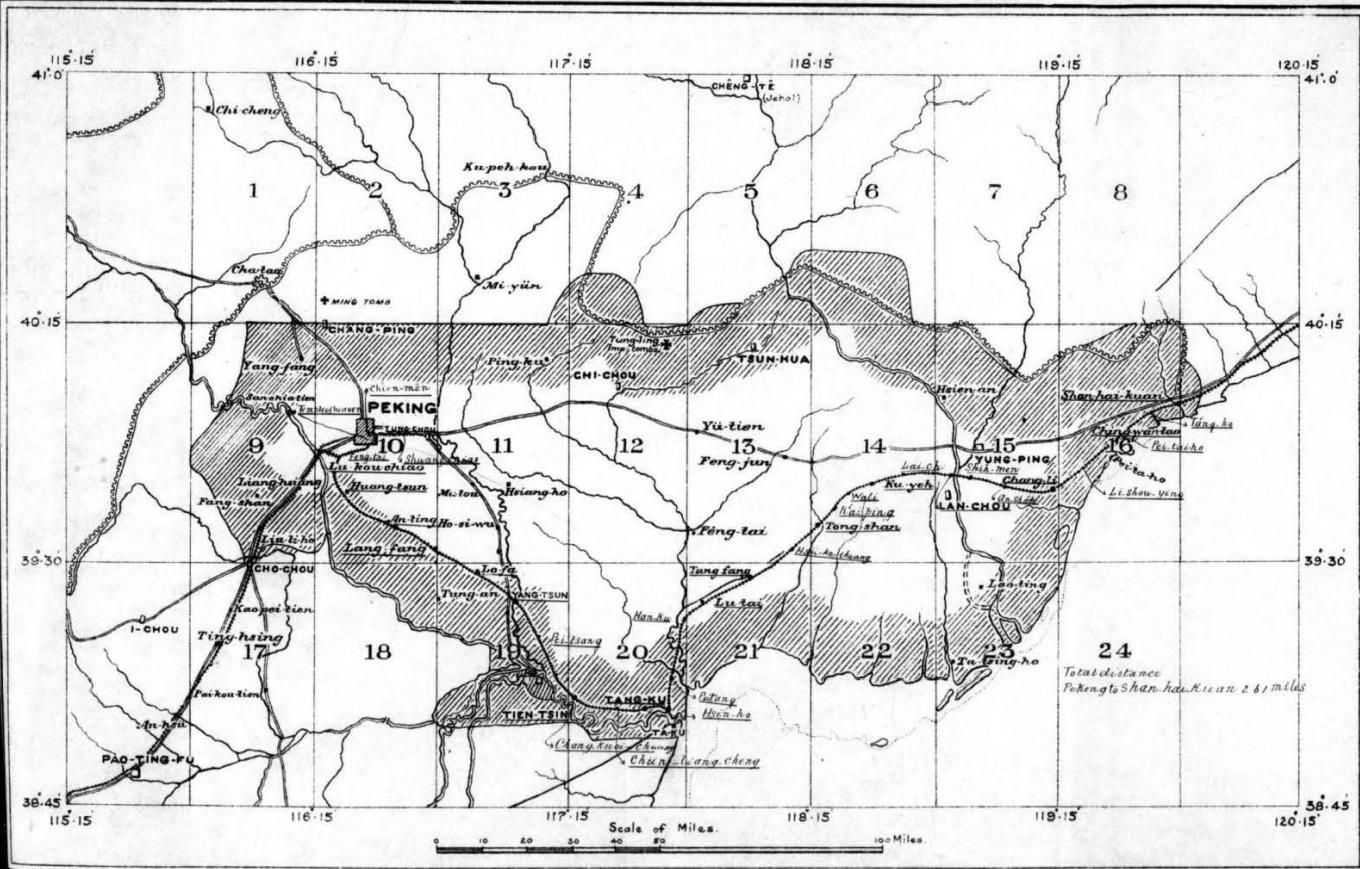
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CHINA FIELD FORCE.

INDEX TO PRELIMINARY HALF-INCH MAPS

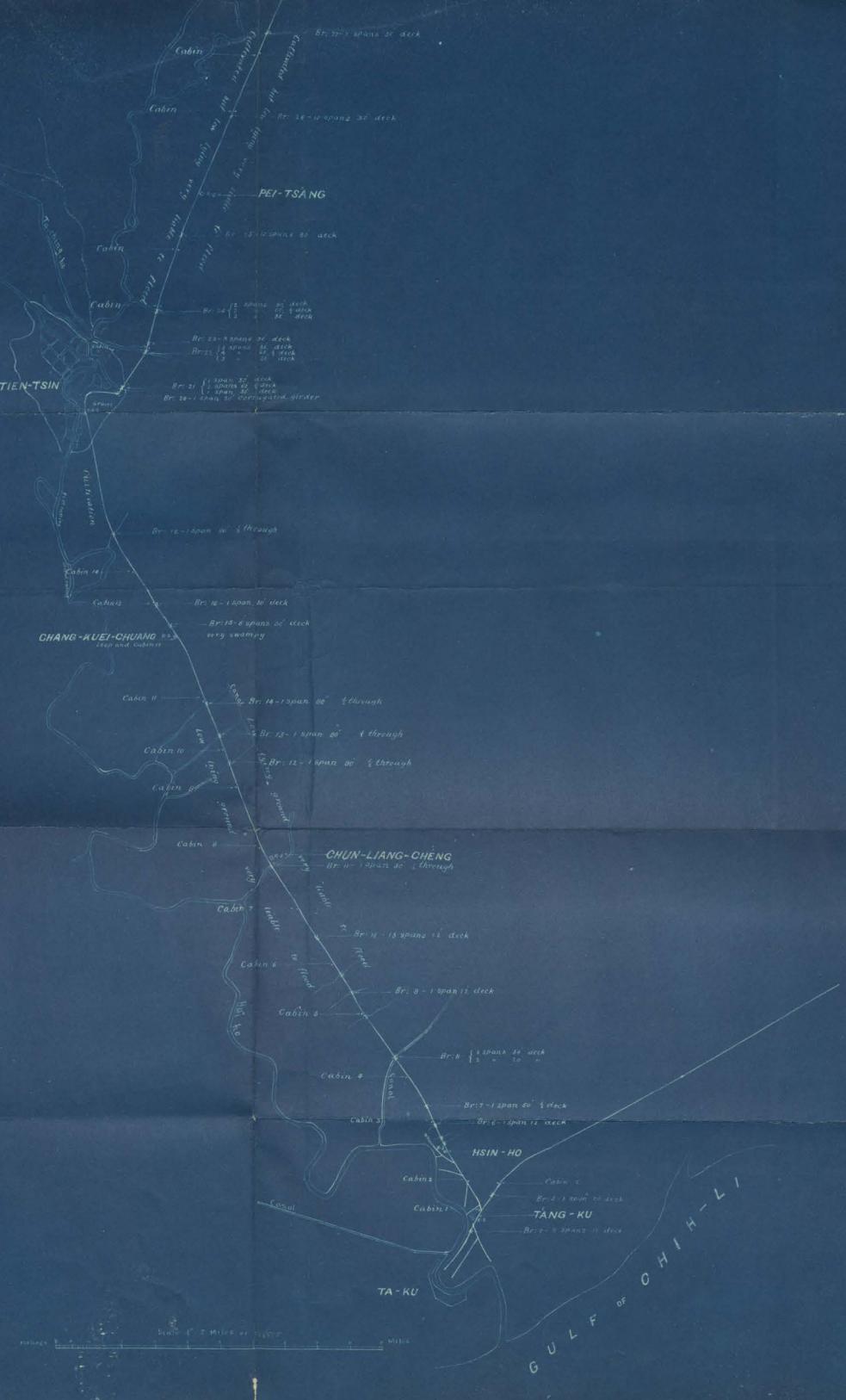


Zincographed at the Survey of India Offices, Calcutta, August 1904.

No. 499-S. 01

MAP OF TANG-KU TO SHAN-HAI-KUAN
SECTION WITH CHIN-WANG-TAO EXTENSION

MAP OF TANG-KU TO PEKING SECTION
WITH
LU-KOU-CHIAO AND TUNG CHOU EXTENSION

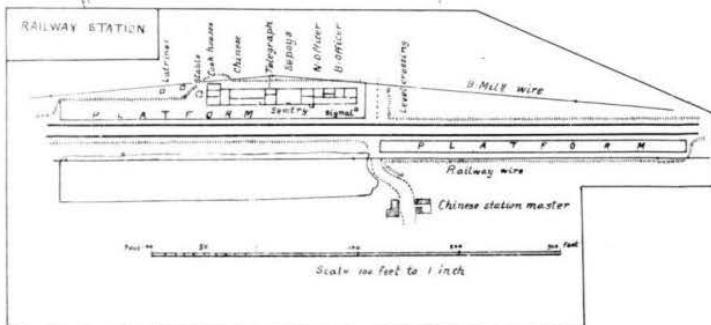
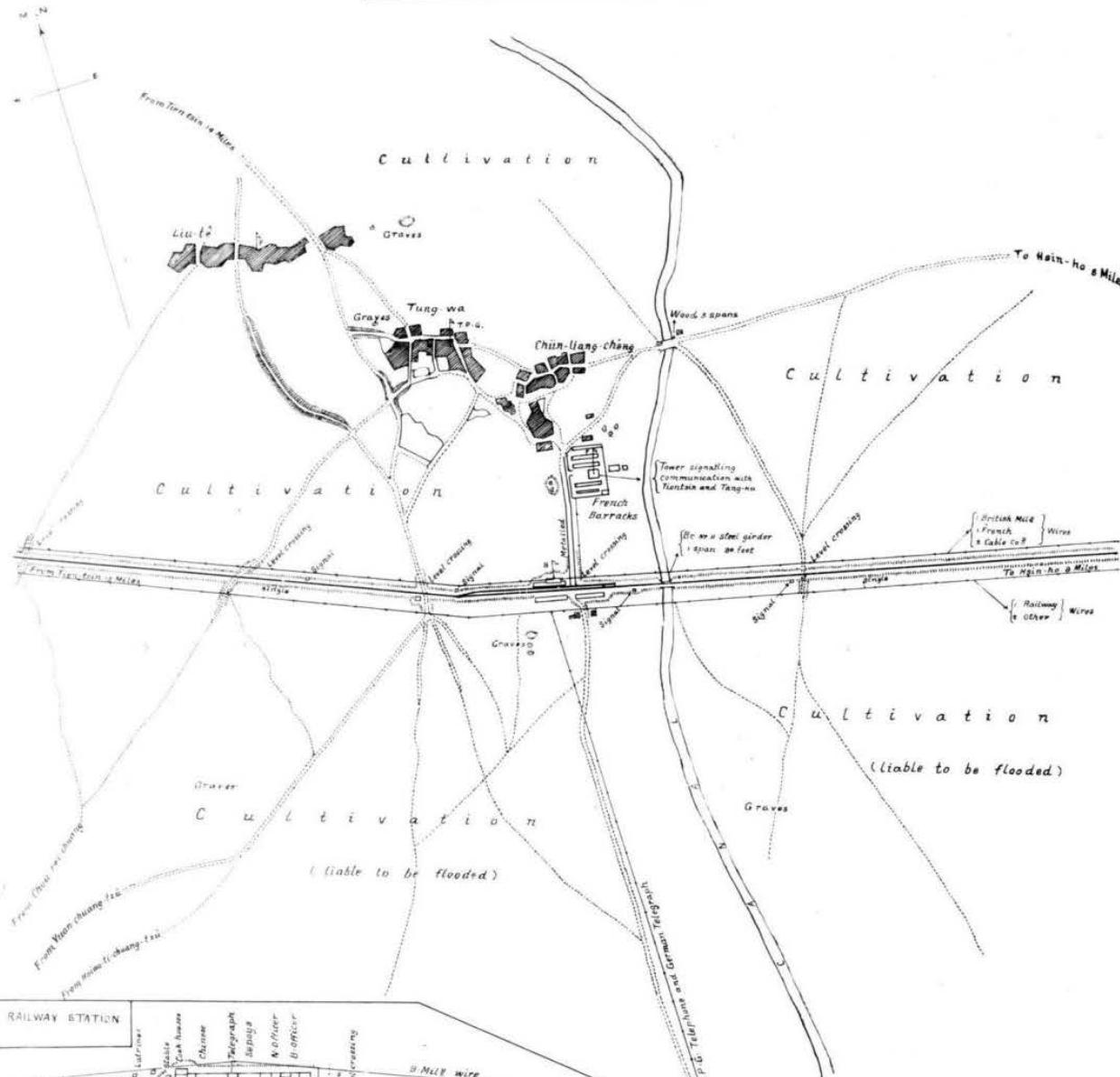


CHÜN-LIANG-CHENG

and one mile round

Scale 6° to 1 Mile R.F. 1:62500

Scale 100 feet to 1 inch
Scale 100 yards to 1 foot

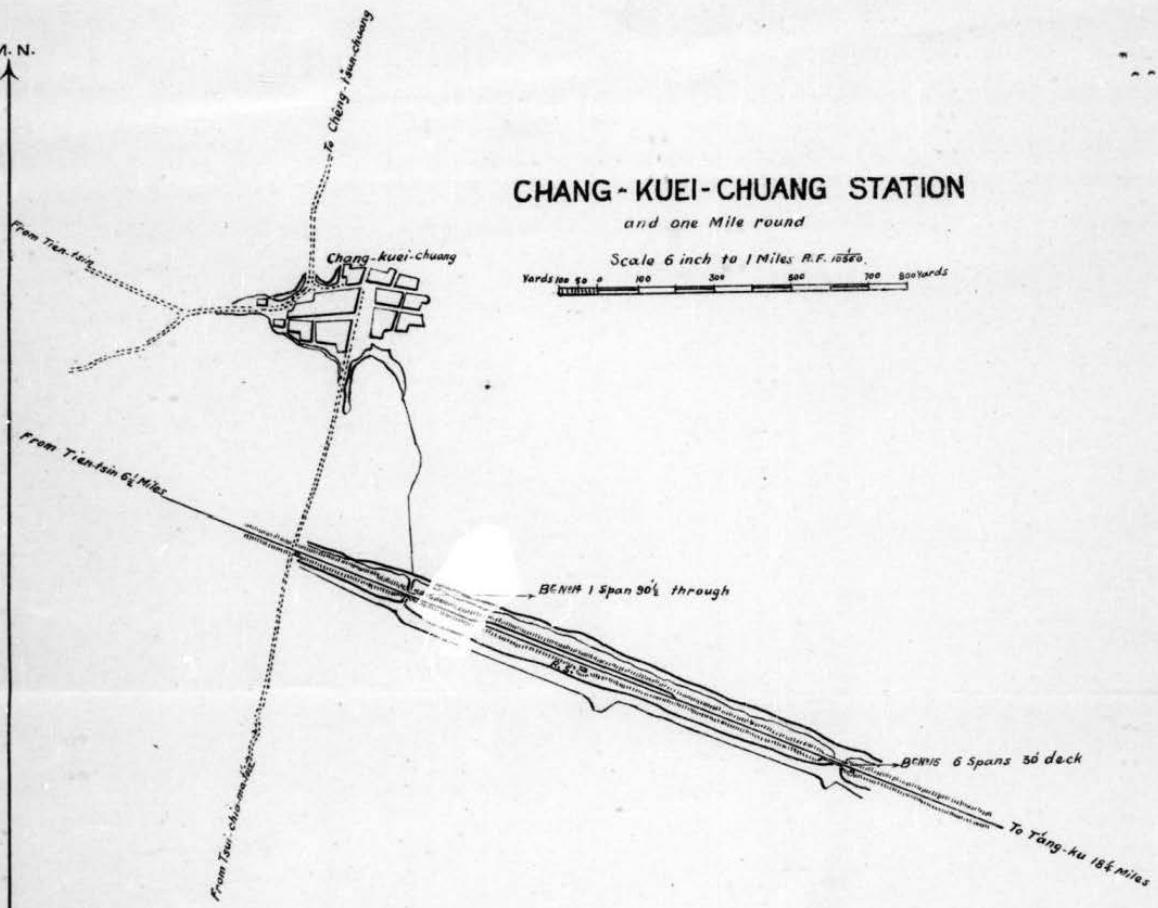


Front West-side 1/2 mile
Front West-side 1/2 mile

ES Early Capt.
Feb. 1902.
14th Sikkim

CHANG-KUEI-CHUANG STATION
and one Mile round

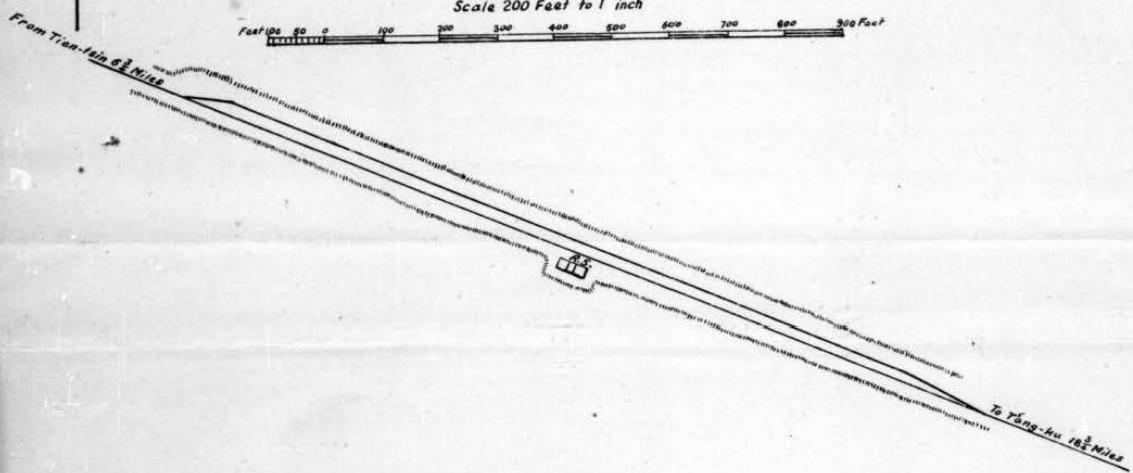
Scale 6 inch to 1 Miles R.F. 10860
Yards 50 100 150 200 250 300 350 400 450 500 550 600 650 700 750 800 Yards



PLAN of CHANG-KUEI-CHUANG STATION

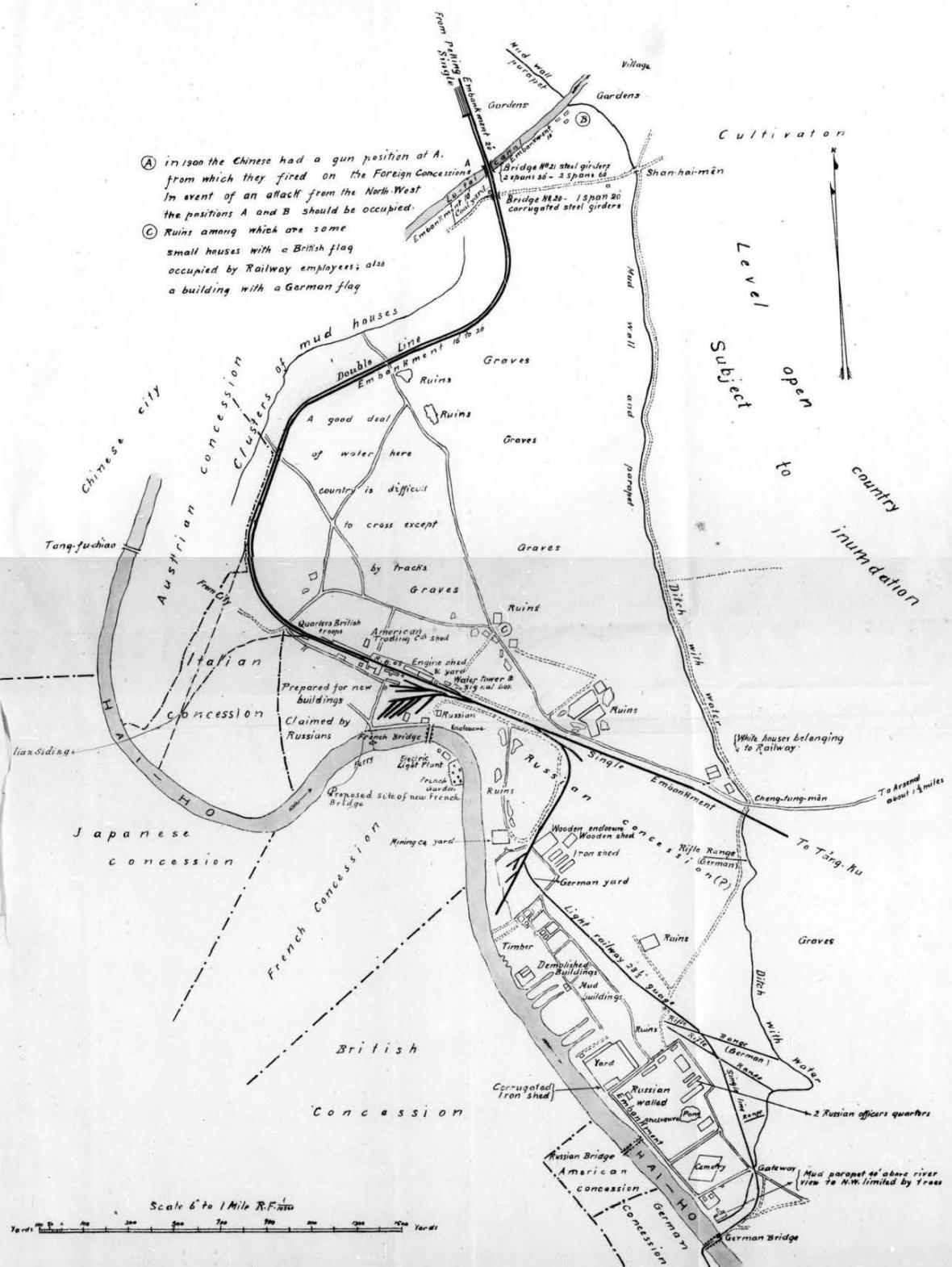
Scale 200 Feet to 1 inch

Feet 100 50 0 100 200 300 400 500 600 700 800 900 Feet



天津
TIEN-TSIN RAILWAY STATION

and neighbourhood



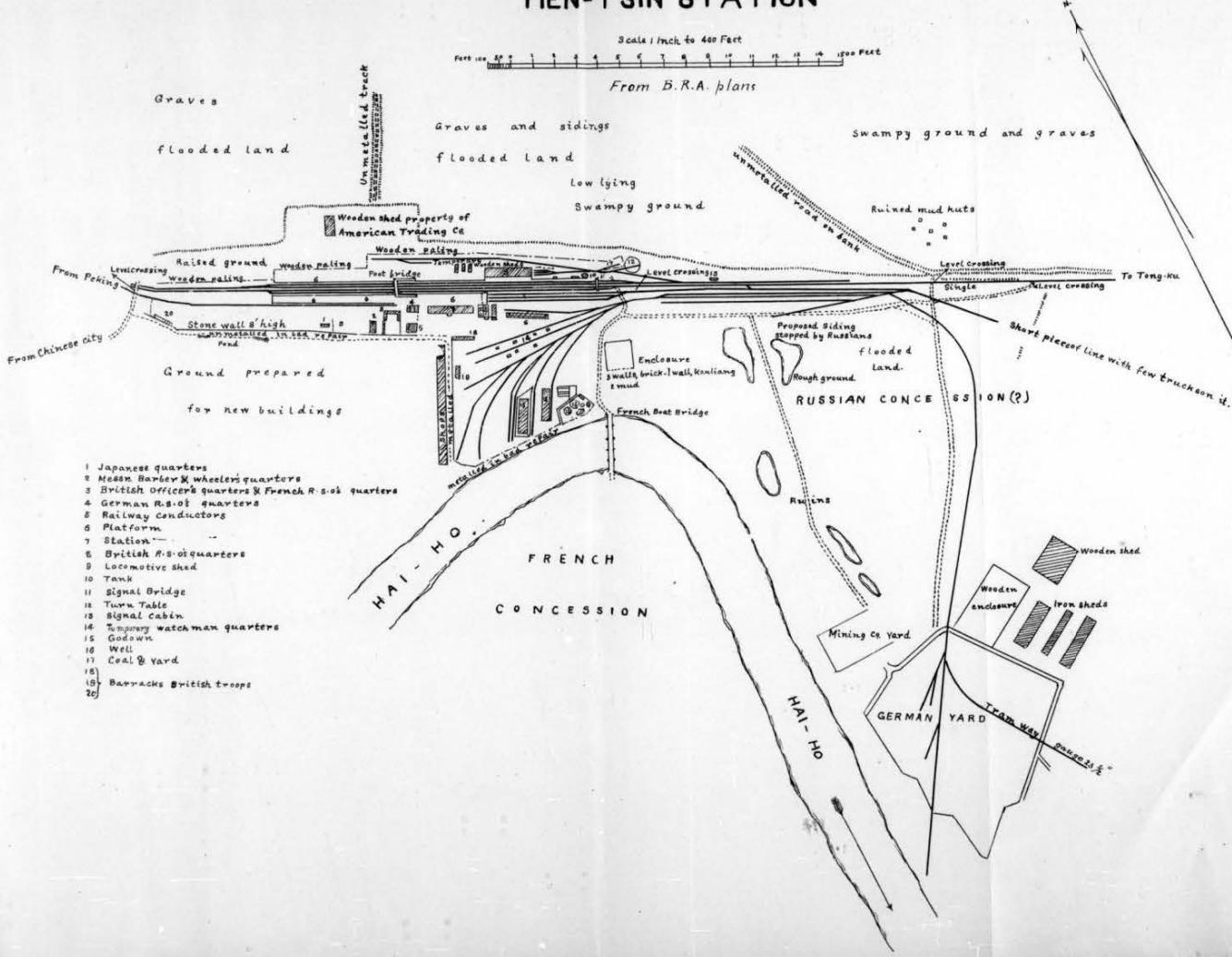
Sketch by Captain P.J. Miles

4th P.J.

January 1902

天津 TIEN-TSIN STATION

13



B 3C
PEI-TSANG STATION
and one mile round

14

Ground lately

under water

Temporary shelter for troops Station and Quarters

Shelter for German Patrol

Shed

Japanese Telegraph Line

French Telegraph Line

American Telegraph Line

Private Co. Line

Inundated Land

Scale 6 Inches to 1 Mile

R. F. 70360

Yds 100 50 1 2 3 4 5 6 7 8 9 10 110 Yards

Shed
square

Station and Quarters

Shelter for
German Patrol

Scale 100 Feet to 1 Inch

R. F. 1500

Feet 100 50 1 2 3 4 5 6 7 8 9 100 Feet

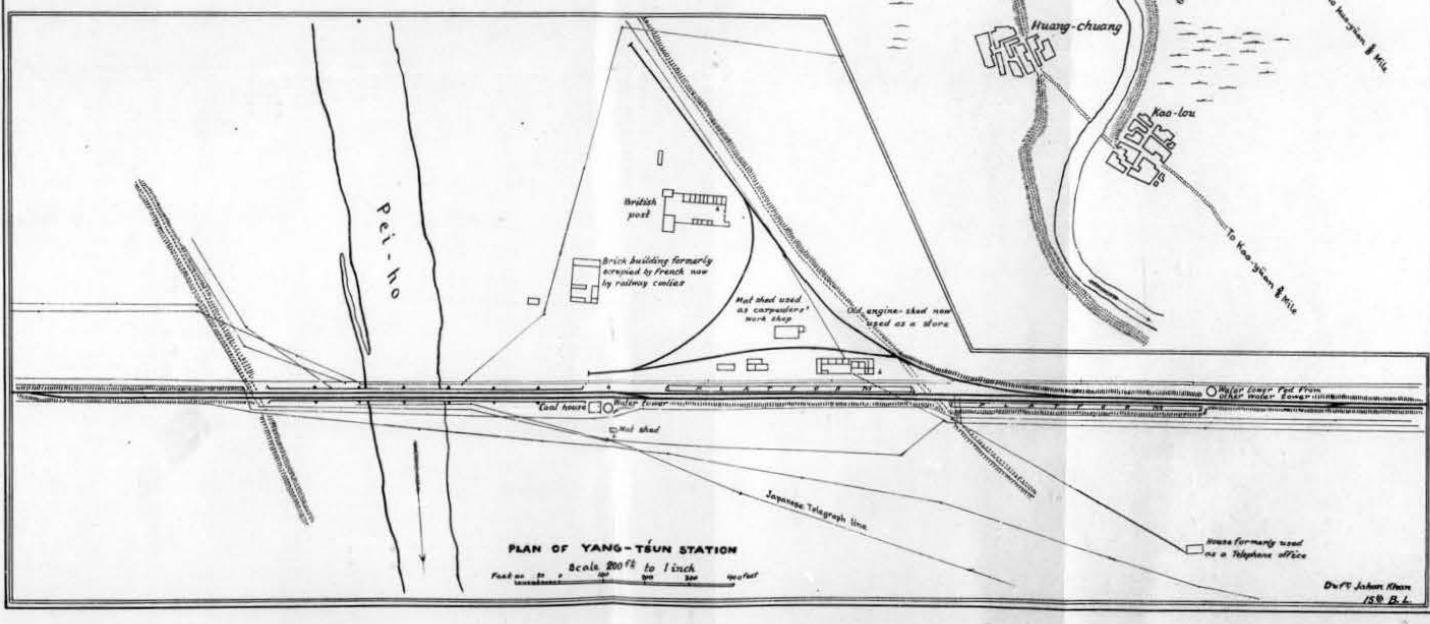
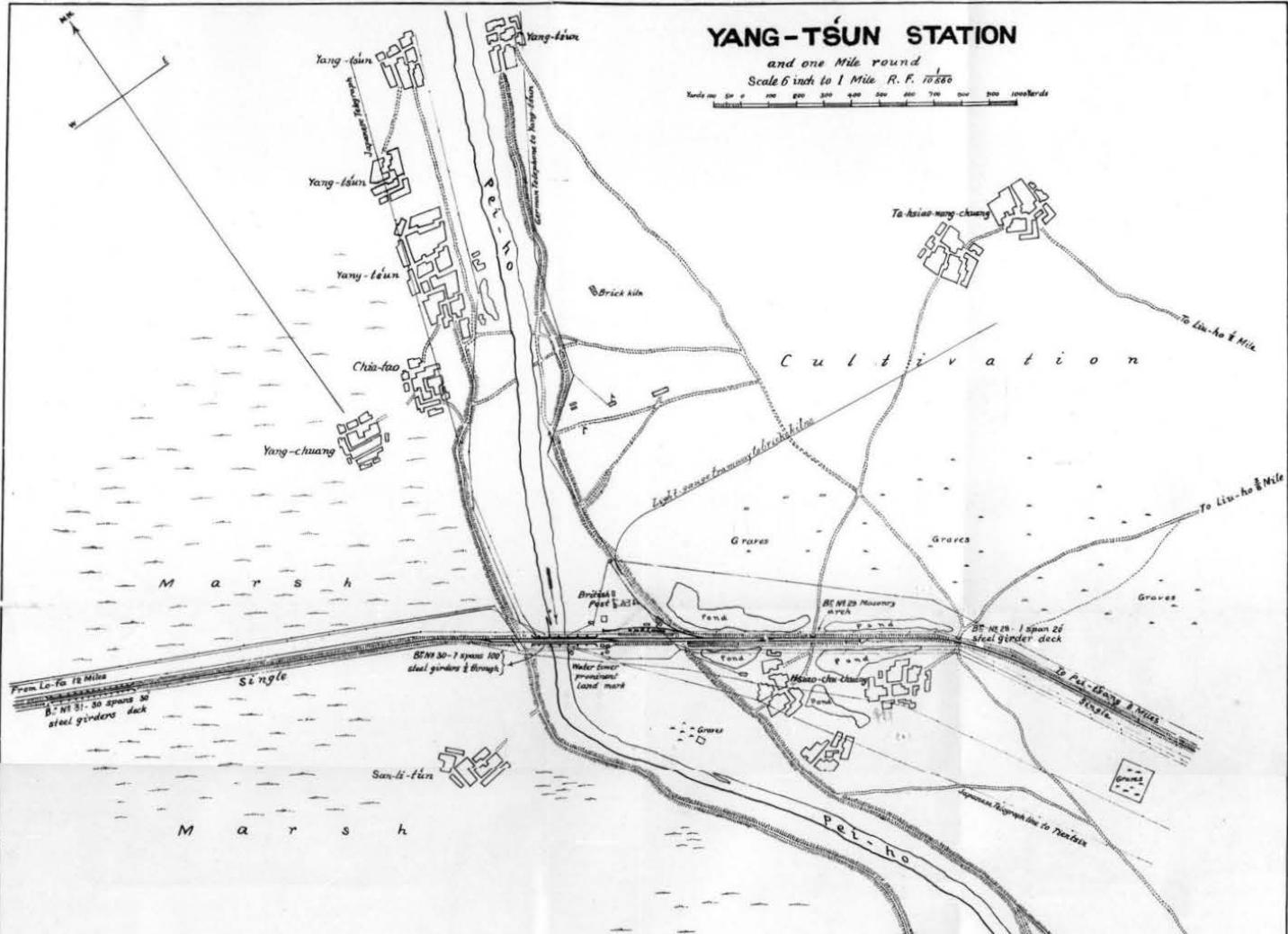
C. M. L. arta Capt.
14 Sixth

楊村

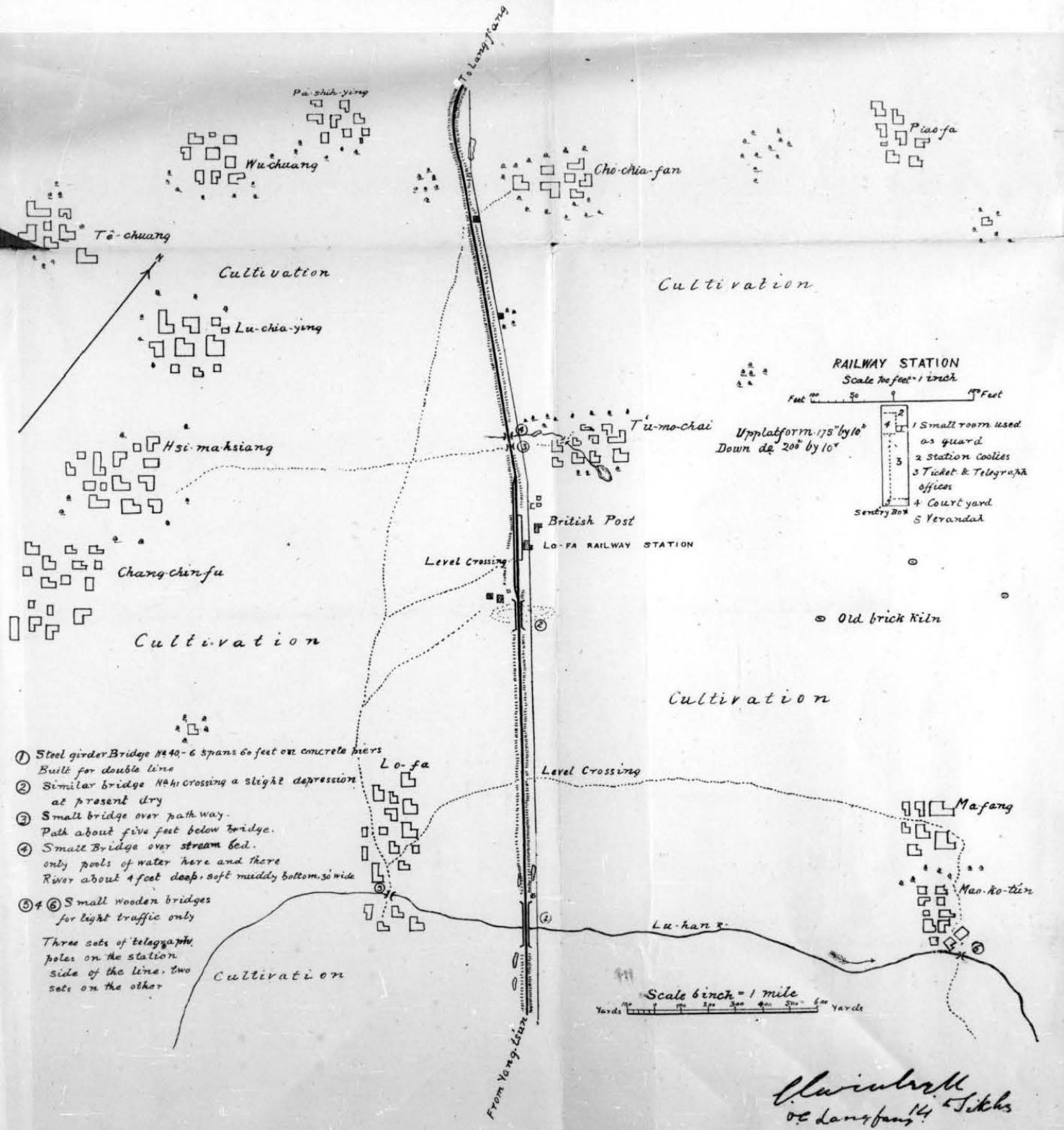
YANG-TSUN STATION

and one Mile round

Scale 6 inch to 1 Mile R.F. 10860

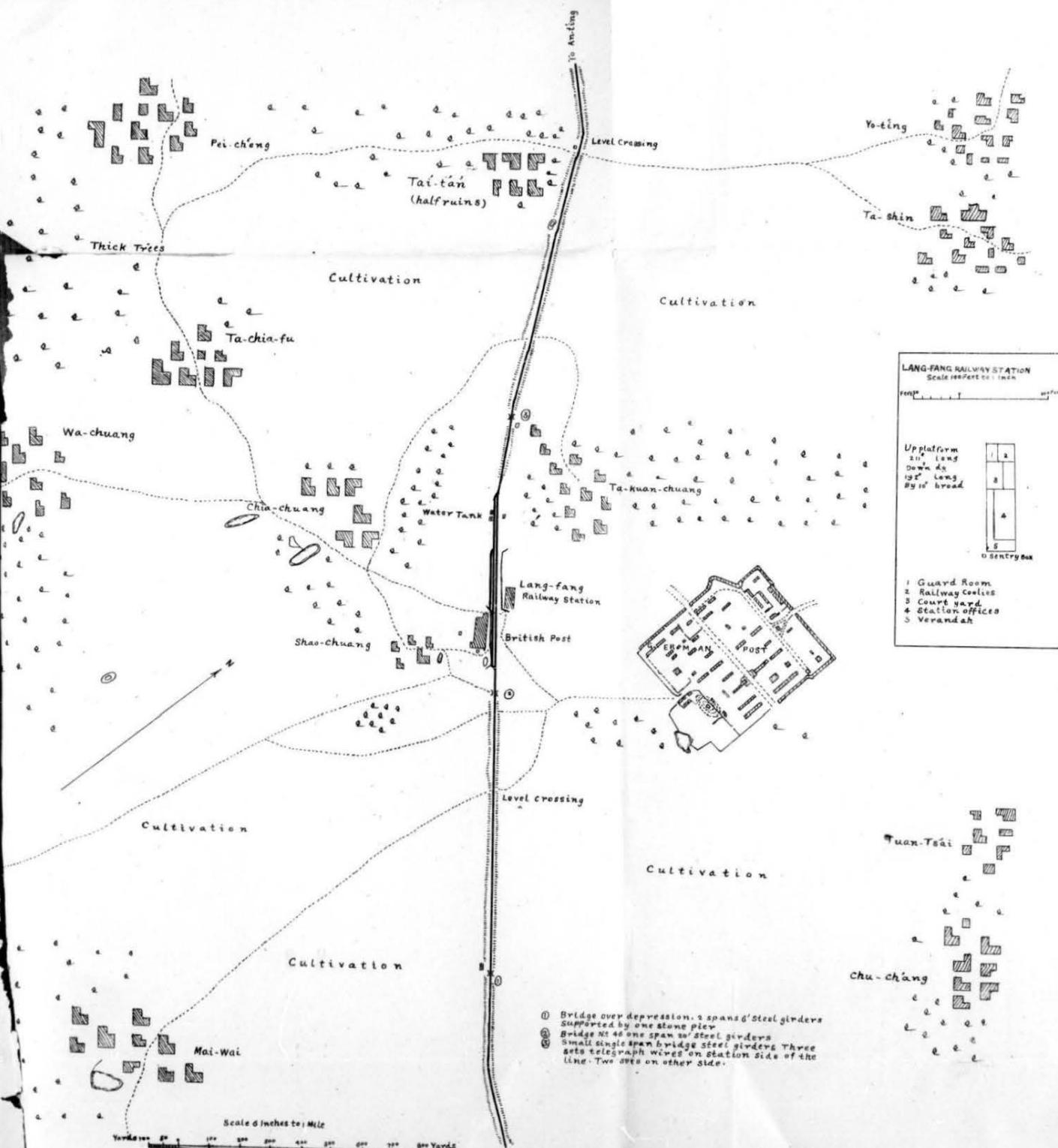
Yards per a
Mile
0 100 200 300 400 500 600 700 800 900 1000

and one mile round



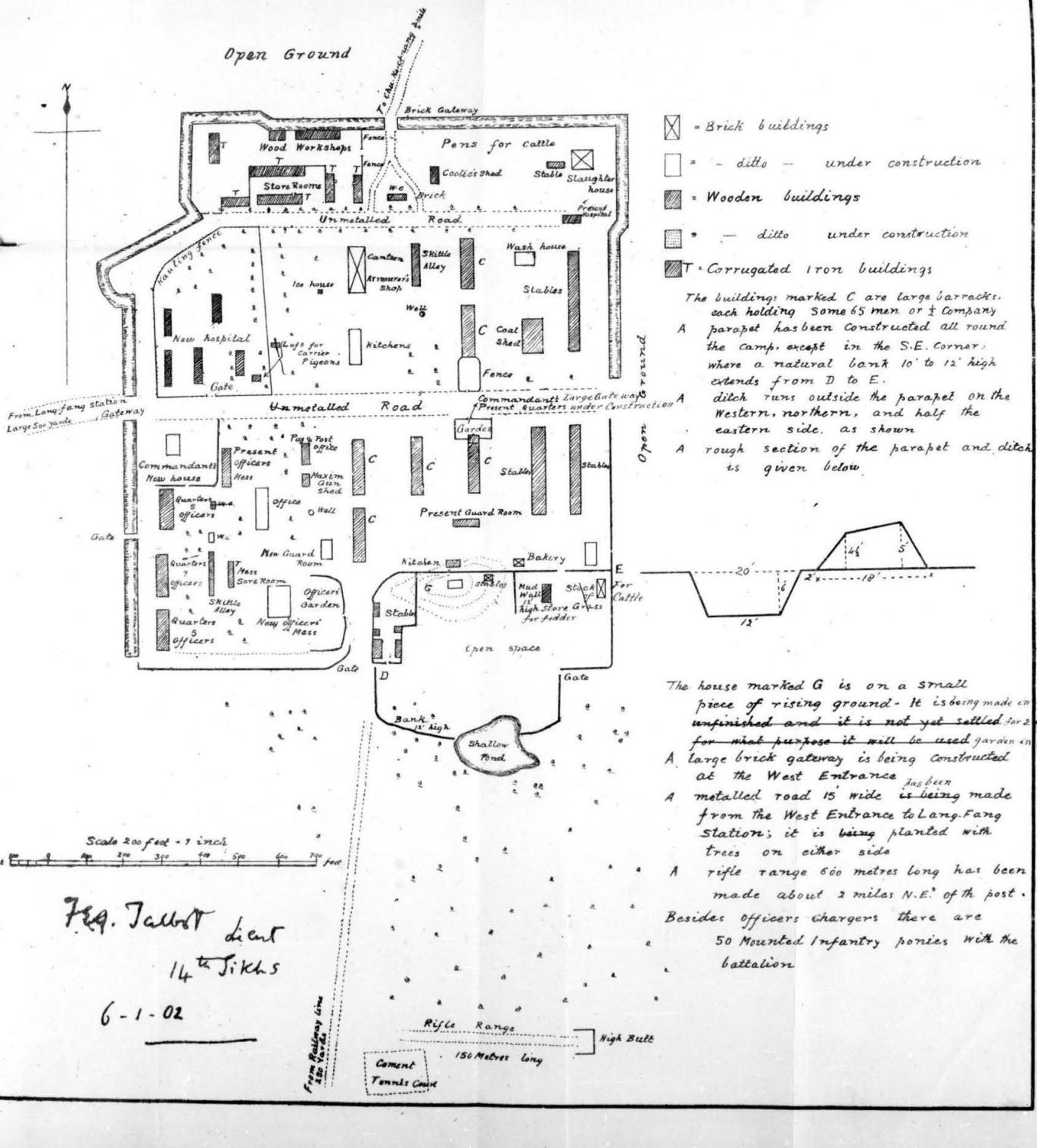
Chinaball
or Langfang 1/4 Sikkim

坊 郎
LANG-FANG STATION
and one mile round



Chinabill
14 miles of Lang-fang
From Lofa

THE GERMAN POST, LANG-FANG

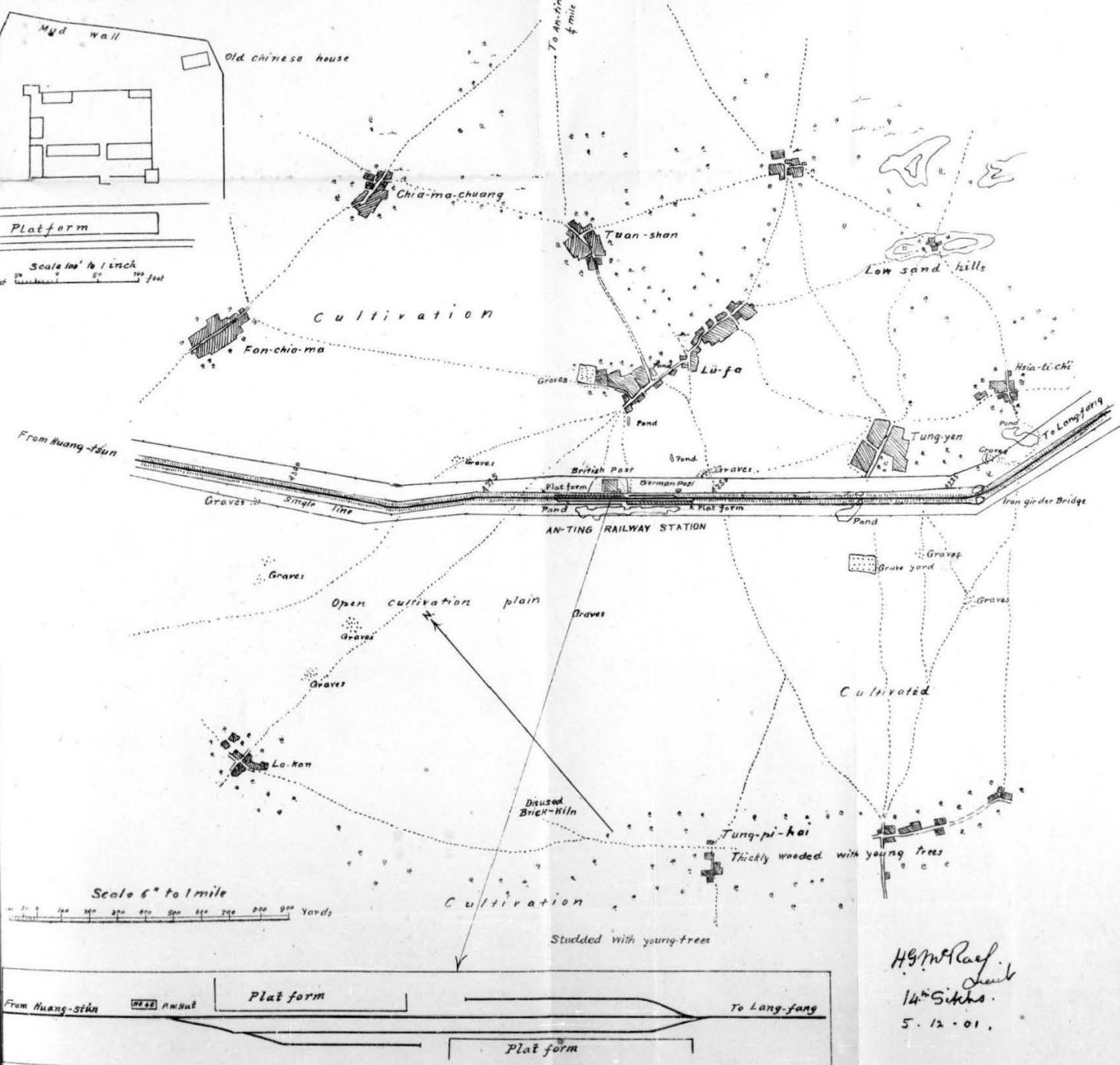


定安

AN-TING STATION

and one mile round

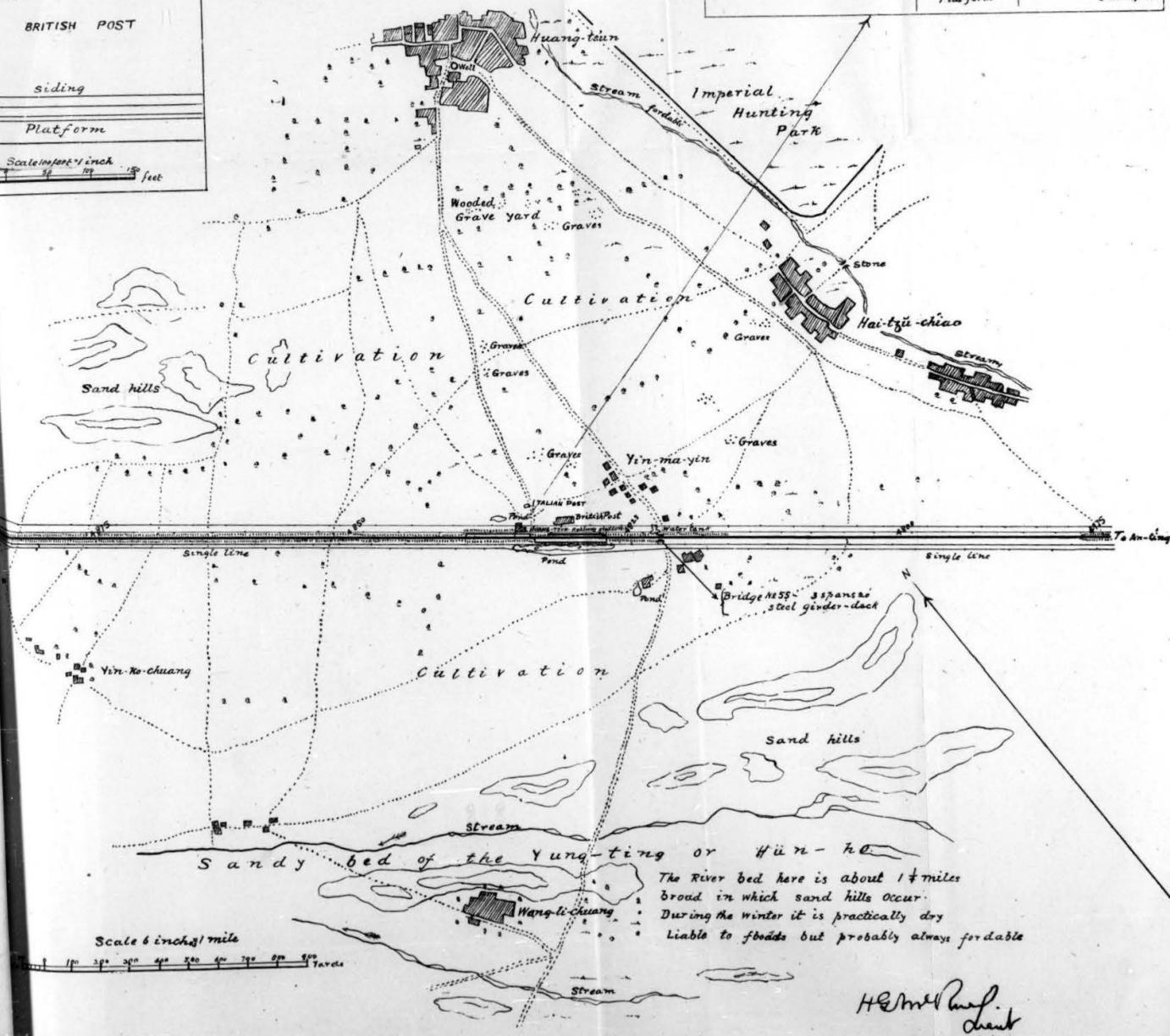
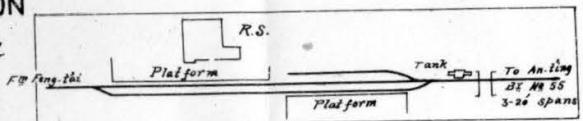
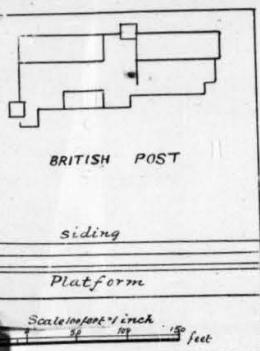
BRITISH POST



村黃

HUANG-TSUN STATION

and one mile round

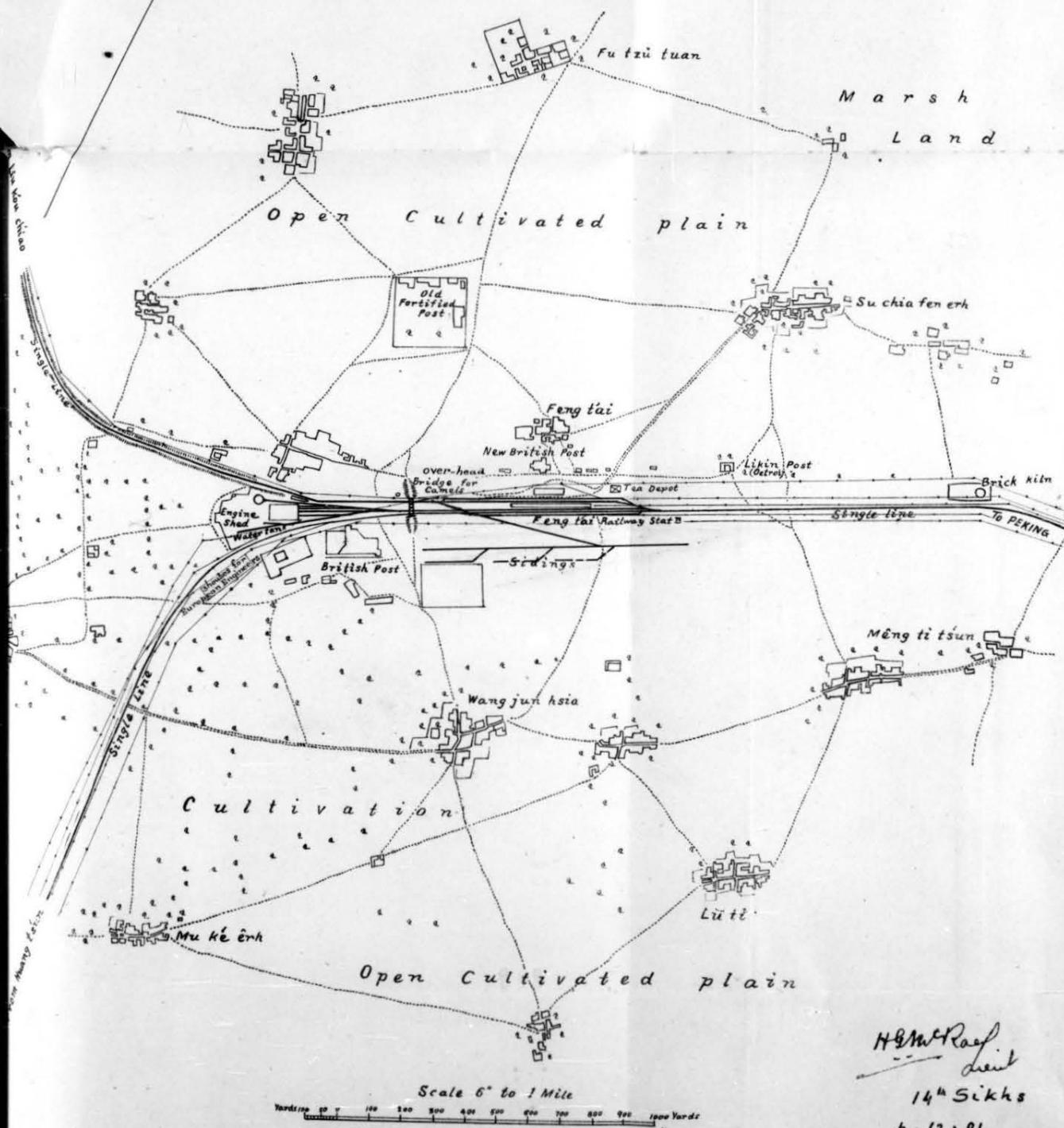


Heathfield
Lieut
14th Sikhs
8-12-07

豐台

Country 1 Mile round

FENG TAI RAILWAY STATION



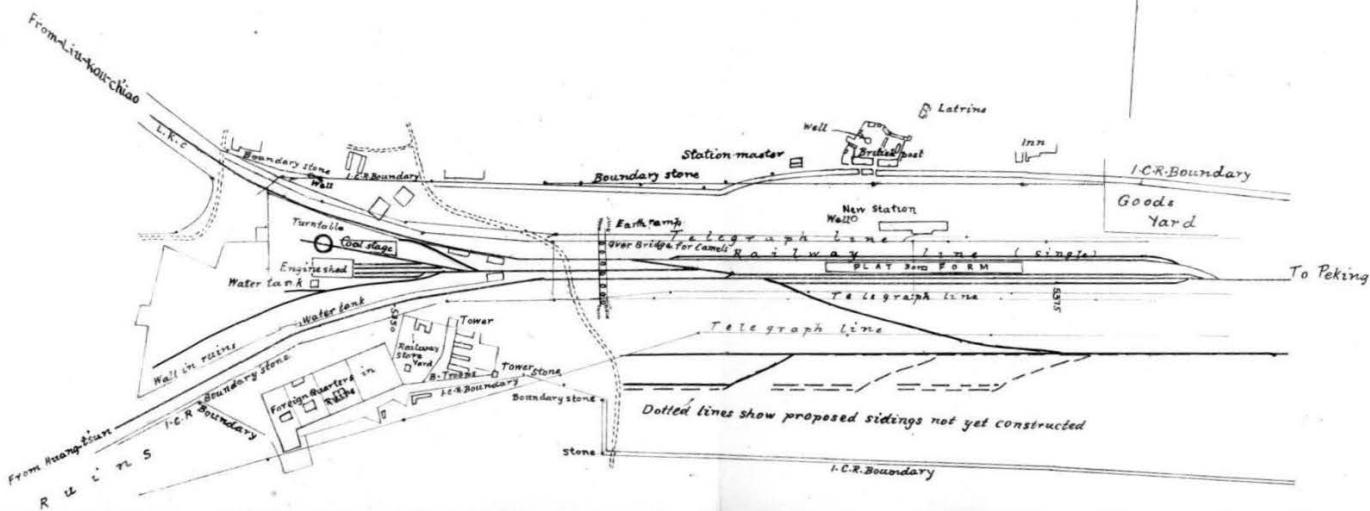
H&M Raef
14th Sikhs

6.12.01.

台 豊
FENG TAI STATION AND POST

82

Scale 400 Feet - 1 inch
 Feet 100 200 300 400 500 600 700 800 900 1000 1100 1200 1300 1400 1500 Feet
 From B.R.A. plans



壇天

PLAN OF TEMPLE OF HEAVEN STATION

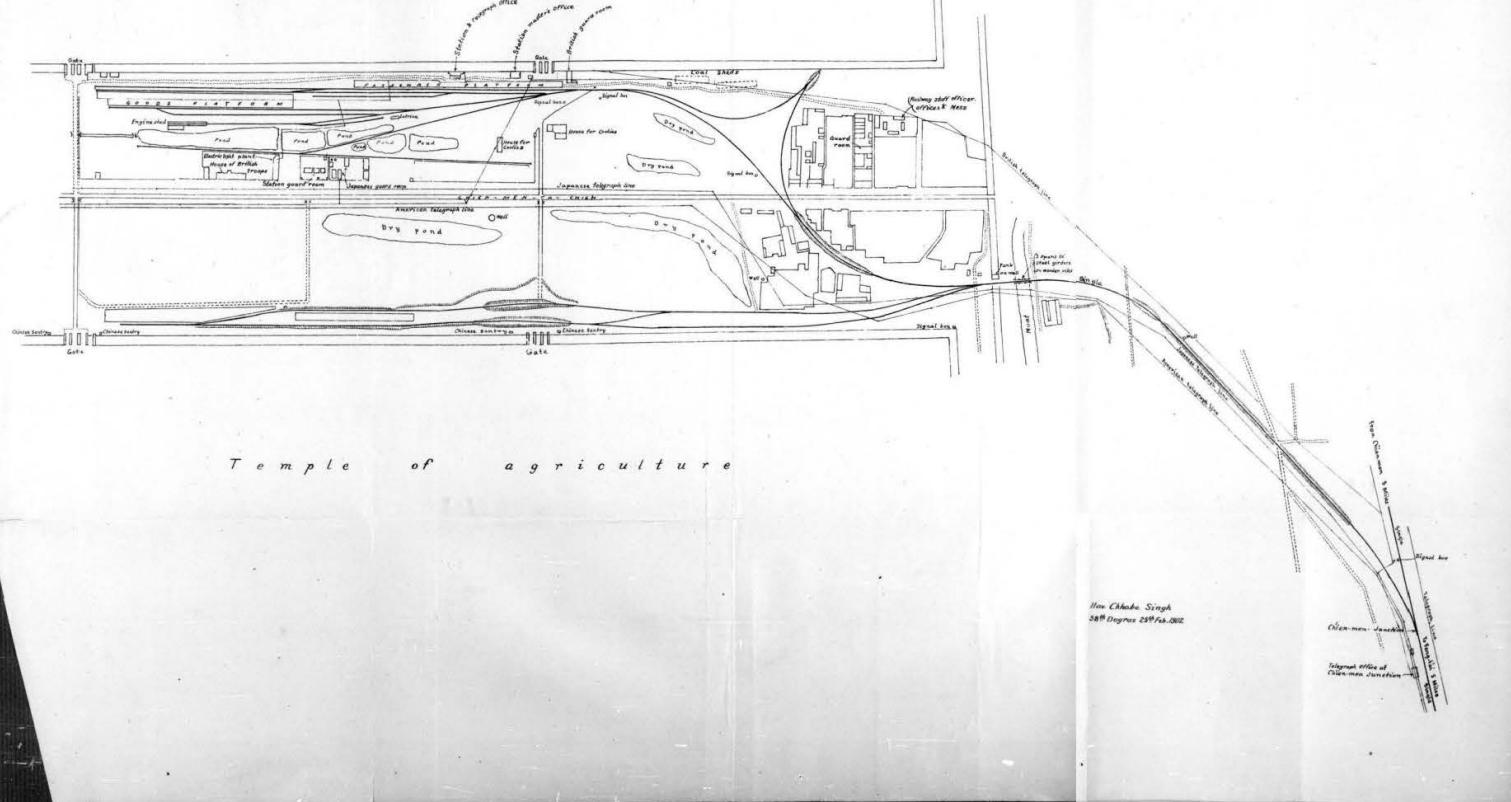
and line from Chien-men Junction

Scale 200 ft to 1 inch R.P. 2⁴/₅

Feet to ft
cm 0 200 400 600 800 1000 1200 1400 1600 1800 2000

23

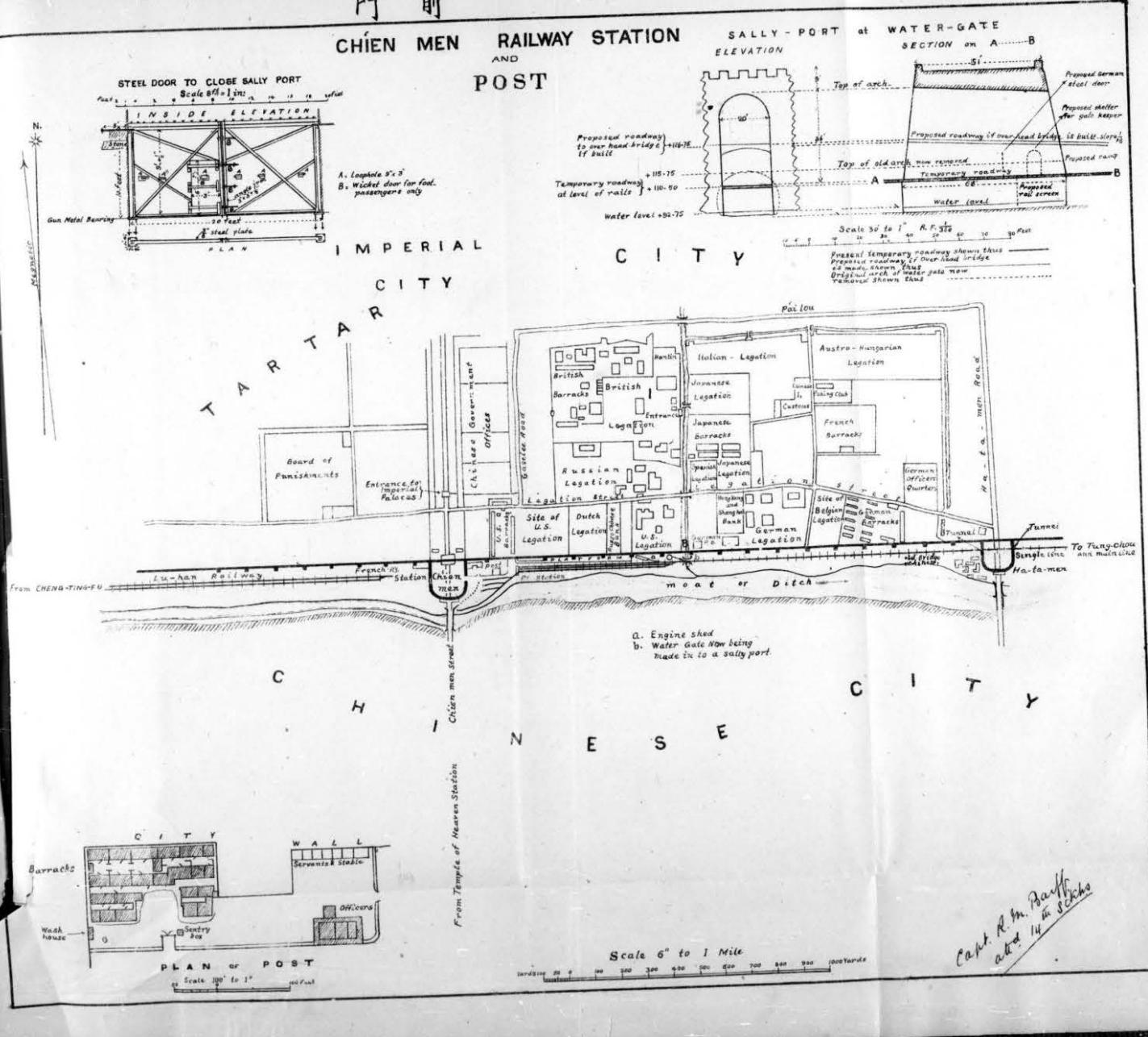
Temple of Heaven



門前

CHIEN MEN RAILWAY STATION
AND
POST

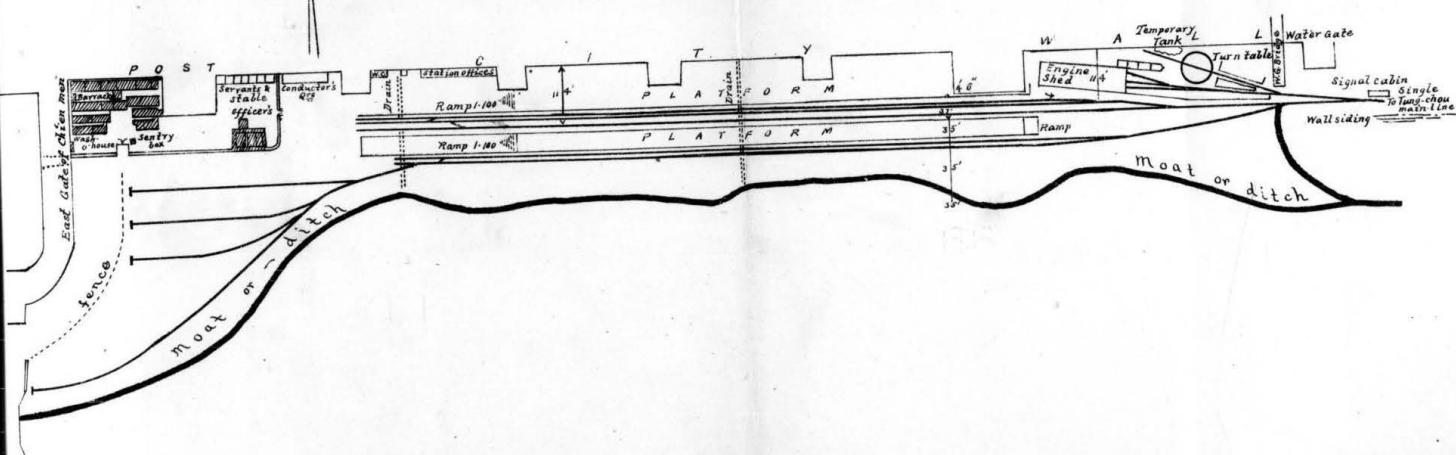
24



門前
PLAN OF CHIEN-MEN STATION
AND
POST PEKING

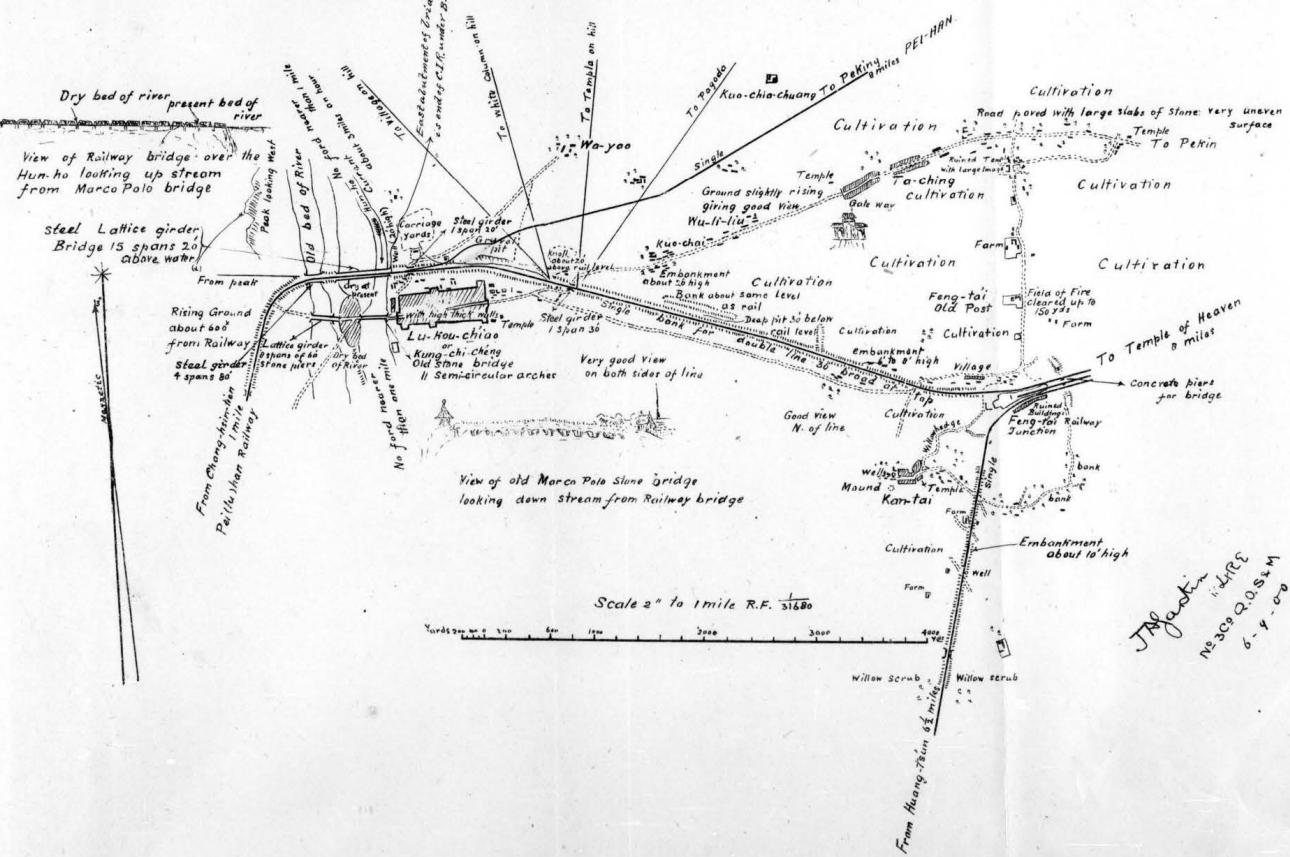
Scale 200 feet = 1 1/8 inch
Feet 0 50 100 200 300 400 500 600 Feet

From B.R.R. Plan



蘆溝橋

LU-KOU-CHIAO EXTENSION SHOWING JUNCTION WITH THE PEI (LU)HAN RAILWAY



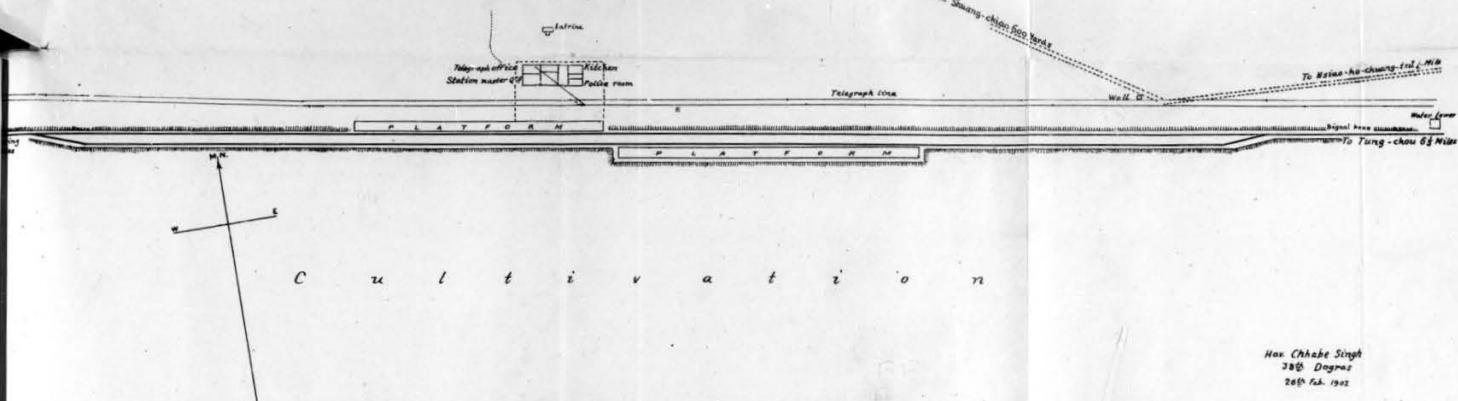
雙 橋

PLAN OF SHUANG-CHIAO STATION

Scale 100ft to 1 inch R.F. miles
Foot 0 50 100 150 200 250

Cultivation

Cultivation

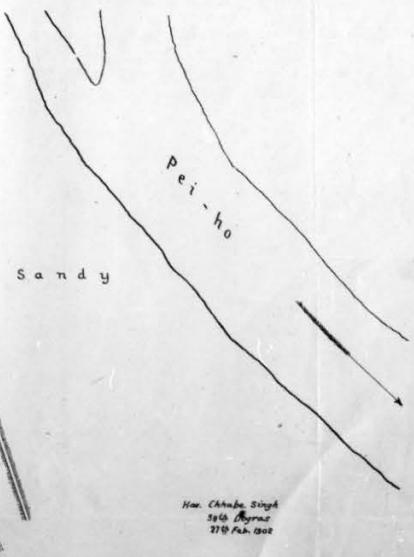


Hon. Chhabe Singh
386 Degrav
26th Feb. 1902

PLAN OF TUNG-CHOU STATION

Scale 200ft to 1 inch
Foot 0 50 100 150 200 250 300 350 400

Cultivation



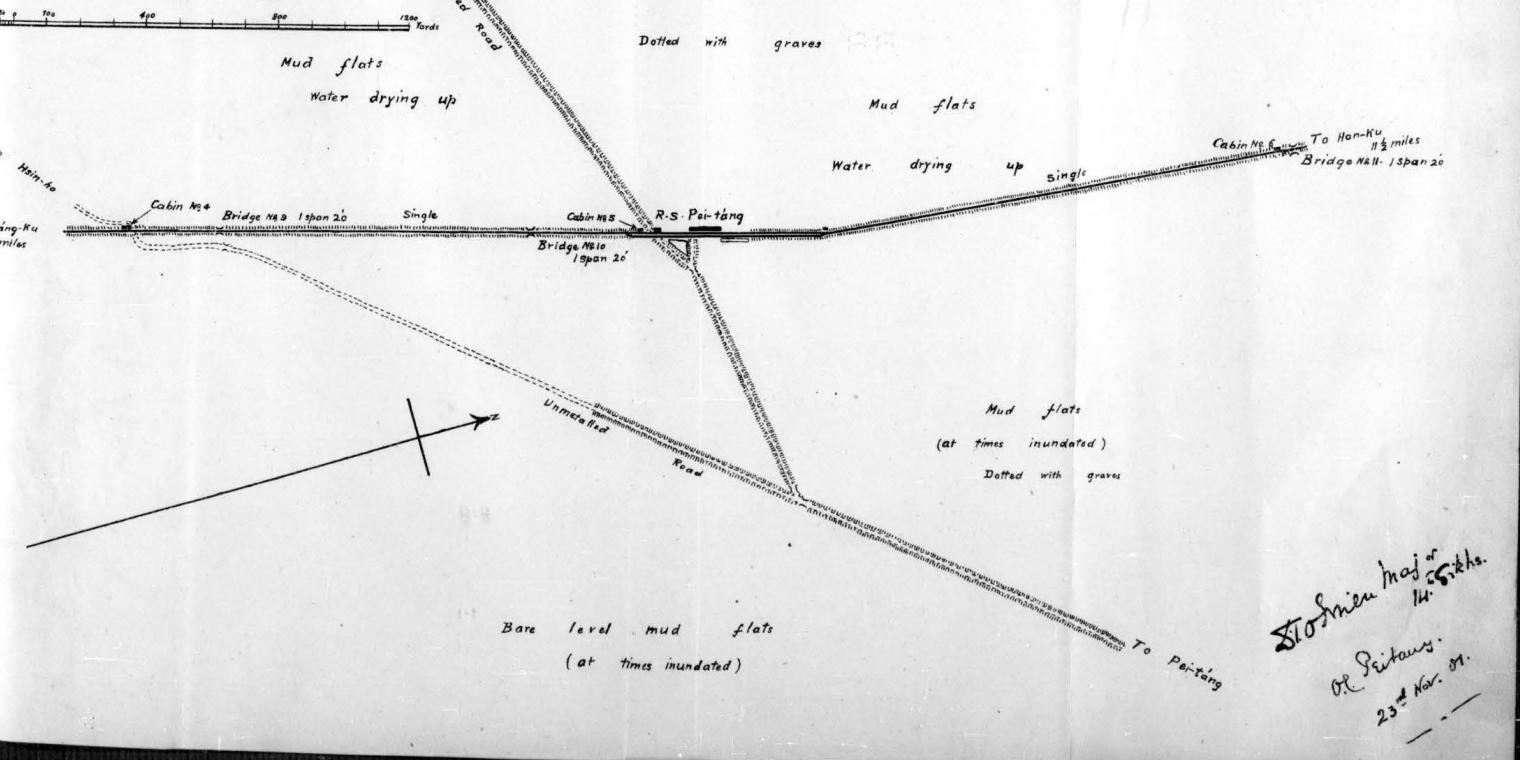
Hon. Chhabe Singh
386 Degrav
27th Feb. 1902



塘北

PEI-T'ANG STATION
and one mile round

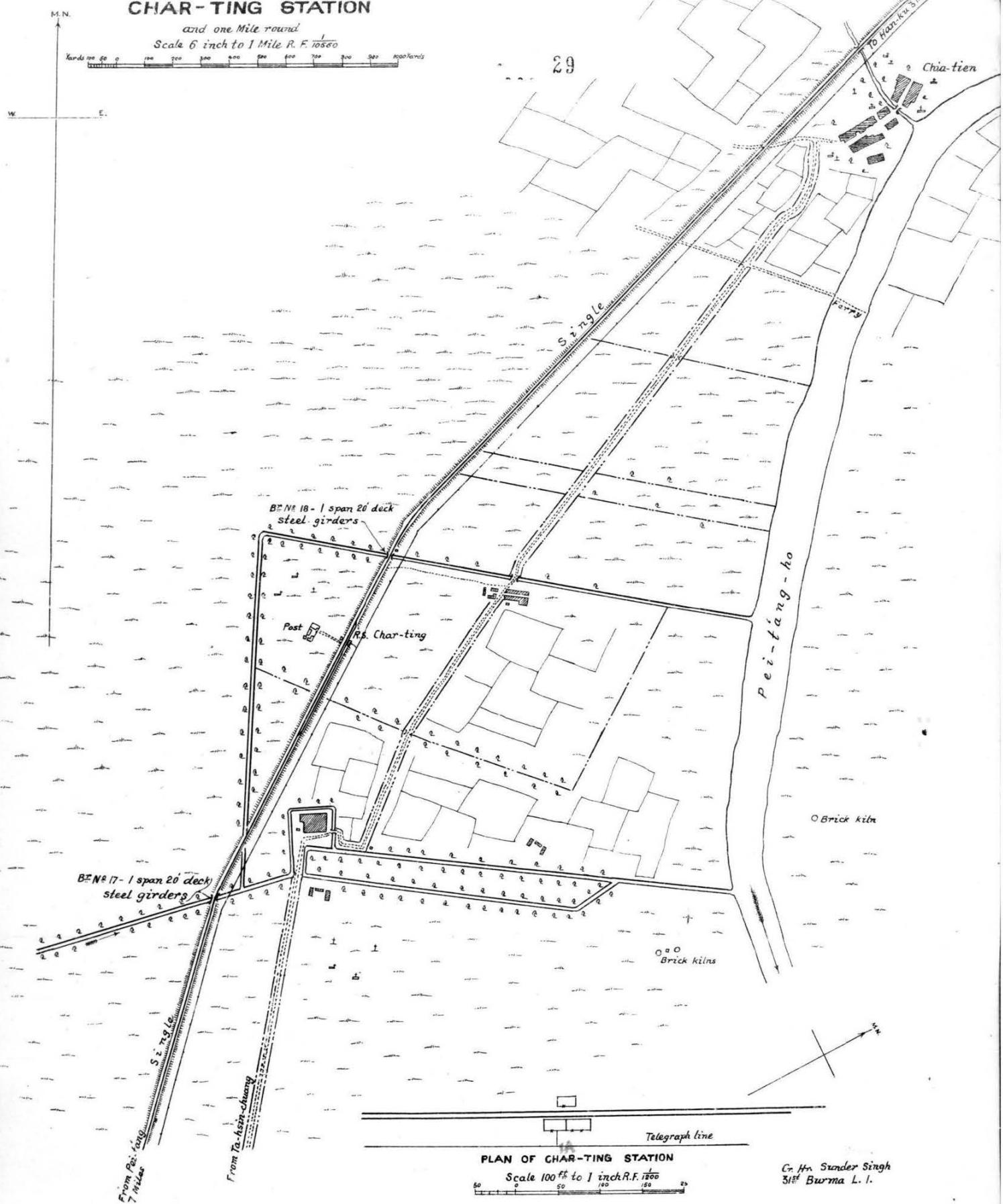
Scale 6" to 1 mile R.F.



CHAR-TING STATION

Scale 6 inch to 1 Mile R.F. 10500
and one Mile round

Yards 0 100 200 300 400 500 600 700 800 900 1000



PLAN OF CHAR-TING STATION

Scale 100 ft to 1 inch R.F. 10500

60 100 140 180 220

Cr. Hn. Sunder Singh
31st Burma L.I.

沽漢

HAN-KU STATION

and one Mile round

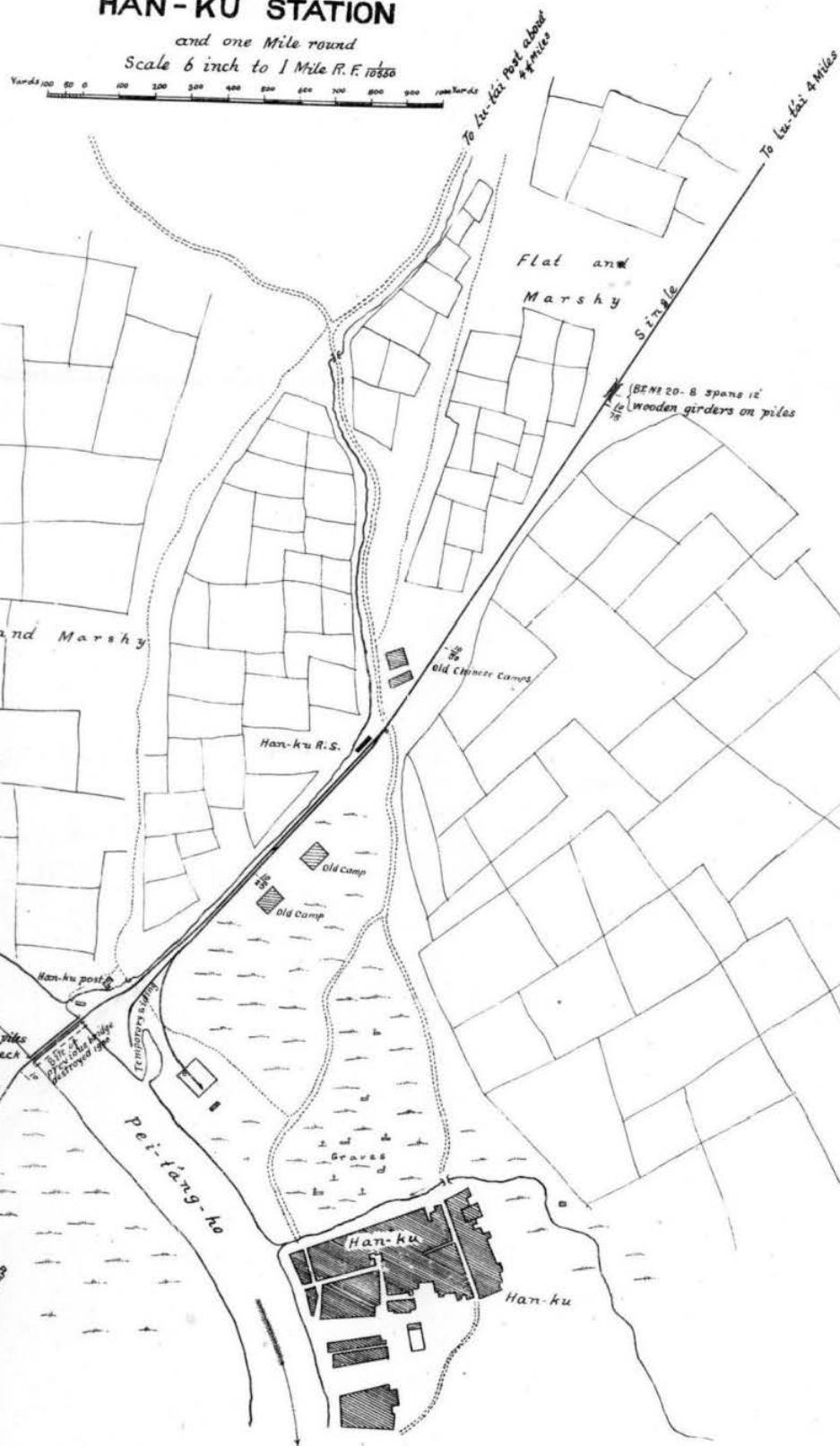
Scale 6 inch to 1 Mile R.F. 1/6500

30

M.N.

W

E



T Timbrell. 16
O.C. Han-ku post

PLAN OF HAN-KU STATION

Scale 100 ft to 1 inch R.F. 1/2000



台盧 LU-TAI STATION

and One Mile round.

Ref. The Squares represent sites of former Chinese Camps now nearly destroyed and the ground being cultivated. The Country is intersected by feed-canals the largest being the canal running East from the river across the railway. The railway embankment is the highest ground in the neighbourhood.

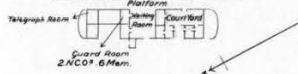
Scale, 6 to 1 Mile. R.F. 10560.

Yards 100 50 0 100 200 400 600 800 1000 1200 1500 Yards.

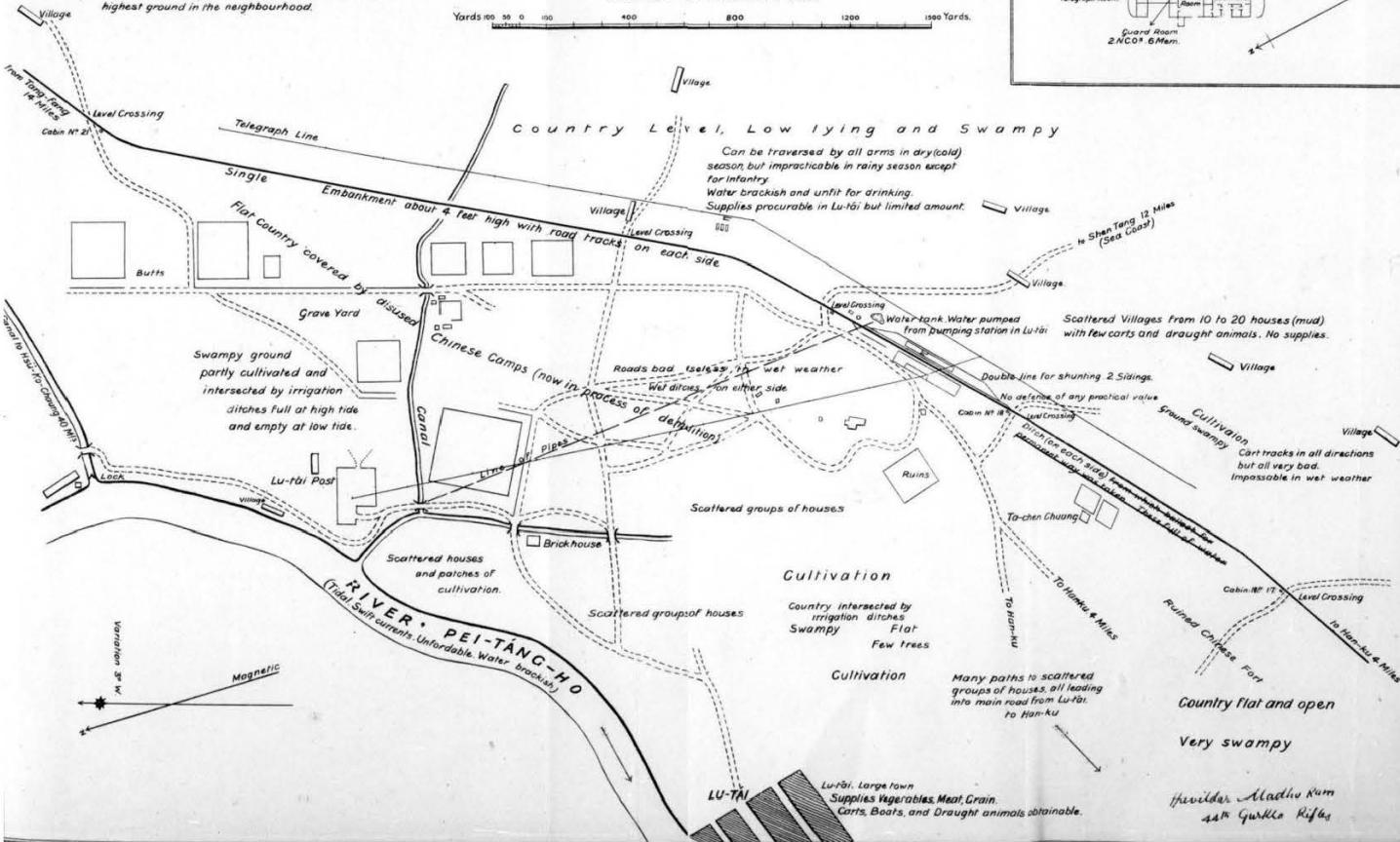
PLAN OF LU-TAI STATION

Scale 100 Feet to 1 Inch

Feet 0 200 400 600 800 1000 1200 1400



31



坊 唐

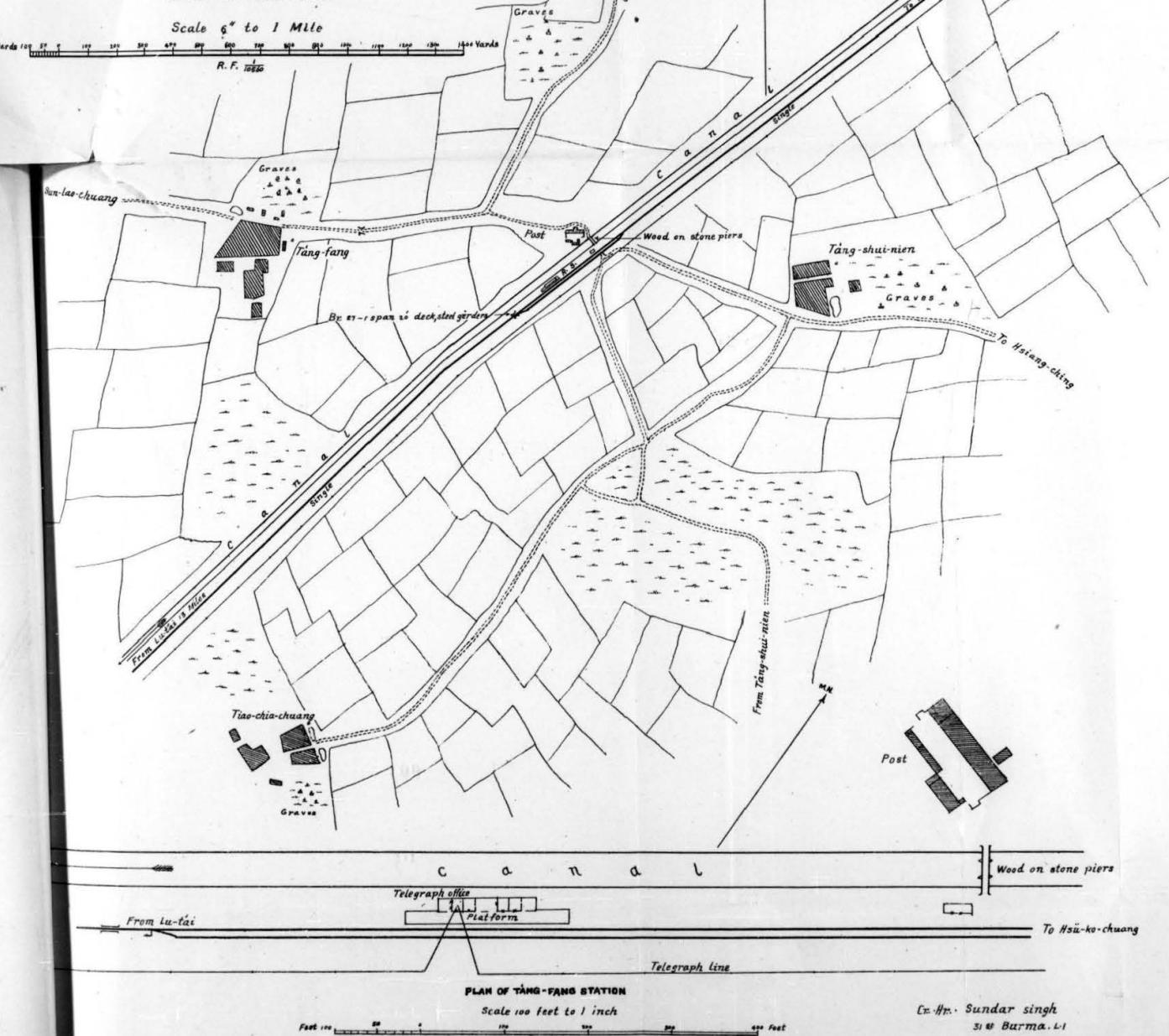
TANG-FANG STATION

and one mile round

Scale 6" to 1 Mile

R.F.

10000



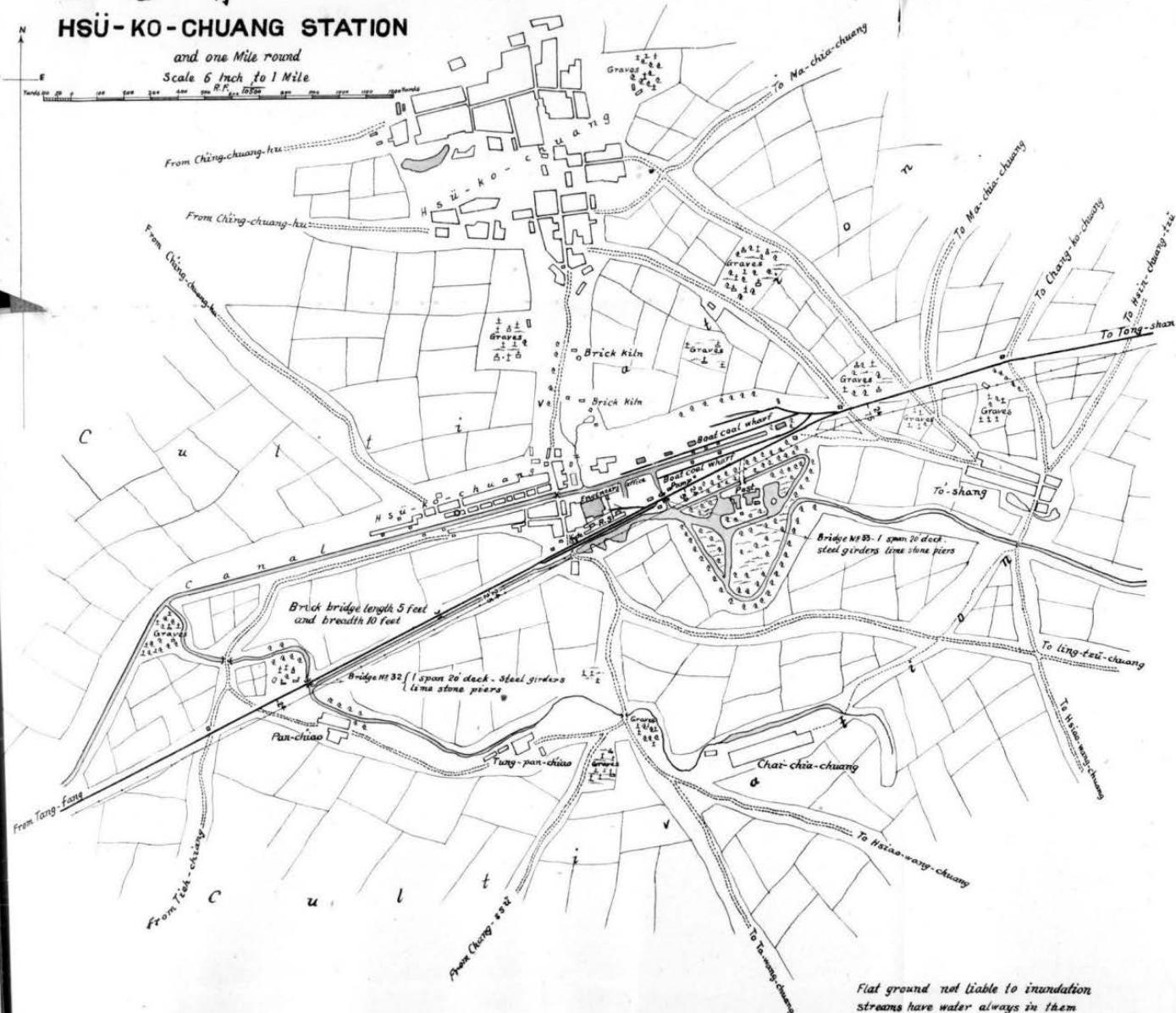
In Hn. Sundar singh
S/o Barma. Li

胥各庄

HSÜ-KO-CHUANG STATION

and one Mile round

Scale 6 Inch to 1 Mile



Flat ground not liable to inundation
streams have water always in them
Canal 10 feet broad and 8 feet deep

PLAN OF HSÜ-KO-CHUANG STATION

Telegraph office

Ticket office

Guard room

Scale 100 feet to 1' R.F. 1/22

Feet 100 200 300 400 500 600 700 800 900 1000

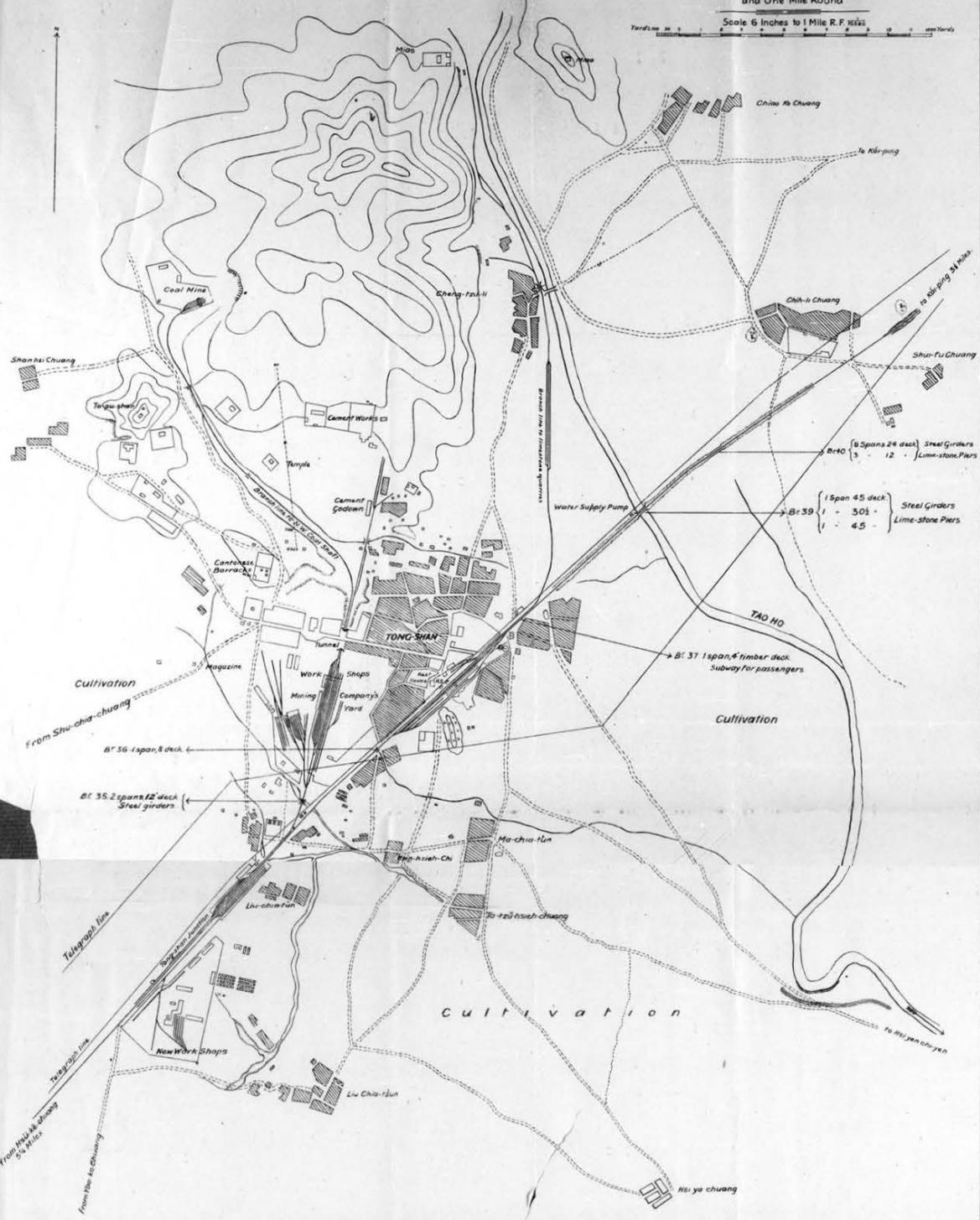
Cr. Mr. Sundar Singh
366 Burma L. I.

TONG-SHAN STATION

and One Mile Round

Scale 6 Inches to 1 Mile R.F. 1922

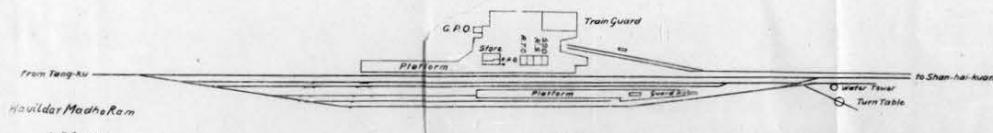
Scale 6 Inches to 1 Mile R.F. 1000 ft.



PLAN OF TONG-SHAN STATION

Scale 200 feet to 1 inch

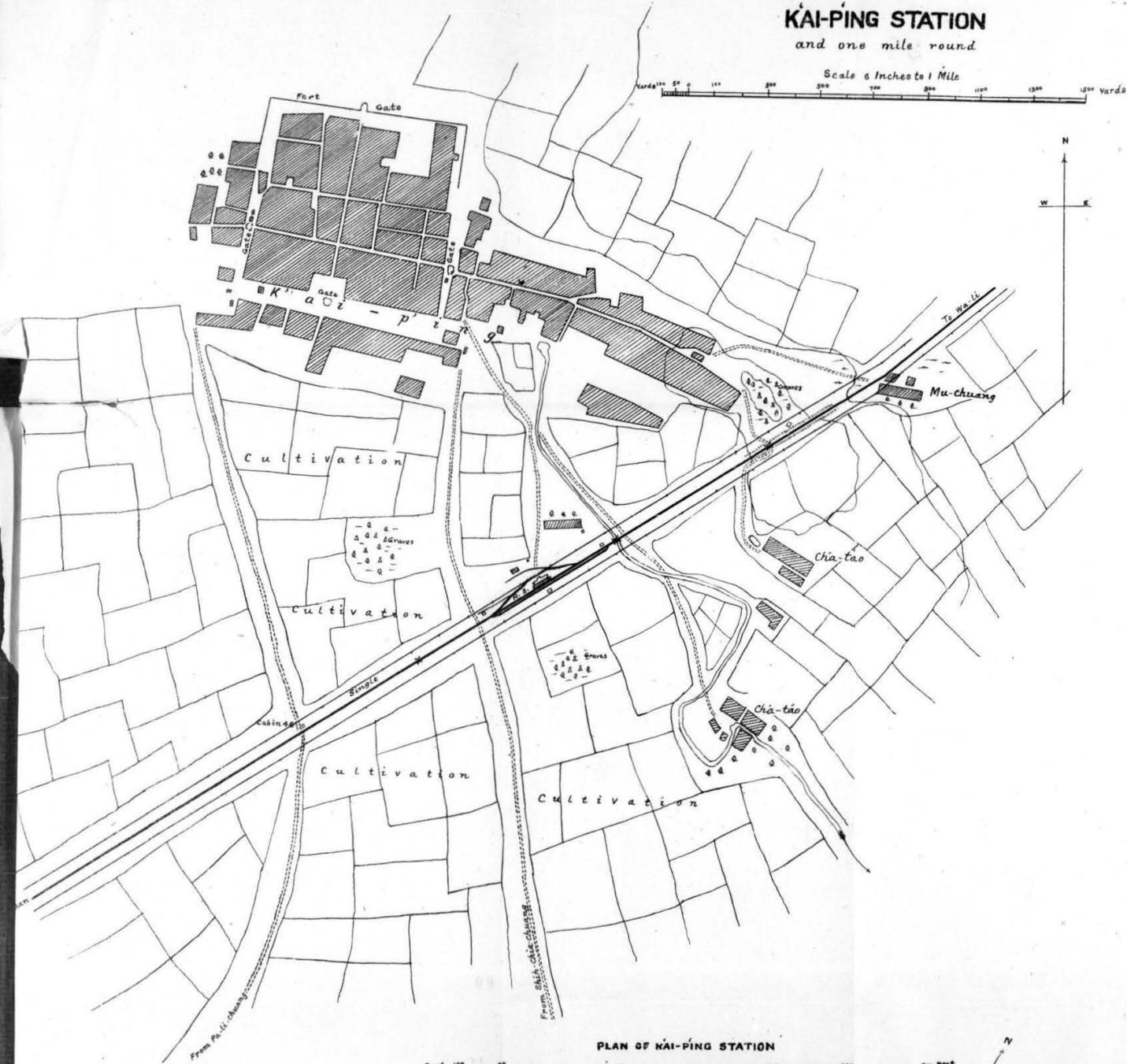
Scale 200 feet to 1 inch



平開
KAI-PING STATION

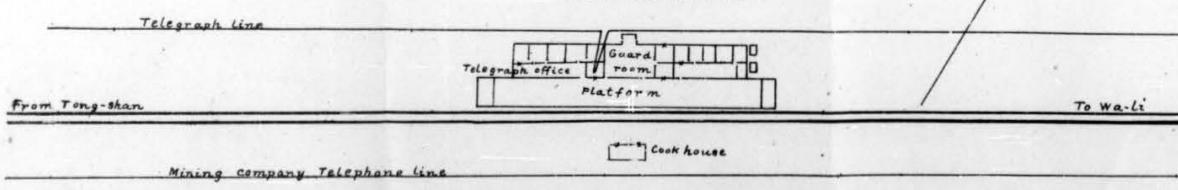
and one mile round

35



PLAN OF KAI-PING STATION

Feet 100 200 300 400 500 600 700 800 900 1000
Scale 1 inch to 100 feet



Cr. Hr. Sundar Singh
6th Burma L.I.

里窪

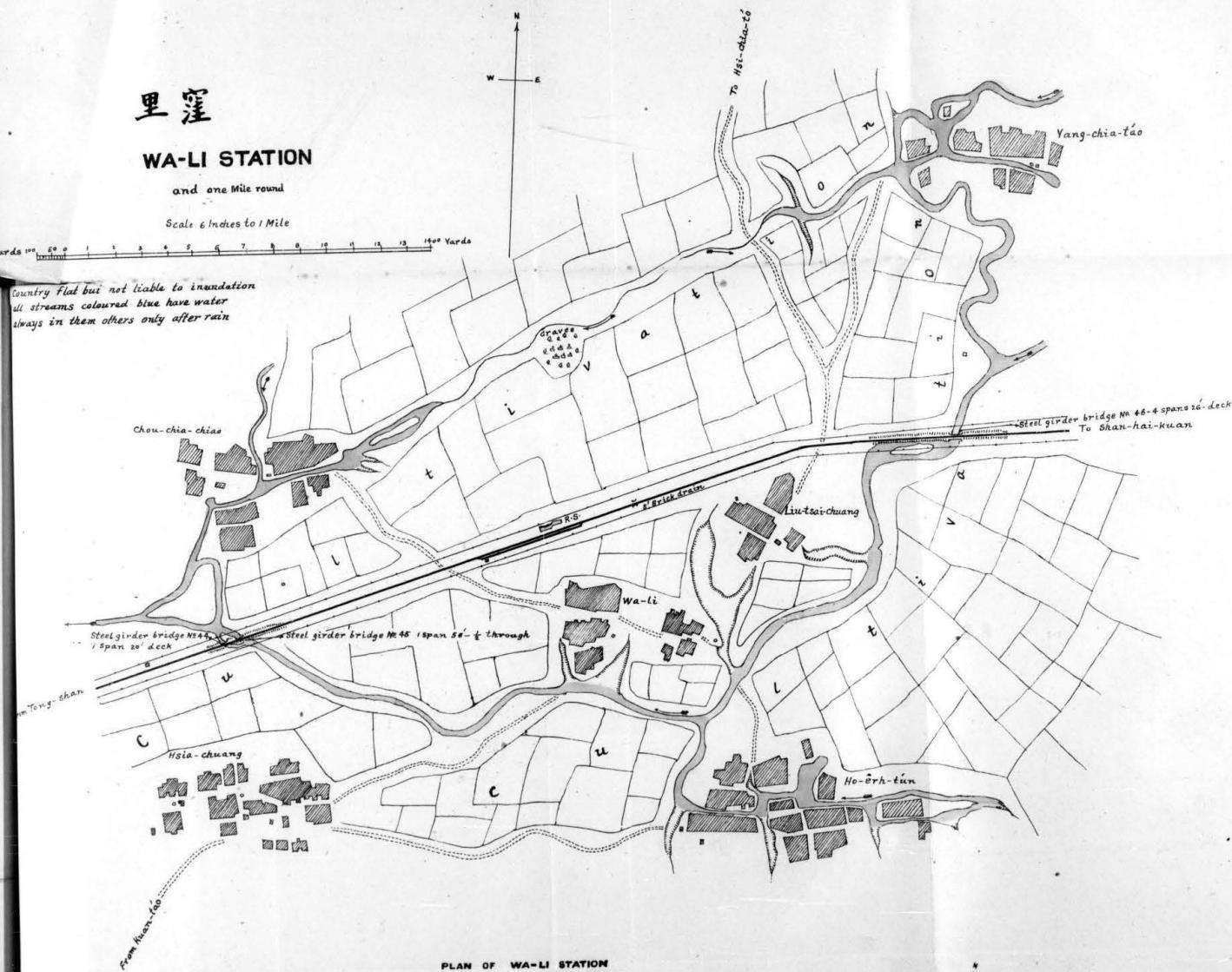
WA-LI STATION

and one Mile round

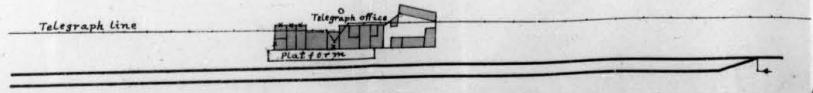
Scale 6 Inches to 1 Mile

yards 100 50 0 1 2 3 4 5 6 7 8 9 10 11 12 13 1400 Yards

Country flat but not liable to inundation
all streams coloured blue have water
always in them others only after rain



PLAN OF WA-LI STATION



Mining company Telephone line Tong shan to Ku-yeh

Scale 100 feet to 1 Inch

Feet 100 200 300 400 500 Feet

Havildar Major Thakur Singh
S.I. Burma L.I.

治古

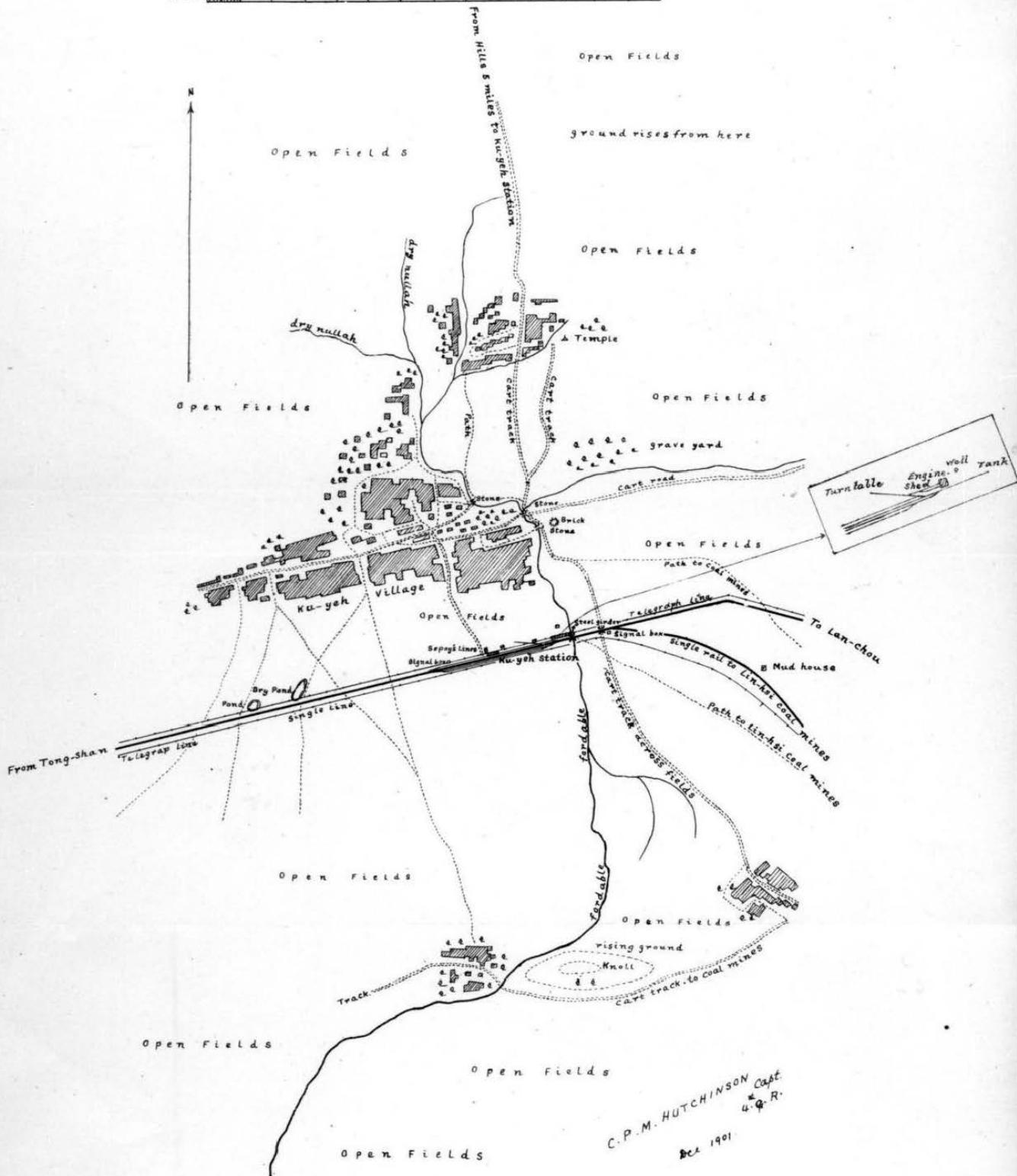
KU-YEH STATION

and one mile round

37

Scale 6 Inches to 1 Mile

Yards 100 200 300 400 500 600 700 800 900 1000 1100 1200 Yards

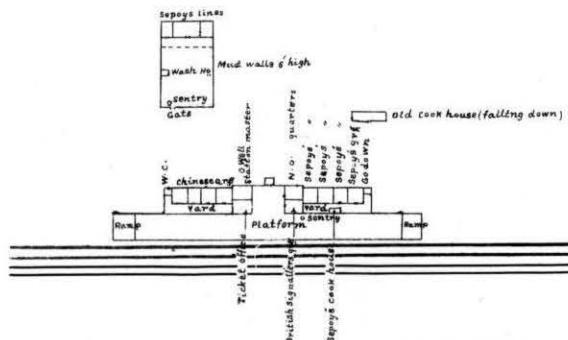


治古

PLAN OF KU-YEH RAILWAY STATION

Scale 100 feet to 1 inch

Footing 50 100 150 200 *Feet*



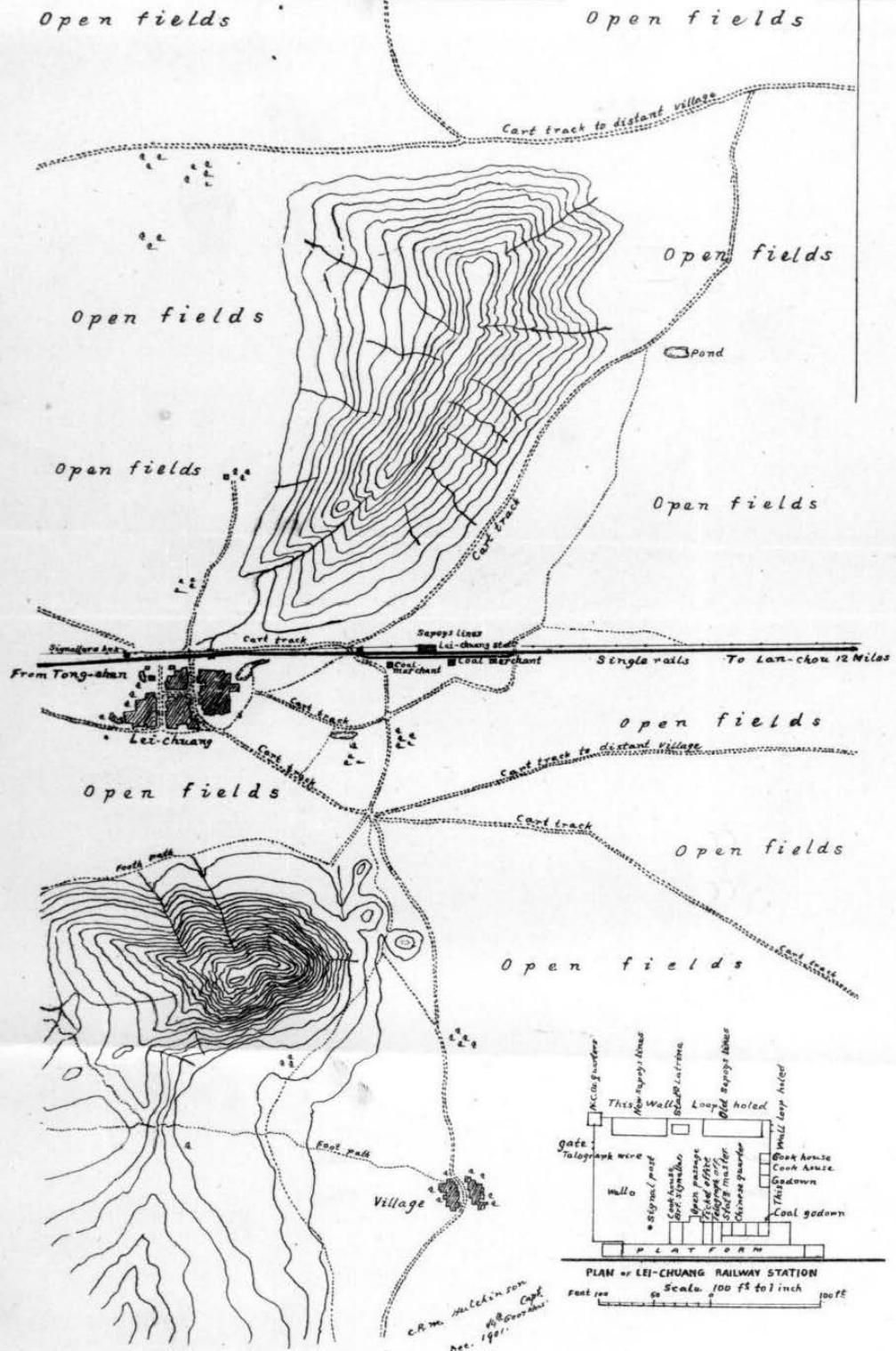
C.R.M. HUTCHINSON
Capt.
Dec. 1901 4th G.R.

雷庄
LEI-CHUANG STATION

and one Mile round
Scale 6 inches to 1 Mile.

Scale 6 inches to 1 Mile

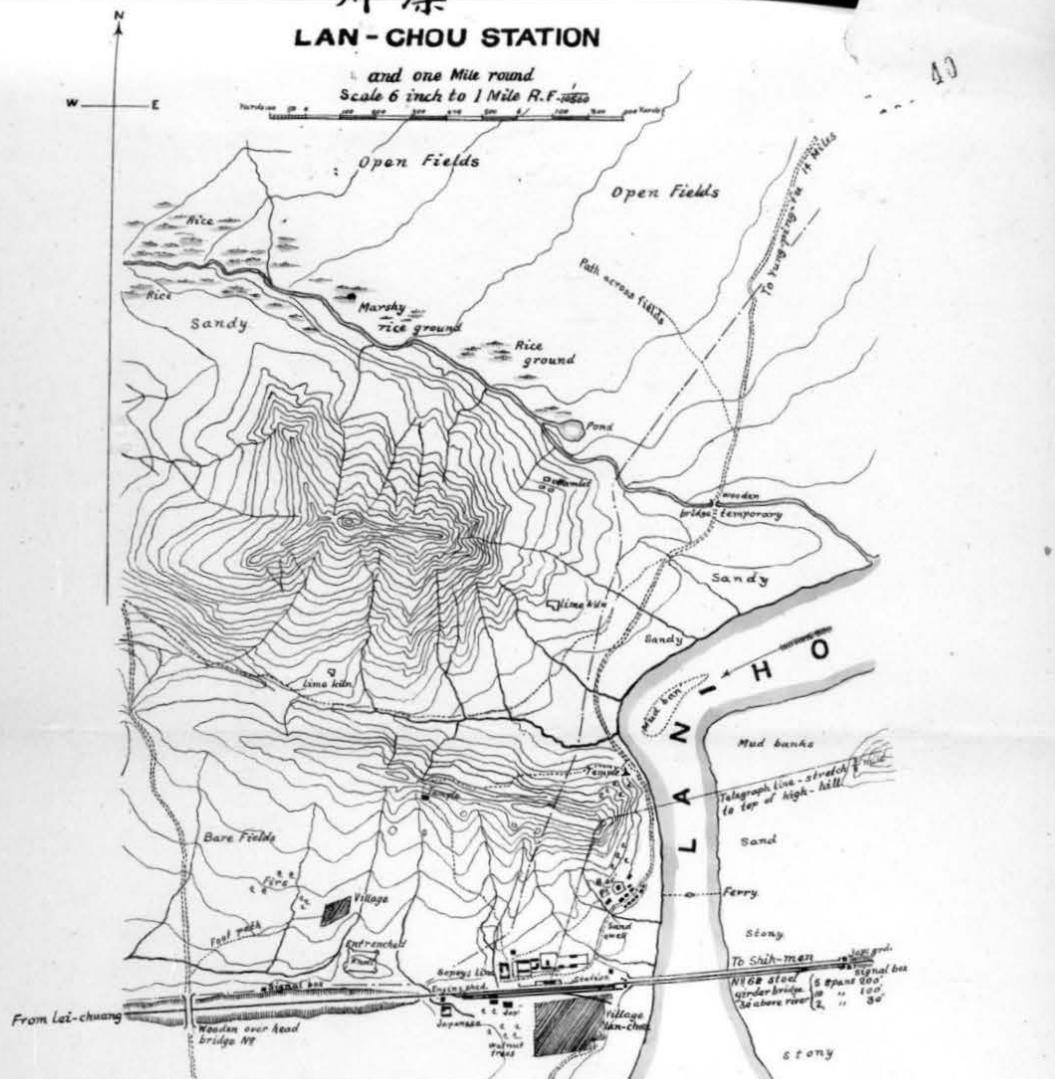
Yards



LAN - CHOU STATION

and one Mile round

scale 6 inch to 1 Mile R.F. ~~10860~~



From Lei-chuang

Open Fields

Opera Fiula
Iacit sandu

57

29

四百三

10

11

- PLAN OF LAN-CHOU STATION

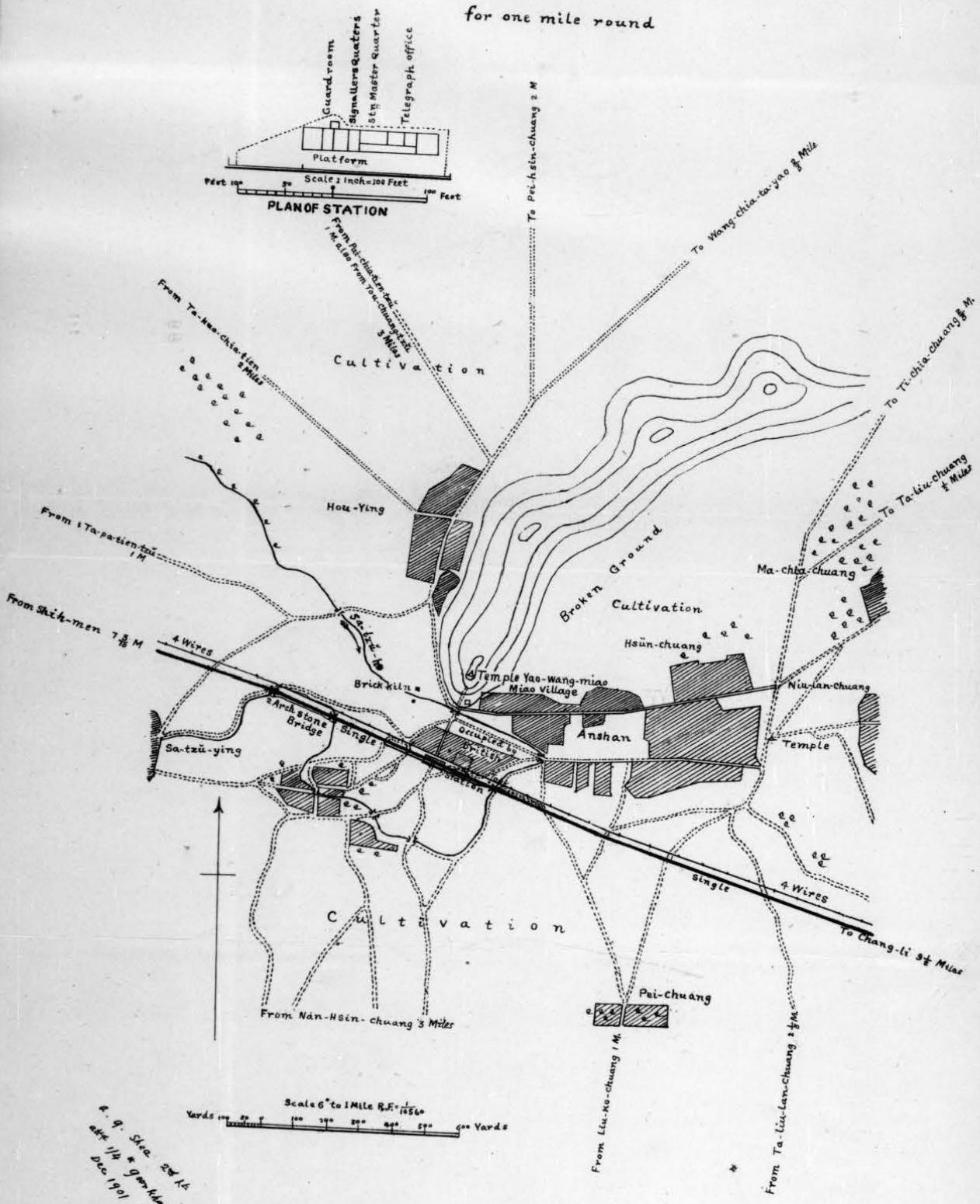
Scale 100' to 1 inch R.F. 1000

Scale 1 mile 5 miles 10 miles 15 miles *Sea Level*

AN-SHAN POST

42

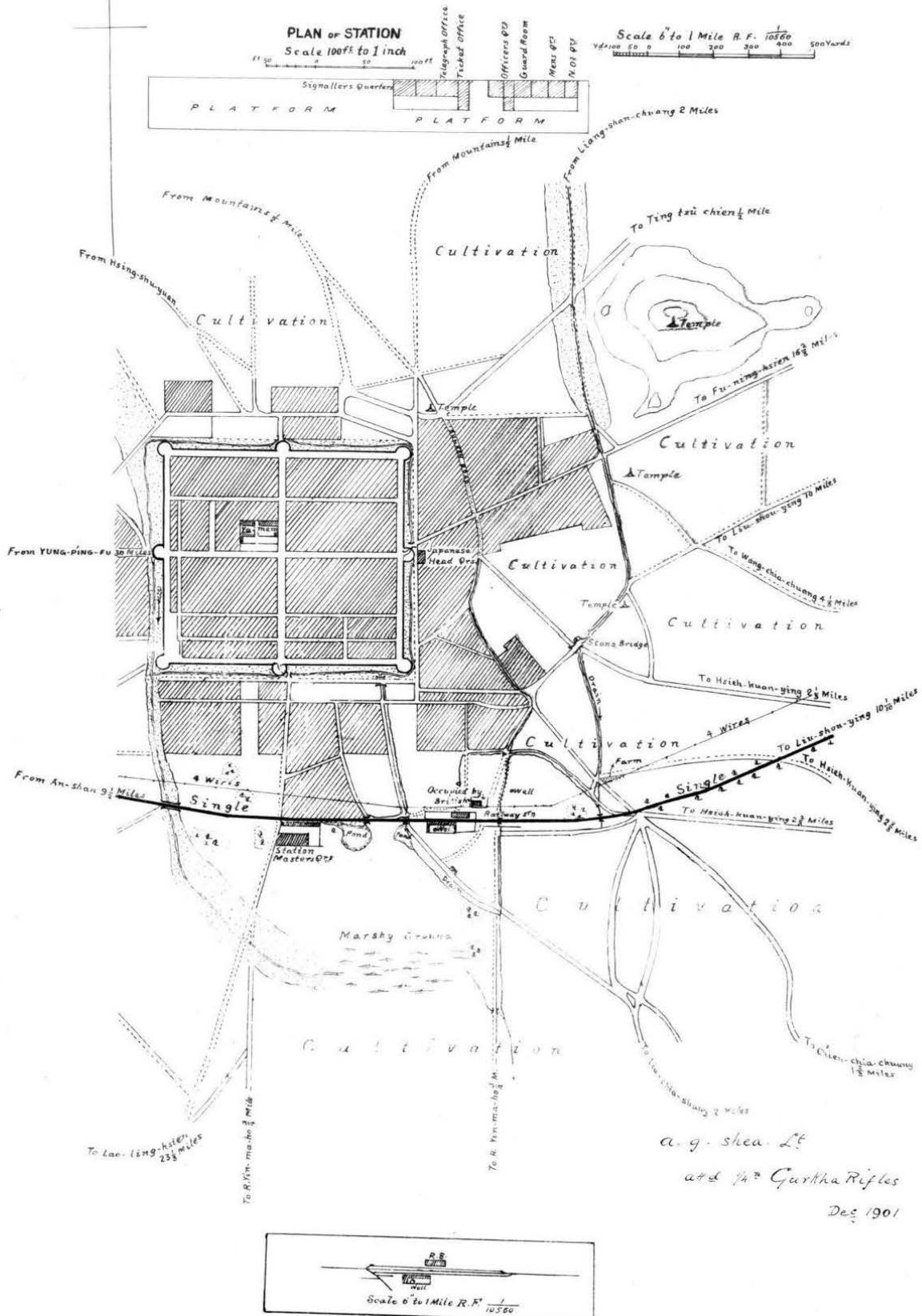
for one mile round



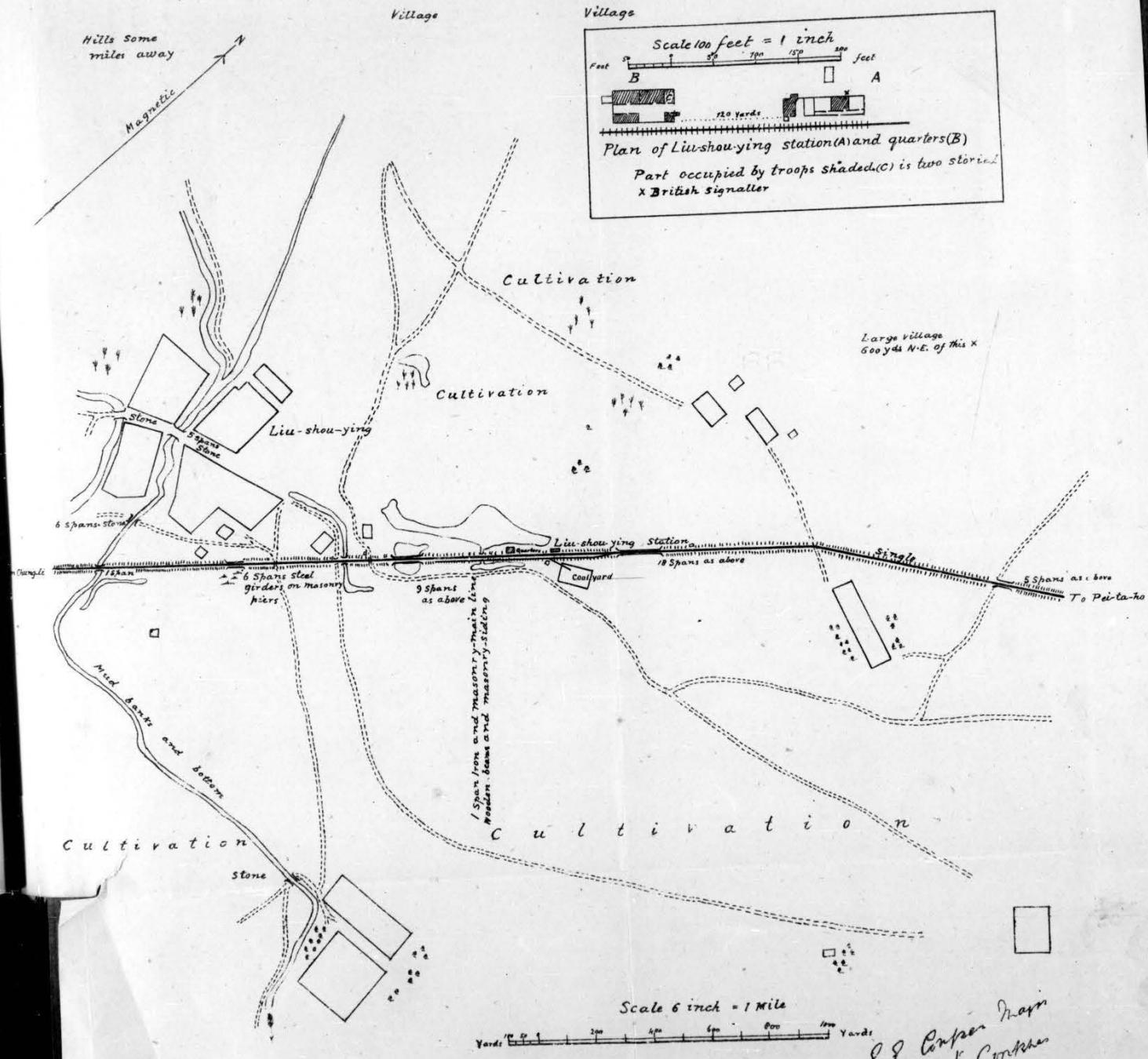
昌黎

CHANG-LI RAILWAY STATION

and Country for one Mile round



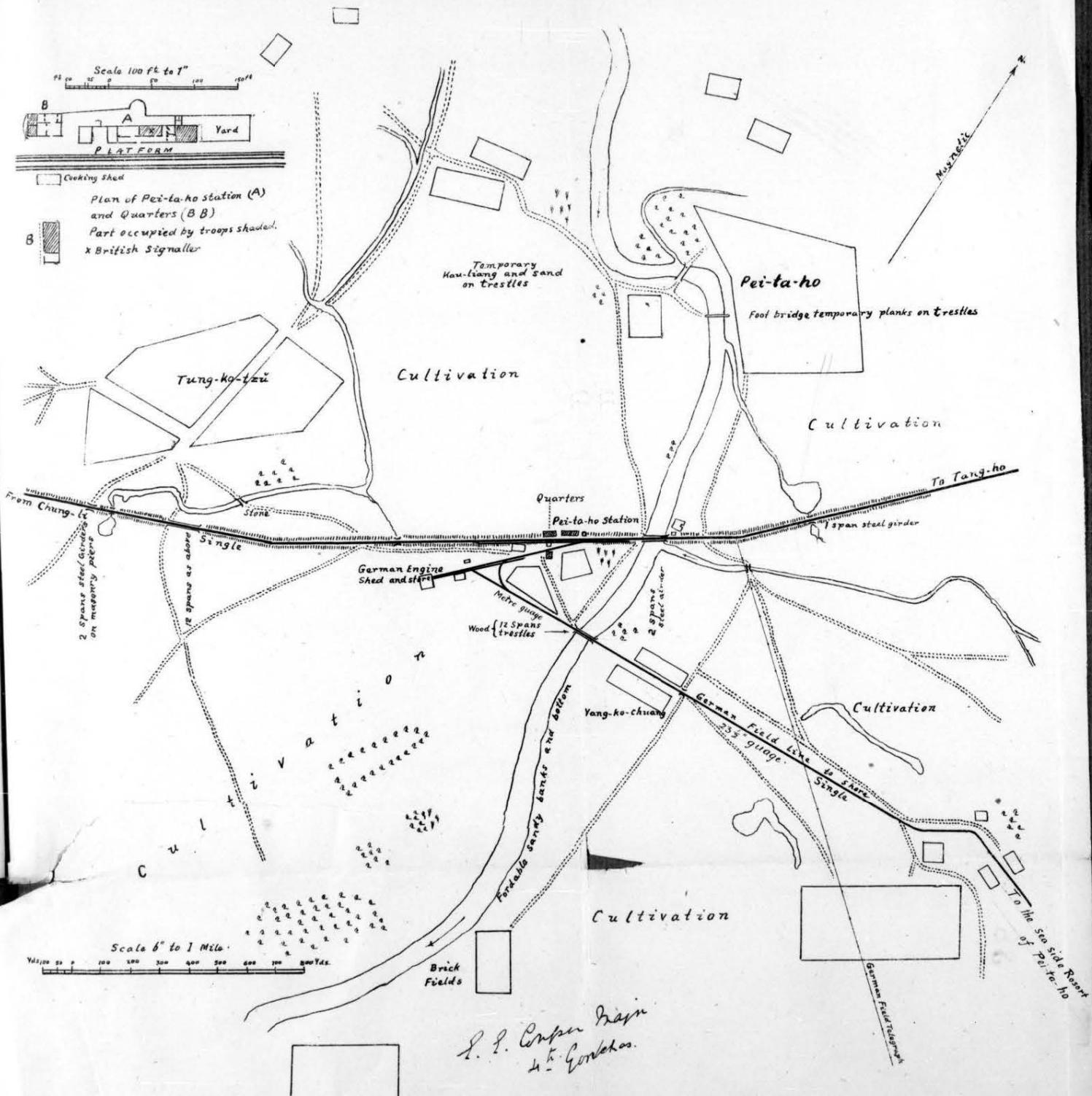
營守營
LIU-SHOU-YING STATION
AND
one mile round

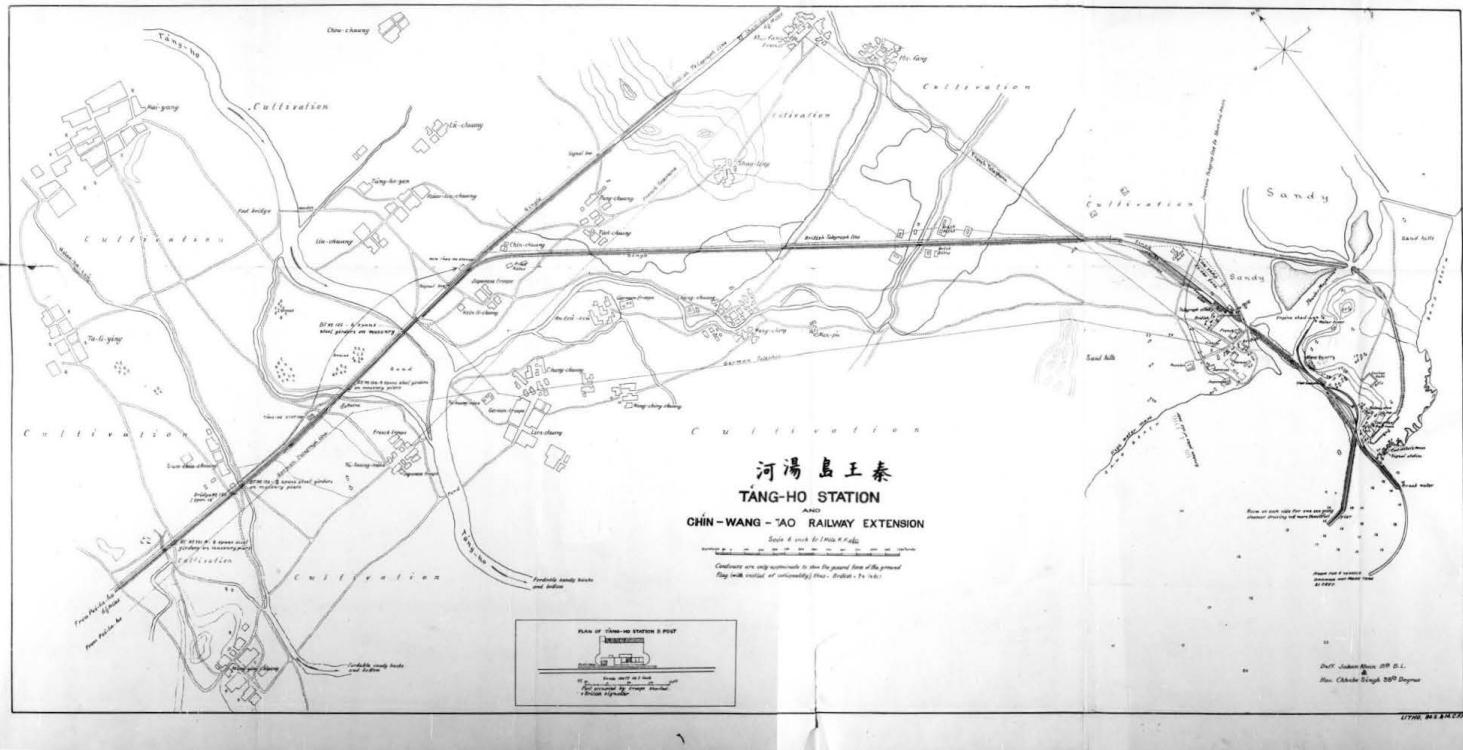


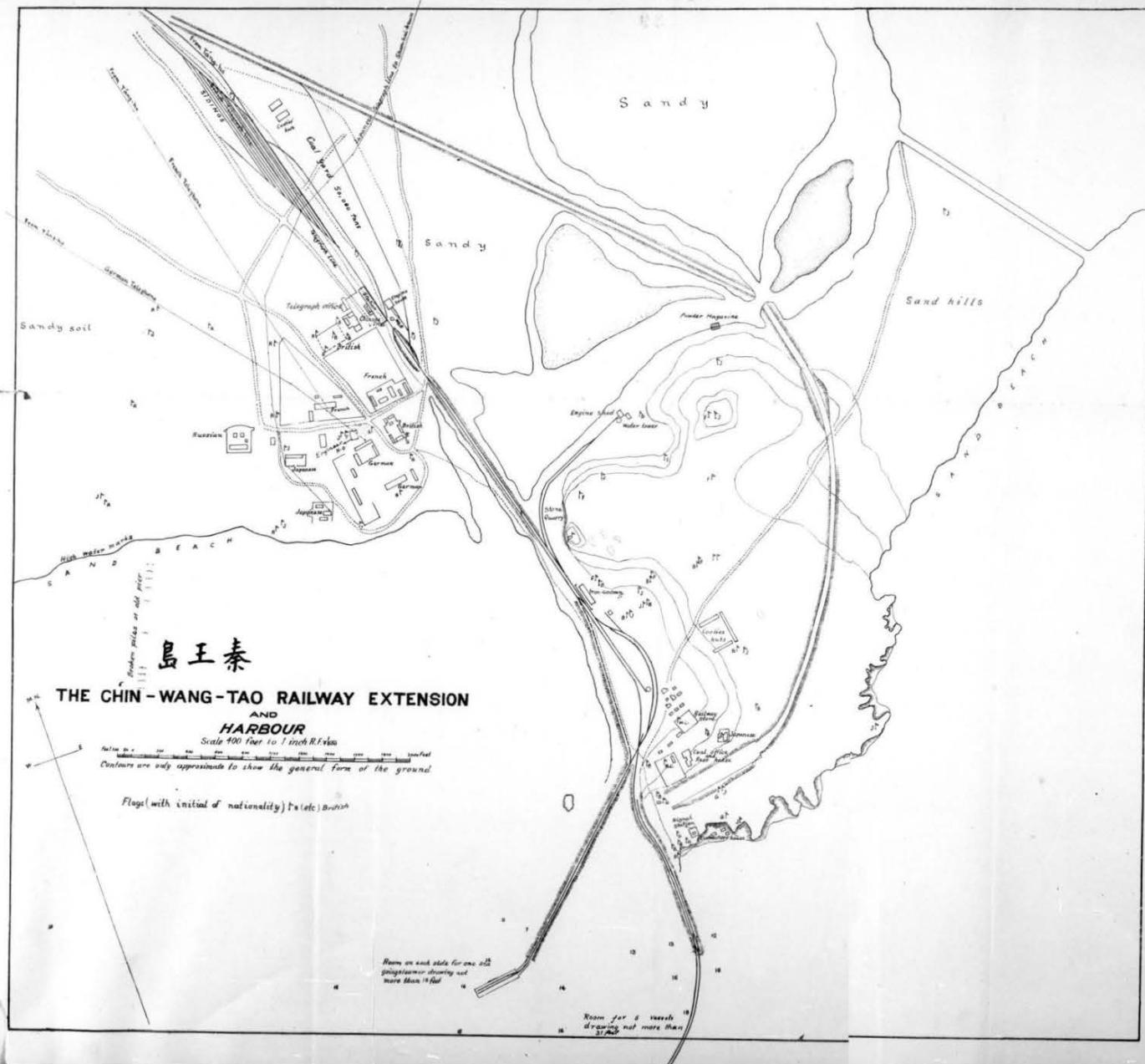
北戴河

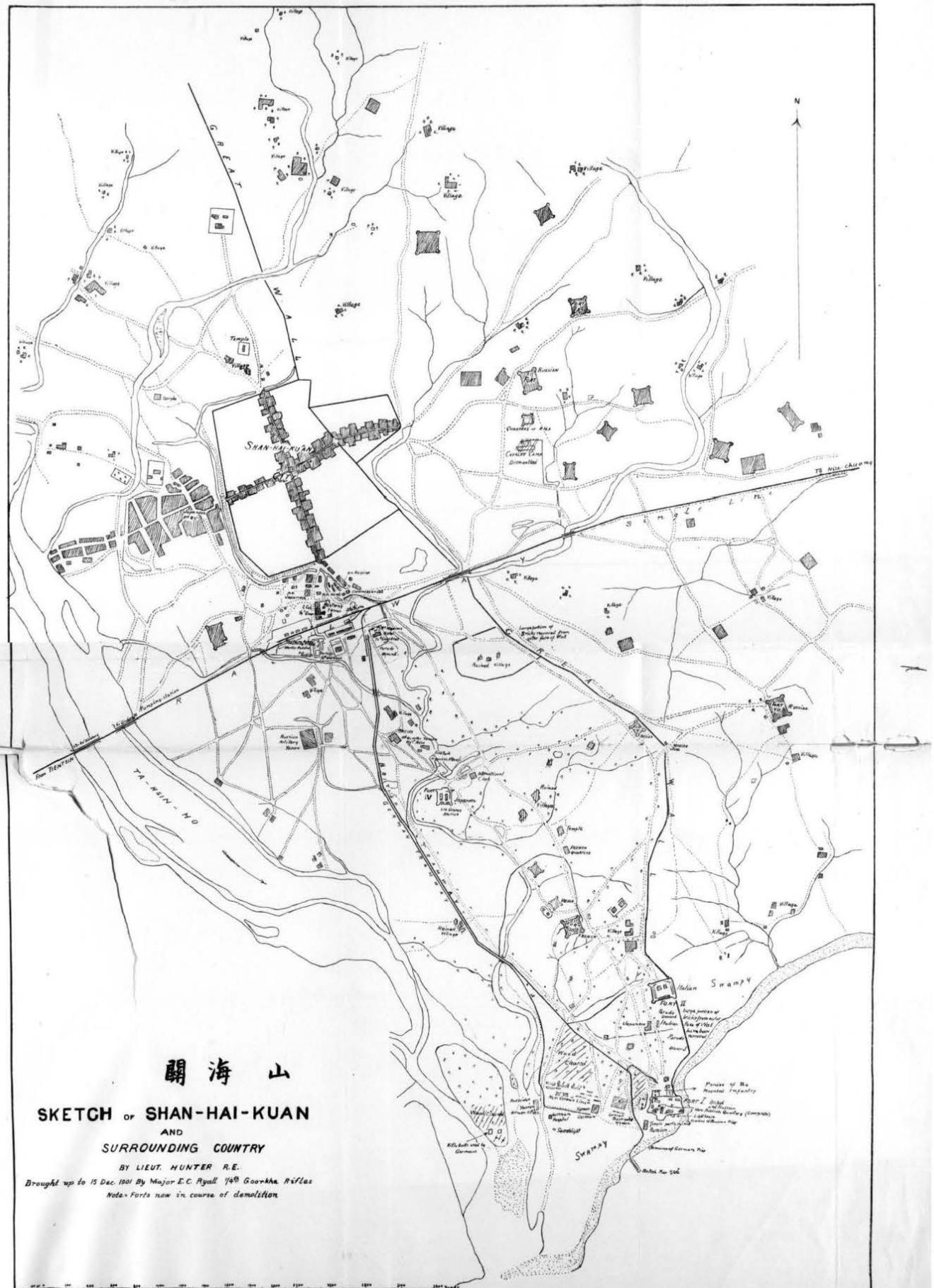
45

PEI-TA-HO RAILWAY STATION and country for one mile round







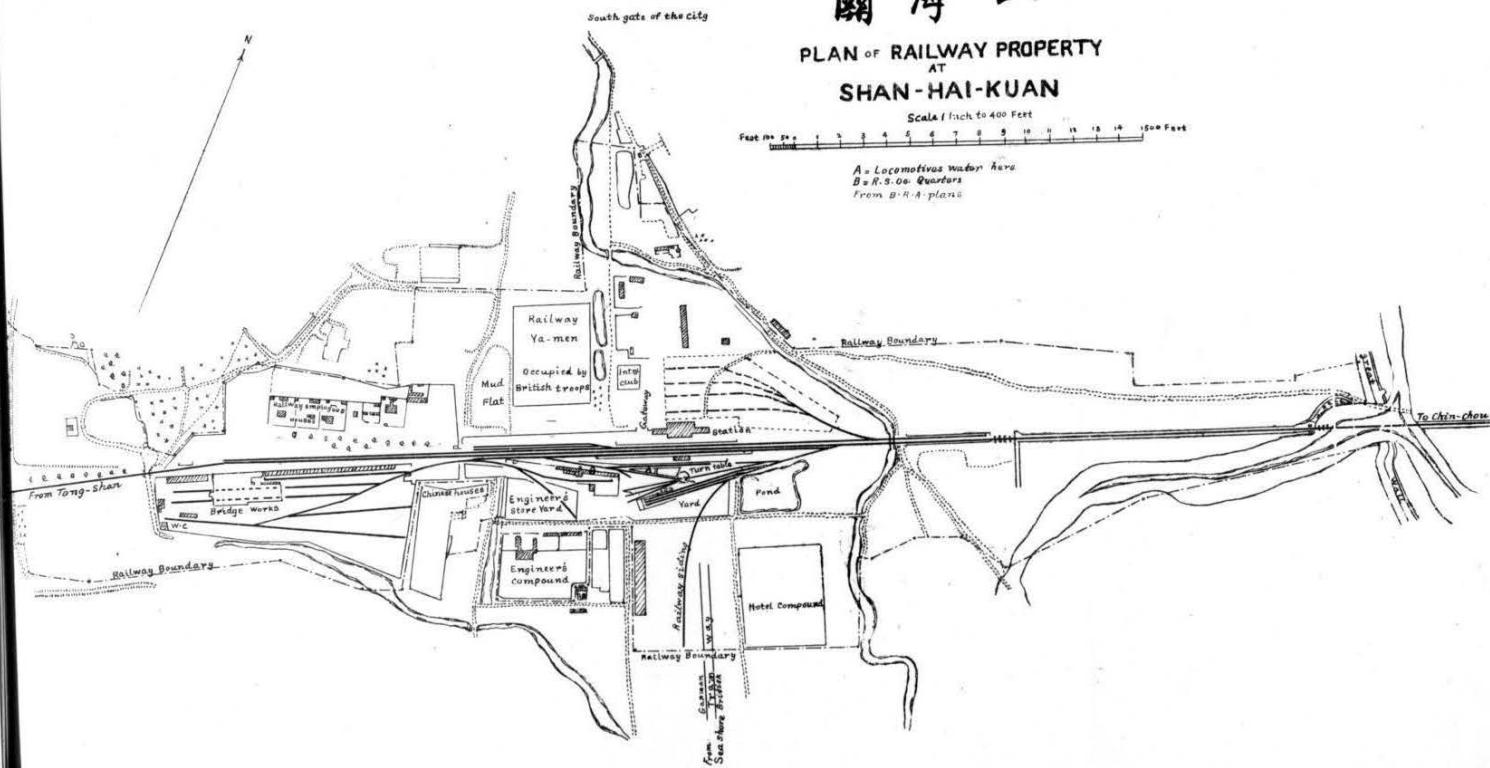


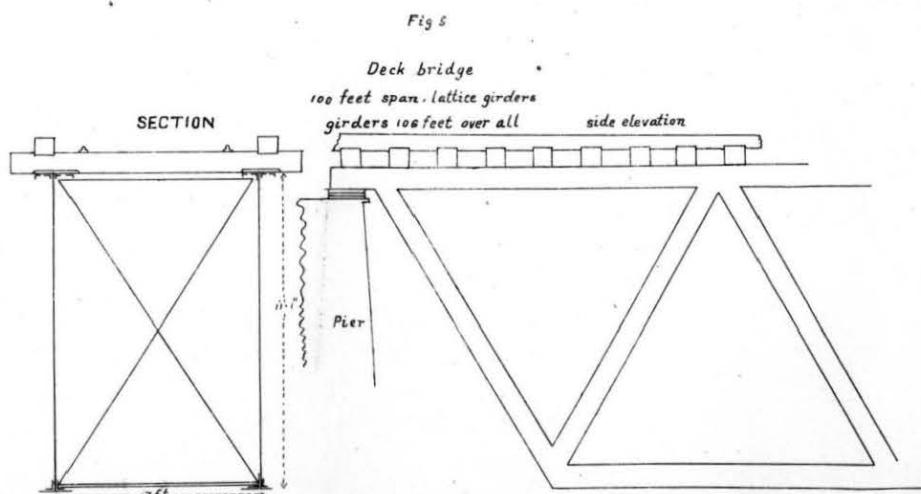
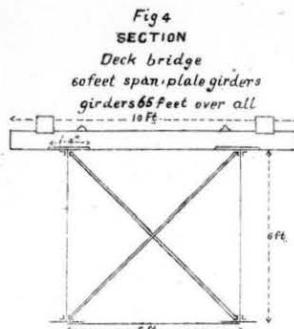
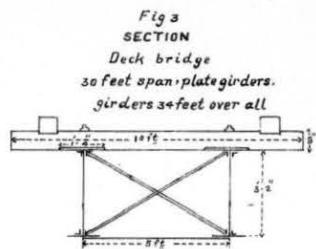
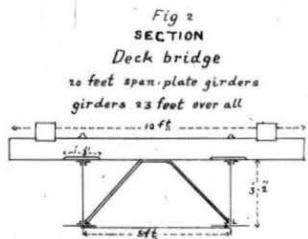
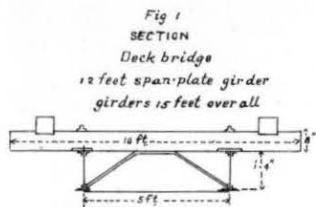
關海山

PLAN OF RAILWAY PROPERTY AT SHAN-HAI-KUAN

Scale 1 Inch to 400 Feet
Feet the Scale 0 1 2 3 4 5 6 7 8 9 10 11 12 13 14 1500 Feet

A = Locomotives water tank
B = R.S.O. Quarters
From B.R.A. plan.





Diagrams showing types of bridges (deck - through & through)
on North China Railway Peking to Shan-hai-kuan

Fig. 1
60feet span 4 deck bridge double line
SECTION

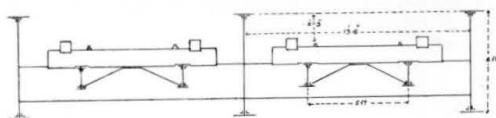


Fig. 3
200feet span through bridge single line
SECTION

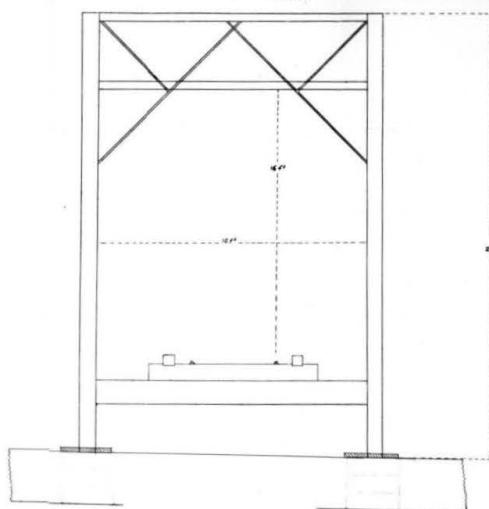
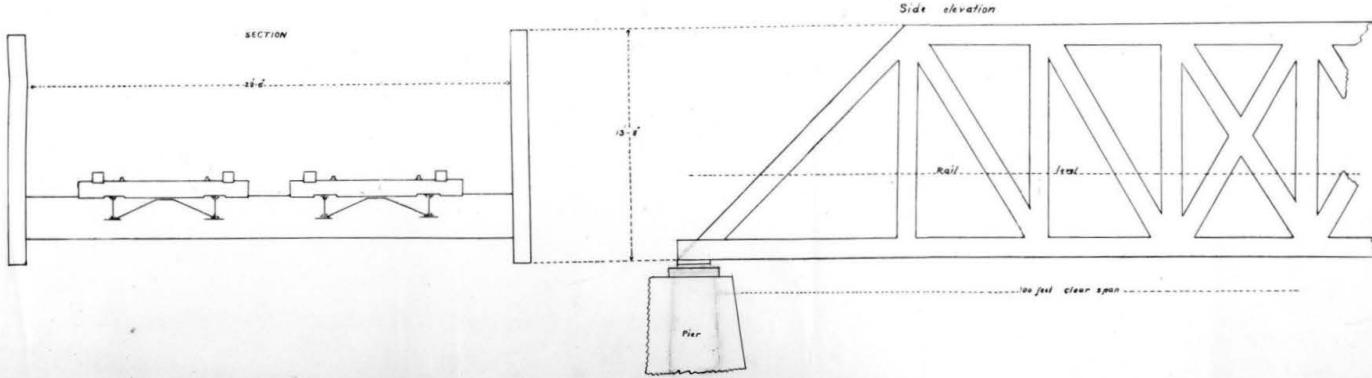


Fig. 2
100feet span 4 through bridge double line



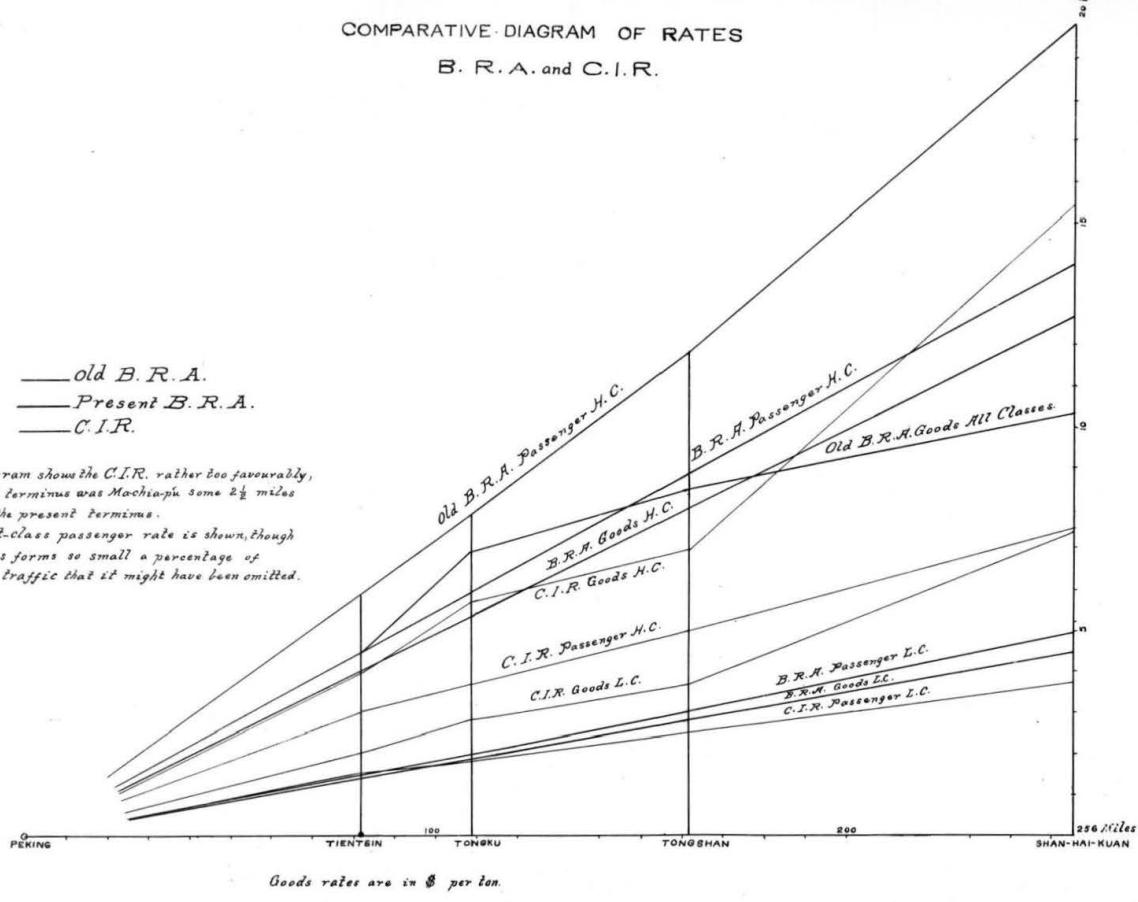
COMPARATIVE DIAGRAM OF RATES

B. R. A. and C. I. R.

— old B. R. A.
 — Present B. R. A.
 — C. I. R.

This diagram shows the C.I.R. rather less favourably, as the old terminus was Machia-pu some $2\frac{1}{2}$ miles short of the present terminus.

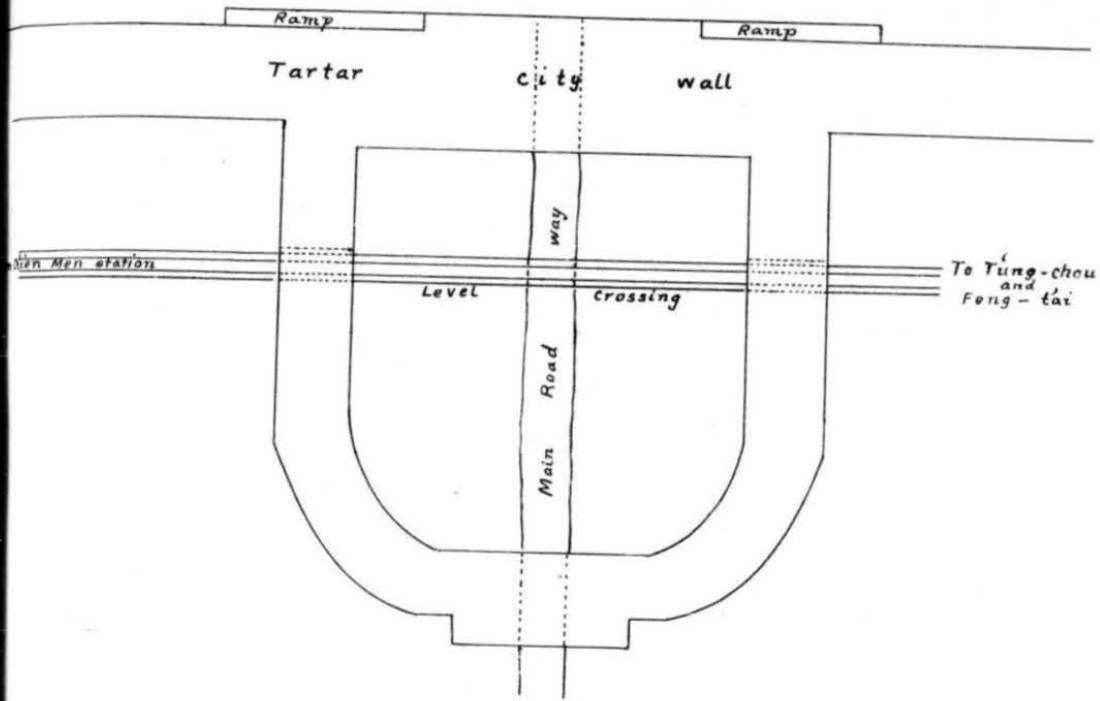
The first-class passenger rate is shown, though that class forms so small a percentage of the total traffic that it might have been omitted.



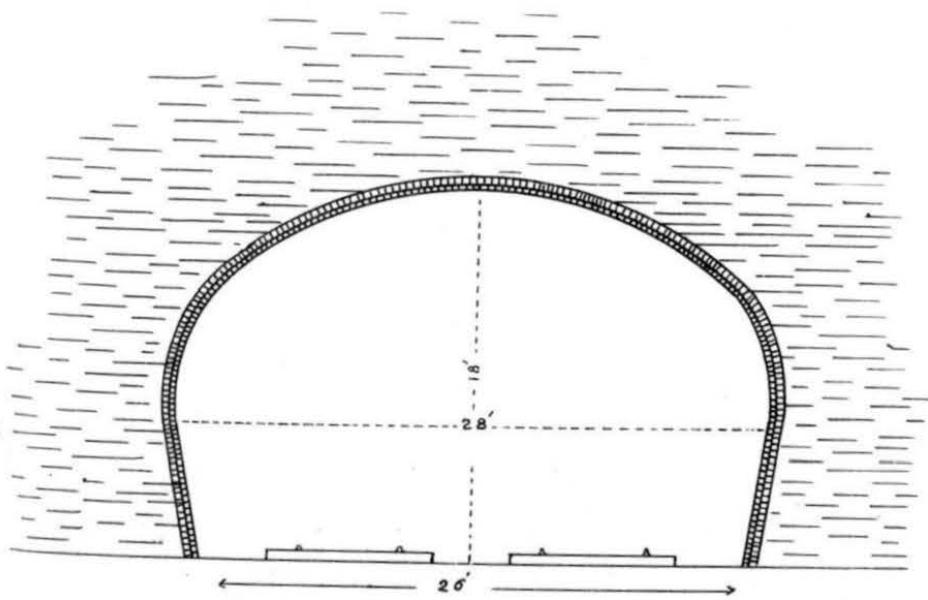
Goods rates are in \$ per ton.

85. *Wuhsien*
 13. *Yi*
 13. *Yi*

Tunnel through The
Hata Men Bastion
PLAN
Scale 120' = 1"



Front Elevation
Scale 8' = 1"



- (1) T'ANG-KU Station. Platform looking North.
- (2) T'ANG-KU Wharf.
- (3) HSIN-HO Wharf and Crane.
- (4) HSIAO-HO Wharf and Barracks.
- (5) TIEN-TSIN UP Platform looking North showing overhead bridge.
- (6) TIEN-TSIN Station looking South showing Engine shed and Water tower.
- (7) The HAI-HO at TIEN-TSIN taken from the road between the French Boat
trades and the Station.
- (8) YANG-TS'UN railway bridge from North end.
- (9) Roads near YANG-TS'UN. (August 1901).
- (10) LU-T'AI Station.
- (11) LIU-T'ANG Station.
- (12) AH-YUNG Station.
- (13) HUANG-TS'UN Station.
- (14) PENG-T'AI showing some railway material collected after Boxer troubles
1900.
- (15) Ruins of old Station MA-CHIA-P'U destroyed by Boxers 1900.
- (16) Gap in the Chinese City wall by which the line to the TEMPLE of HEAVEN
enters the Chinese City PEKING, taken from the outside showing bridge
over moat.
- (17) Seen from the inside showing Engine watering tank on the City wall.
- (18) Panoramic view of the YUNG-TIANG-MIN (TRUMPET of HEAVEN) Station.
- (19) The CH'IAN-WEN TA CH'IEH, or main North and South road PEKING looking
North taken from top of South Gate Chinese City.
- (20) CH'IAN-WEN Station and Post.

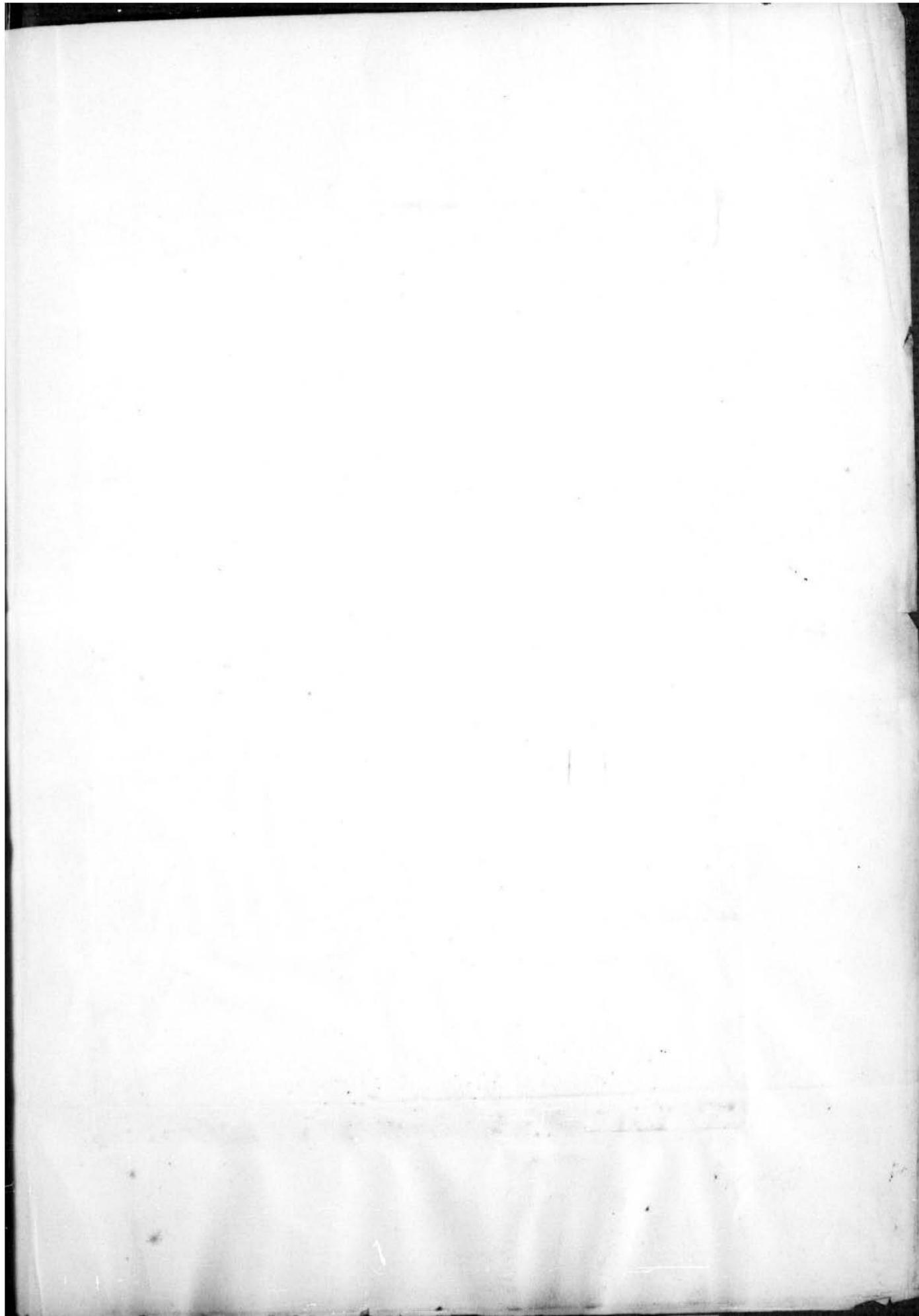
LIST OF PHOTOGRAPHS T'ANG-KU TO SHAN-HAI-KUAN SECTION.

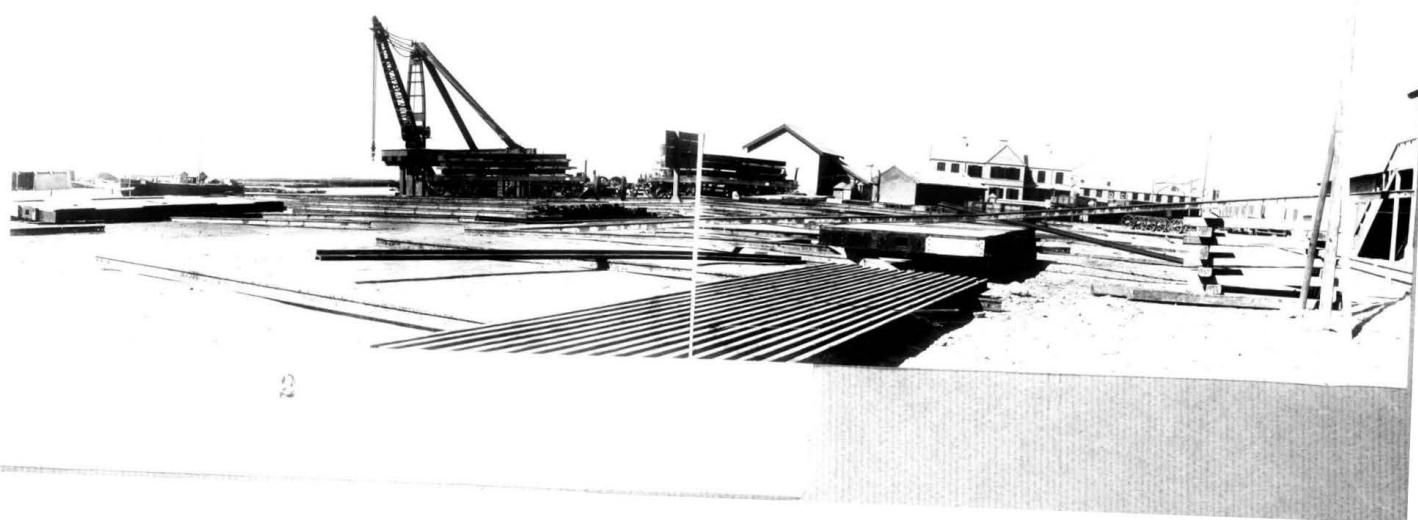
- (21) LU-T'AI PORT.
- (22) View of country round LU-T'AI, showing demolished Port.
- (23) TONG-SHAN Station.
- (24) Cantonese Barracks TONG-SHAN.
- (25) Officers Quarters TONG-SHAN.
- (26) Hospital and new workshops TONG-SHAN.
- (27) KAL-CHOU Post.
- (28) SHIH-KUAI Barracks
- (29) SHAN-HAI-KUAN Station.
- (30) North view of railway Yamen SHAN-HAI-KUAN.
- (31) English and German railway lines at SHAN-HAI-KUAN.
- (32) Railway Hotel SHAN-HAI-KUAN, occupied by Gurkhas.

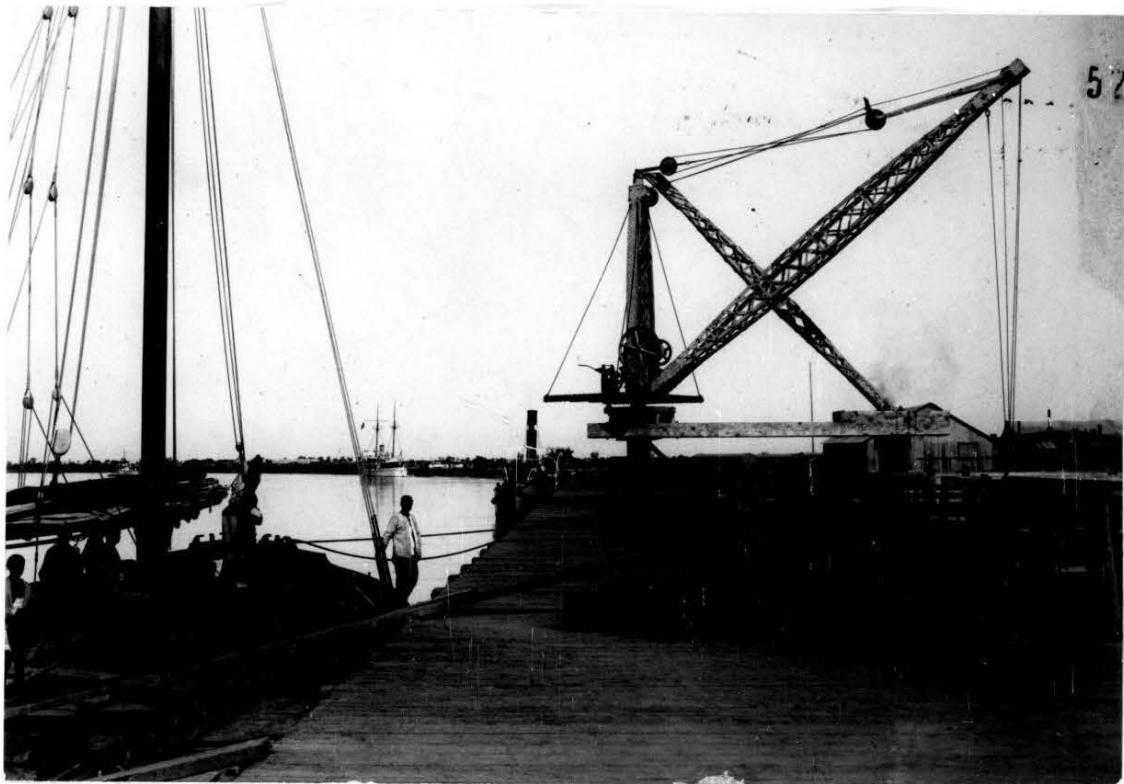
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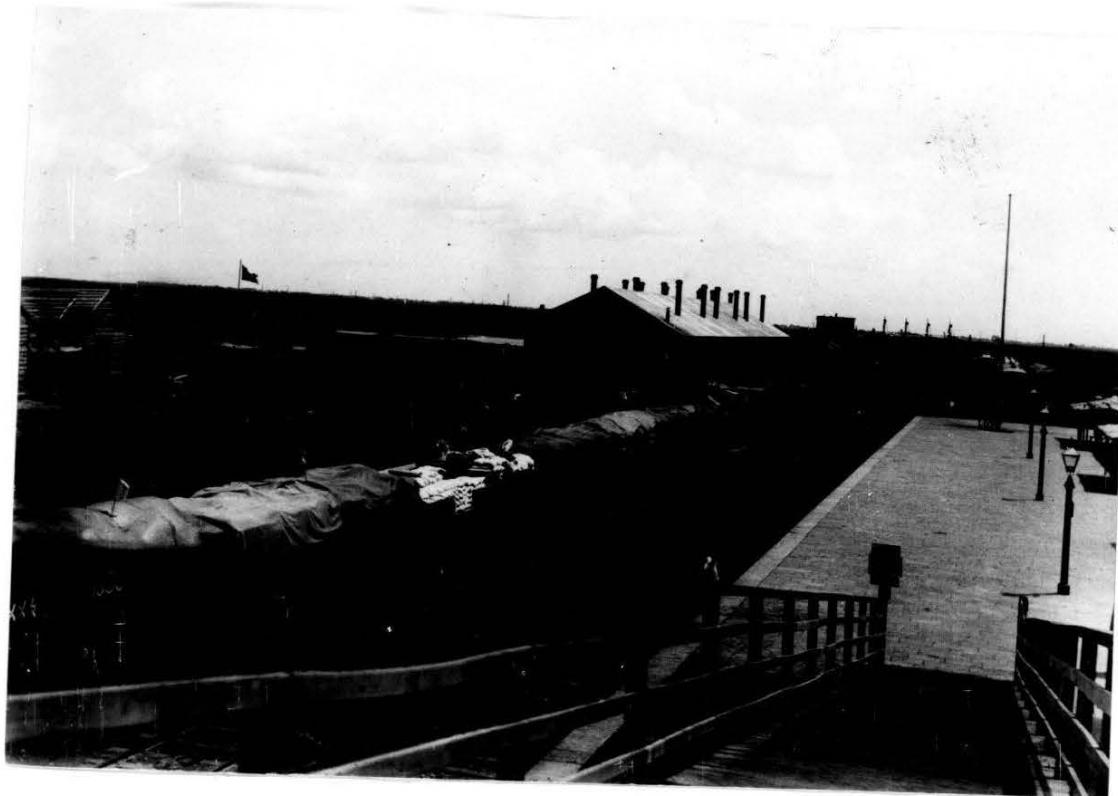
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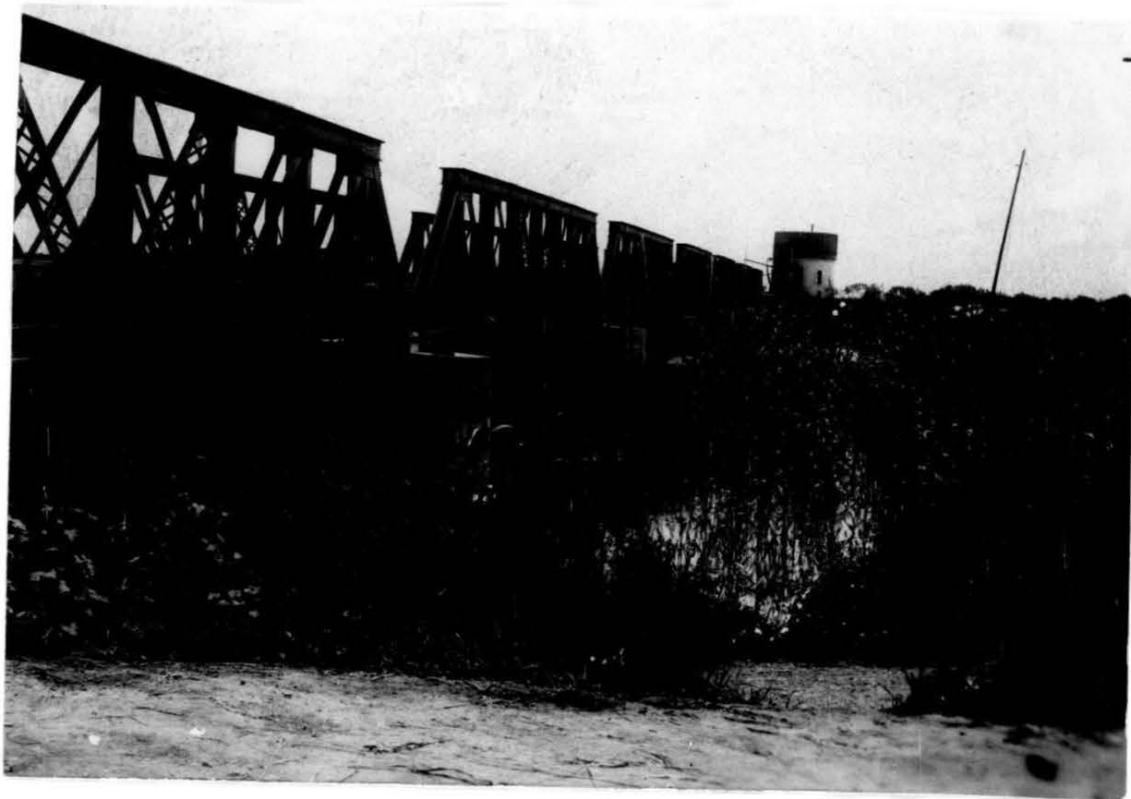


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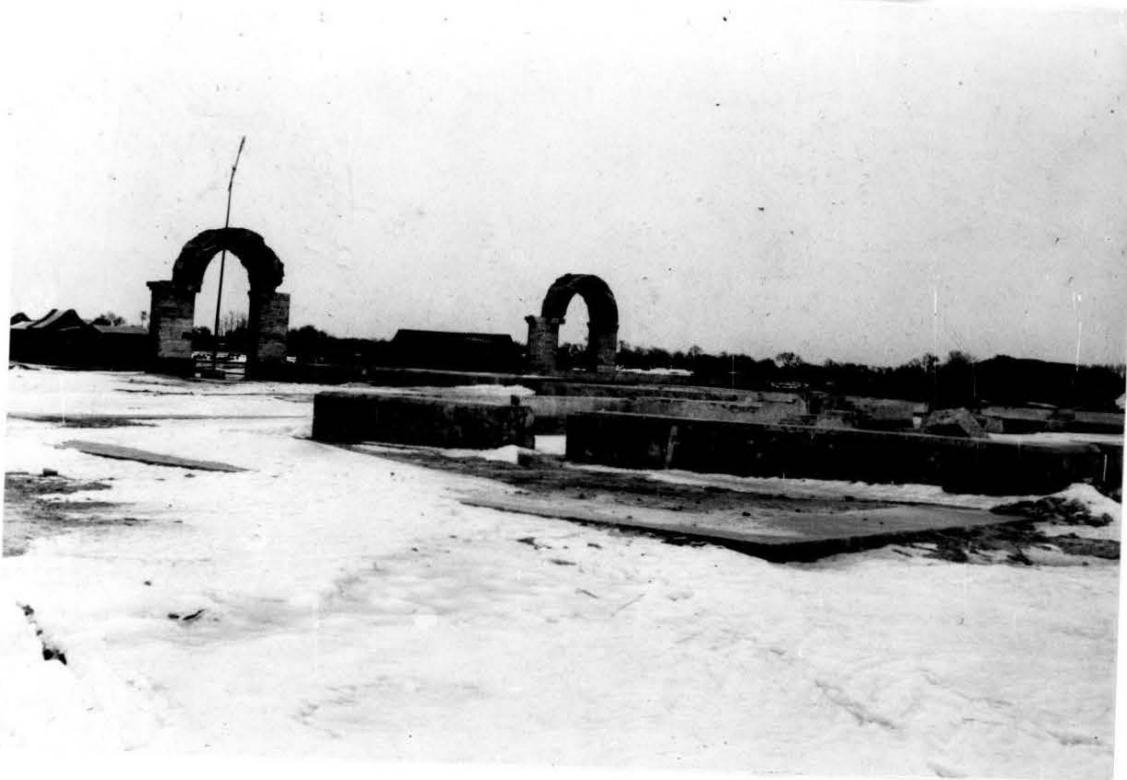


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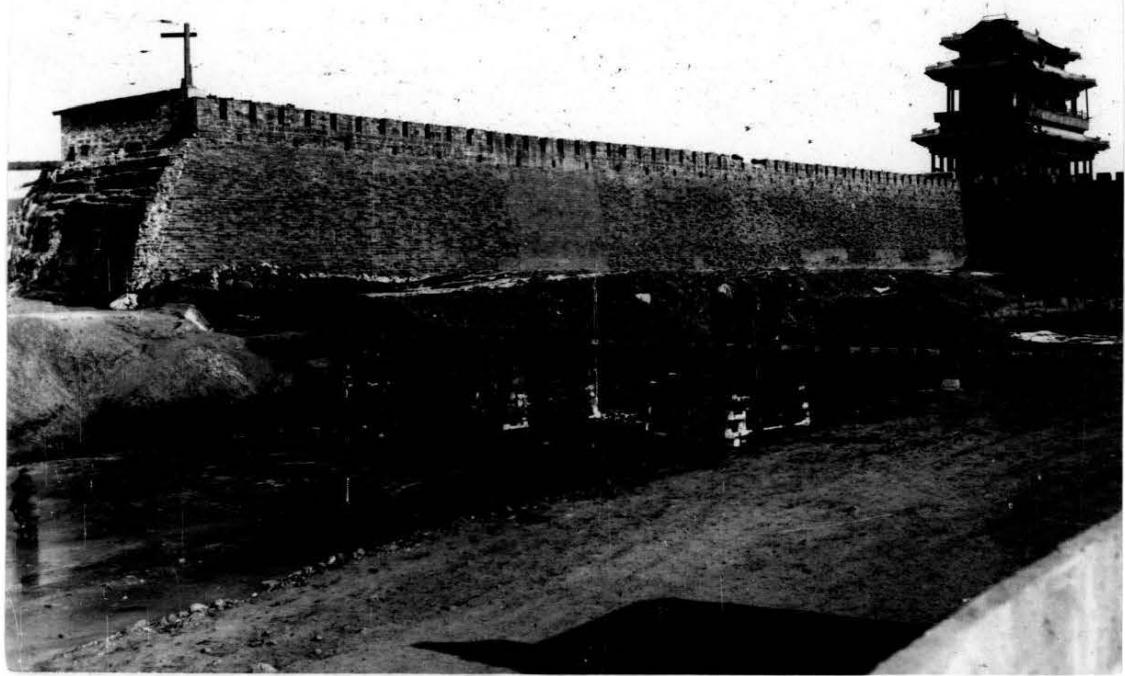


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28

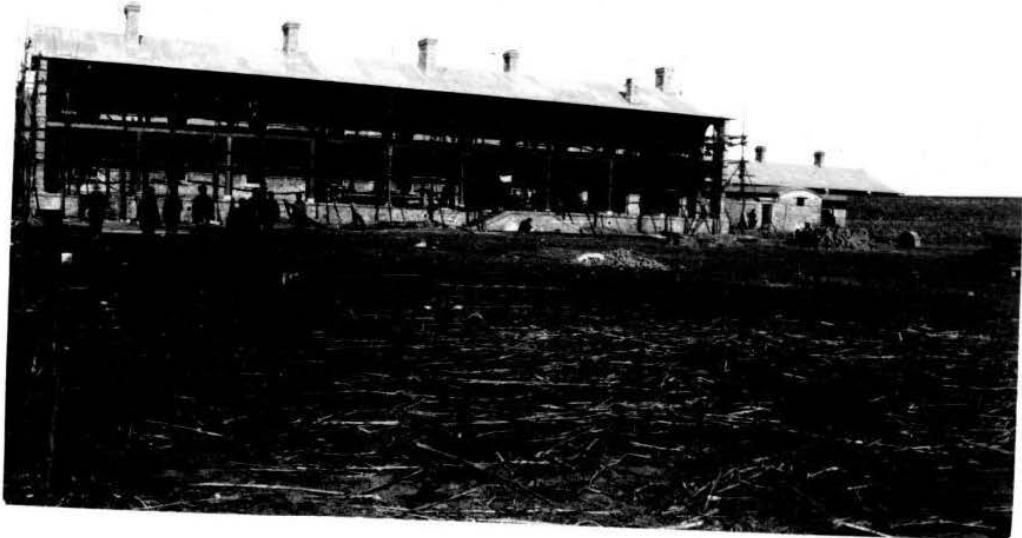


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315

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80



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Peking.

26 January, 1903.

No 38.

My Lord,

In the course of a recent correspondence with Mr Kinder, as Managing Engineer of the Imperial Northern Railways, upon the subject of a claim submitted by Messrs. Whittall and Co. for bridge material, I quoted a statement made by Sir E. Satow in a letter dated the 22nd September last to Mr Hillier, then acting as British Delegate on the Bankers' Commission at Shanghai, to the effect that certain charges should come out of the sum of £500,000 in Chinese 4% Bonds reserved to meet the Northern Railways claim.

This statement produced a letter from Mr E. Cousins, as Representative of the British and Chinese Corporation, copy of which I have the honour to enclose, stating that this was the first intimation that the bondholders have

had

The Marquess of Lansdowne K.G.

etc. etc. etc.

2181 33

Mr. Cousins
10 Jan. 1903.
Mr. Cousins
26 Jan. 1903.

had of the manner in which it is proposed to liquidate their claim, and asking that, pending the receipt of a more official communication, action in the **sense** indicated may be suspended.

In my reply to Mr Cousins, copy of which I have also the honour to enclose, I have informed him that the manner of liquidation would not appear to have been as yet finally decided upon, and I have explained why the claim was included in the British Government claim, as put before Your Lordship in Sir E. Satow's despatch No 172 of the 16th June last, whilst pointing out that it cannot be determined until the accounts come to hand what the actual amount of money due to the shareholders will be.

I have the honour to be,

With the highest respect,

My Lord,

Your Lordship's most obedient,

humble Servant,

Walter Townley

PEKING.

26 January, 1903.

Sir,

I beg to acknowledge the receipt of your letter of the 10th inst. upon the subject of the Northern Railways Indemnity claim, and a statement made in a letter to Mr. Kinder by me, to the effect that the sum of £500,000 intended to meet the claim had been reserved in Chinese 4% bonds. The above statement was taken from a letter written to Mr Hillier, as British Delegate on the Bankers' Commission at Shanghai by Sir E. Satow, on the 22nd September last, respecting Messrs J. Whittall & Co's claim.

It would not appear that any final decision has as yet been arrived at as to the manner in which the Northern Railways indemnity claim should be dealt with, nor indeed as to the actual amount of money that will be required to liquidate it.

The claim was included in the British Government claim because His Majesty's Government had advanced £90,000 on the indemnity or compensation to be received by

Edmund Cousins Esq.,

Tientsin.

the

26/1/1903 28

1870, Augt, 28

to caused party to expenses and expenditure of you I
 original intended out to Foochow and again back 1870 and
 of all required a mi also required a hospital which
 600,000 to now out their route out of you of which
 would in previous had had also some of behemoths
 and a small road was constructed across out along 24
 hours and so required about 1000 men of soldiers
 which will no, more. I will go back to the
 main out of the road, small passengers, and
 so and consider how far you can charge for this of
 and appear in return out of we to British road for
 the road of places make up which required
 in 1870 had gone to Chungking out of an hundred men
 1000000 required out of British now said out
 passengers and passengers a' (sic) all caused mis-
 deliveries of a passengers to which out to 600,000

the British and Chinese Corporation. In addition to the
 above mentioned sum, it would seem that £30,000 were
 expended on the railway during its administration by the
 British military authorities, presumably out of revenue,
 and it has been estimated that another £15,000 will be
 required to put the railway in complete order. These
 figures can of course only be verified when the accounts
 of the British Railway Administration come ~~time~~⁸ to hand,
 and the present state of the line can be accurately
 gauged.

I am transmitting a copy of your letter under reply
 to Lord Lansdowne, and will let you know in due course
 what answer I receive from His Lordship.

I am,

Sir,

Your obedient Servant,

ST 1870 p 3

Peking.

12 March. 1903.

No. 98.

Confidential.

My Lord,

I have the honour to report that the Imperial Northern Railways are constructing a branch line from a point near Teng-tai, on the Peking-Tientsin line, which runs due North, crossing the Luhun line by an elevated bridge, passes close to the Hsi Chih Men, or North-West Gate of the Tartar City, and then continues north for a mile or more, with a branch to the Summer Palace. The line will be open for general traffic, whilst it is hoped to eventually

extend

The Marquess of Lansdowne K. G.

et. et. et.

20. 1. 1903. old 6

extend it to Kalgan; it will also probably form a link in the circular line round Peking which Mr. Kinder hopes to construct some day. It would in this case connect with the branch line to Tungchow which was constructed under the direction of the B. Railway Administration.

The French and Belgian Ministers made strenuous efforts to secure the construction of this line, and of the newly laid line from a point on the Luhai line to the Western Tombs, for the Franco-Belgian enterprise, and would probably have secured the building of both lines but

for

for Viceroy Yuan Shih-k'ai's firm opposition.

I have the honour to be,
With the highest respect,
My Lord.

Your Lordship's most obedient
humble servant.

Walter Lowley

PEKING.

9 April, 1903.

My Lord,

I have the honour to transmit herewith to Your

Lordship, in original, and with reference to your Despatch
Number 42 of the 13 February last, a detailed statement
of the Northern Railways claim as furnished to me by Mr.
Cousins.

Your Lordship will perceive that the total claim
for both intra and extra mural lines amounts to £448,836-13-9
to which will have eventually to be added a sum of £4000, h
the estimated cost of the resurvey of the line which will
have to be made in order to establish the titles to lands
of which the deeds were lost in the destruction of the head
office at Tientsin. The above mentioned sum, divided I

THE

MARQUESS OF LANSDOWNE, K.G.

&c.

&c.

&c.

2 1961 23

No. 138.

*Summary of claim
of Cousins
4 Apr. 1903*

1903. March 12.
Peking. Re Turner
Re 98²
Confidential.
Rec'd 27 April. 1903 -
by us.
Northern Railway
Projected Branch to Summer
Palace, with possible extension
to Kalgan.

Mr. Kinder who is
proposed to arrange
a trial & Peking-
Mahan line, after
it is thrown down
to be submitted to
Peking. See
l

15
36

l

4

This looks like a score

for Mr. Kinder, but it may
prove convenient

opinion from Russia before
he gives his line to
Kinder.

C. S. B. & C.
Corporation

347.C.L.1

am told, as well as may be between the two lines, is made up of:-

Intra-mural line	£319,680 .18. 10
Extra-mural line	£121,655. 14. 11
Contingent fund	£ 7,500. 0. 0
	£448,835. 13. 9

I understand that a fuller and more detailed examination of the extra-mural line than Mr. Kinder had been able to make before he furnished Sir E. Satow with the information supplied in his Despatch Number 320 of the 29 October last, has caused him to place the engineering department's claim for damage to that line at £104,010. Only a small portion of this last figure covers damage to, rolling stock, and that chiefly applicable to engines, and since it was not possible to make a full separate allocation of rolling stock to the one line or the other, and where separation has not been made the rolling stock and supplies have been attributed to the intra-mural line.

Mr.

Mr. Cousins tells me that it is possible that the sum of £300,000 claimed by His Majesty's Government on account of the extra-mural line will still be sufficient to cover both the present actual claim and the amount payable by the Chinese to the Russian Government which he is given to understand amounts to about £150,000, although as far as he can ascertain no accounts have as yet been submitted by the Russian Railway Administration. I am addressing a letter on this point to the Chinese Northern Railways Administration, and shall have the honour to submit their reply to Your Lordship at a later date.

From the appended statement, it would however appear that the sum of £500,000 claimed on account of the intra-mural line should prove sufficient to cover the losses sustained on both lines, if it should be found possible to devise some means of meeting the very large discount to be encountered in negotiating Chinese Government indemnity bonds. Mr. Cousins has represented to me that the Railway Administration is in urgent need of immediate funds to enable it to carry out pressing repairs and to replenish its

its rolling stock, and that the requisite funds are only forthcoming out of the Indemnity claim, which however in the shape of bonds is not a negotiable factor except at a ruinous discount. He will write to me further upon the subject, and I will submit his communication to Your Lordship upon its receipt.

Mr Cousins points out that the sum of £30,000 referred to in Your Lordship's despatch under reply, on account of extensions to the Temple of Heaven and Peking, and from Peking to Tungchow, was specifically applied to the extensions and was drawn from loan funds, no claim on this account being made in the statement of claim, in which Your Lordship will see that the sum of £90,000 advanced by His Majesty's Government is duly included.

I have been carefully through the statement of claims with Mr Cousins and Mr Allen, and would beg to submit that I have found it, as far as I am able to judge, a reasonable one. The contingent fund of £7,500 seems to be a precautionary measure to take, and if the money is not required it will not be used.

References

References 15, 16, and 17 deal with claims on behalf of the railway staff, both those retained in China and those who have left the East. The names of all the Europeans employed on the railway at the time of the troubles figure on these lists, and where claims have been made by them the full amount of claim is given, although in most cases the claim is subject to reduction. The large majority of those employed by the railway who have left the East have made no claim at all, so that it is possible that the sum of close upon £9000 at which the amount of possible compensation has been fixed may be somewhat reduced. A notification will be issued shortly in the London "Times" that all claimants of this class must make application for payments due to them before a certain date, probably six months after publication of the notice. It is clear that without such public announcement the Railway Administration could not be sure for an indefinite time of the full amount of its liabilities.

I should be glad to be furnished with a copy of the statement

3 - 1961 Z³

statement of claims if the document is printed, as press of
work has prevented a copy being made here.

I have the honour to be,

With the highest respect,

My Lord,

Your Lordship's most obedient,

humble Servant,

Walter Sowley

P.S. Since writing the above I have received an
amended statement from Mr Cousins which includes the £4000
on account of resurvey, together with a memorandum explaining
the necessity for the same. In transmitting these documents
to Your Lordship I have also the honour to enclose copy of
a letter just received from Mr Cousins covering the Railway
claim, pointing out that the Railway Administration is seriously
hampered by want of funds, and expressing a hope that His
Majesty's Government will come to an early decision as to
when and how the final claim allowed will be made available,
and in what amounts.

W. J.

Peking. Apr. 9.

Mr. Townley

No 138

(ref. F.O. No 42 of Feb. 13)

Recd. 23 May 1905 - 2 inclos.
by bag.

Indemnity: Northern Railways

claim

& detailed statement

Yours
Princ. (China)

And consider the

print:

Feb 6

442 Ch. W.

	Description	Endeavour		Cutterdale		Total	
		L	s. d.	L	s. d.	L	s. d.
1	General Matheson & Co Asper Statement £ 10,517 16.5 and Bills 22,057.09 2 50 = £ 32,165 6.0	5117	2 5			5117	2 5
2	Thomas Iron Works Interest for 3 Years from 1 st July 1900 to 1 st July 1903 on their 9%. £ 3,832.91 2 35% per Annun (for material landed at Tidewaters)			402	8 0	402	8 0
3	Taun Wan & Co Interest on their 9%	175	19 10			175	19 10
4	John Birch & Co Interest on their 9% for £2,500 for stores also for tenders stored in England for 3 Years from 1 st July 1900 to 1 st July 1903 2 35% per Annun	262	10 0			262	10 0
5	Harkins & Co For Boiler Tubes & Lining materials £ 6 Chang woo	756	13 4			756	13 4
6	American Tractingle Co For 100 Cases Window Glass	67	5 9			67	5 9
7	Amidale Harbour & Co For 1291 tons firebricks coal delivered at Lyngpoor			1067	4 7	1067	4 7
8	Chang Shan Bank For 3 Years Interest 2 7% per Annun from 1 st July 1900 to 1 st July 1903 on Bills 5,000.00 - Bills 31,647.70 2 7% 2 7% £ 1			452	0 0	452	0 0
9	For Sung Heng Bank For 3 Years Interest 2 7% per Annun from 1 st July 1900 to 1 st July 1903 on \$ 8,000.00 - Interest \$ 1,680.00 2 7% £ 1			148	0 0	148	0 0
10	Freight & Store & Wharf Charge on cargo stored in Shanghai & brought to Amurho in December 1902 by S.S. Ningpo & Changzhou			4350	14 5	4350	14 5
	Summary of Return of Engineering department as per Mr. Kinders Statement	209161	0 0	104010	0 0	313171	0 0
	Amount advanced by British Government for replacement & renewal caused by Boxer Demise	90,000	0 0			90,000	0 0
	Losses at Head Office, Tientsin	6414	0 0			6414	0 0
	Miscellaneous Claims as per Statement	618	11 6	470	0 0	10 88	11 6
	Empanelment to Staff, retained by B.R.A.	3173	0 0	789	0 0	3962	0 0
	Staff remaining in the East, but not employed by B.R.A	511	0 0	178	0 0	689	0 0
	Staff which left the East, services not being required	2719	16 0	808	6 6	3528	2 6
	Wages & Compensation to coolies accompanying Labour Expedition \$ 40,017.98	402	0 0			402	0 0
	Contingent Fund	7500	0 0			7500	0 0
	British & Chinese Corporations claim for losses at Hankow			8980	1 5	8980	1 5
	Estimated Cost of survey of Railway Lands	7500	0 0	319680	18 10	121655	14 11
						452886	13 9

J. Marshall.

Secretary, Imperial Railway of North China.

(Signature)
Tientsin 17th December 1902.

M. T. Liang Esq.

Director Imperial Chinese Railways.

Present.

Dear Sir,

Railway Indemnity Claims.

Referring to our communication of 1st Ultimo and documents attached we now beg to hand you herewith an amended Statement of Telegraphic Expenditure which please substitute for the one rendered.

Our claims are now summarised as follows:-

Amounts due for Material lost through the Boxer troubles in 1900 together with interest thereon as per separate statement

Taels. 20344.80

Expenditure on telegrams as per Statements attached

£ 1,951.16.5 and 1712.90

British & Chinese Corporation's Claim for losses at Nampiao less amount allowed by H.B.M. Claims

Commissioner £ 8980. 1.5

£ 10981.17.0 and Taels. 22057.70

We are, Dear Sir,

Yours faithfully,

(Signed) JARDINE, MATHESON & CO.

W.S. Johnson

7-1961 Z 3

6 1961 Z 3

95

Summary of Amounts due by the Imperial Chinese Railways to the undersigned
on account of Material imported under their Indents and lost in consequence
of the Boxer troubles of 1900- also of Interest as per Statements attached.

A/c	Indent	No.	Tls:	
"	38		4457.31	18. 1
"	38		694.17	
"	1605 (part)		1612.27	
"	1605/1606		5174.39	
"	1610		336.56	
"	1676			
	£ 619.14.9 at 2/4 $\frac{7}{16}$		5230.31	
	Various			
	Delivered to S.R.A.		2637.77	
	Total Tls: Taeles		20344.80	

E. & O. E.

Tientsin 1st November 1902.

JARDINE, MATHESON & CO.

Jardine

Tientsin 1st November 1902.

JARDINE, MATHESON & CO.

Jardine

5 cases Split & Cotter pins	£ 47. 7. -
58 cases Iron Rivets	232. 5. -
3 cases Brass Sheets	
3 " Copper "	
2 " " Tubes	
1 " " & Brass Tubes	379.12. 1
Freight & B/Lading	£ 38. - 4
Insurance on £ 770 at 12/6 & Stamp	* 4.18. 3
Bill Stamp	* 7. - 43. 5. 7
at Ex: 2/4 3/8	
plus interest from 10.4.00 to 31-12.02 =	Tls: 5941.71
995 days at 7 o/o per annum	* 1133.61
	Tls: 7075.52

Loss amounts received from the B.R.Administration
for portions of above Material taken over by them }
plus interest calculated thereon as per memo:at foot } 2618.21
Balance due by the Imperial Chinese Railways Taeis 4457.31

--- Memo.---

1901.			
October	Amount received from B.R.A.	Tls. 1339.24	
	add- Interest from 1.11.01		
1902	to 31.12.02 = 14 months & 7 o/o	109.37	Tls: 1448.61
April	Ditto - Ditto -	Tls. 1117.45	
	Add- Interest from 1.5.02		
	to 31.12.02 = 8 months at 7 o/o	52.15	1169.60
			Taeis 2618.21

— E. & O. E. —
Tientsin 1st November 1902.

JARDINE, MATHESON & CO.

Ed. Currier

one dozen & 12 lbs each
each dozen Kips
each dozen 10 lbs
each dozen 2 lbs
each dozen 1 lb
each dozen 1/2 lb
each dozen 1/4 lb

each dozen 1/8 lb
each dozen 1/16 lb
each dozen 1/32 lb

each dozen 1/64 lb
each dozen 1/128 lb
each dozen 1/256 lb

each dozen 1/512 lb
each dozen 1/1024 lb

each dozen 1/2048 lb
each dozen 1/4096 lb

each dozen 1/8192 lb
each dozen 1/16384 lb

each dozen 1/32768 lb
each dozen 1/65536 lb

each dozen 1/131072 lb
each dozen 1/262144 lb

each dozen 1/524288 lb
each dozen 1/1048576 lb

each dozen 1/2097152 lb
each dozen 1/4194304 lb

each dozen 1/8388608 lb
each dozen 1/16777216 lb

each dozen 1/33554432 lb
each dozen 1/67108864 lb

each dozen 1/134317632 lb
each dozen 1/268635264 lb

each dozen 1/525905792 lb
each dozen 1/1051811584 lb

each dozen 1/2103623168 lb
each dozen 1/4207246336 lb

each dozen 1/8414492672 lb
each dozen 1/16828985344 lb

each dozen 1/33685985344 lb
each dozen 1/67371970688 lb

each dozen 1/13474391136 lb
each dozen 1/26948782272 lb

each dozen 1/52321956544 lb
each dozen 1/104643913088 lb

each dozen 1/209287826176 lb
each dozen 1/418575652352 lb

each dozen 1/837143304704 lb
each dozen 1/167428660944 lb

each dozen 1/209857321888 lb
each dozen 1/419714643776 lb

each dozen 1/839535287552 lb
each dozen 1/167907057504 lb

each dozen 1/209914515008 lb
each dozen 1/419829030016 lb

each dozen 1/839858060032 lb
each dozen 1/1679716120064 lb

each dozen 1/2099890240128 lb
each dozen 1/4199780480256 lb

each dozen 1/8400000000000 lb
each dozen 1/1680000000000 lb

each dozen 1/2090000000000 lb
each dozen 1/4180000000000 lb

each dozen 1/8399999999999 lb
each dozen 1/1679999999999 lb

each dozen 1/2099999999999 lb
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each dozen 1/1679999999999 lb

each dozen 1/2099999999999 lb
each dozen 1/4199999999999 lb

each dozen 1/8399999999999 lb
each dozen 1/1679999999999 lb

each dozen 1/2099999999999 lb
each dozen 1/4199999999999 lb

I.C.R. Indent No. 1600 (part) London Invoice No. 99 due 19th April 1900.

I.C.R. Indent No. 38 London Invoice 99 due 19th July 1900.

2 Cases Crucibles	£ 32.16. 3
2 " Saws & Cutters	" 56.11. 0
2 " Vices	" 1.16. 0
1 " Ratchet Braces	" 64. 3. 5
2 " Jacks	" 53.16. 0
1 " Letter Stamps	" 1. 1. 0
Bulls Eyes	" 7. 8. 1
Freight & R/Lading	£ 11.17.3
Insurance on £ 240	" 1.10.6
Bill Stamp	" 2.0
	" 13. 9. 9
	£ 222. 4. 1

at Ex: 2/4 - $\frac{3}{8}$ = Tls: 1879.44
Plus Interest from 19.7.00 to 31.12.02 = 895 days at 7 o/o " 322.42
Tls: 2201.86

Less Amounts received from the B.R.A. for portions of above taken over by them and Interest calculated thereon as per Memo at foot

Balance due by Imperial Chinese Railways Tls: 694.17

-- Memo --

March Amount received from B.R.A.	Tls: 1407.20
Add- Interest from 1.4.02 to 31.12.02	
= 9 months at 7 o/o	73.88 Tls: 1481.08
Just Ditto Ditto	Tls: 26.-
Add Interest for 4 months	" .61 " 26.61
	Taels 1507.69

E. & O. E.

Tientsin 1st November 1902.

JARDINE, MATHESON & CO.

per G. S. S.

18 Cases Bolt Ends	£ 96.19. 0
16 * Weighing Machines	* 97. 6. 3
17 Bars Angle Steel	* 27.19. 1
18 Cases Galva Iron	* 401. 6. 2
19 Kegs Brown Paint Oak	
20 Drums drying Oil thinning	
21 --- Turpentine	
22 Case Chalk	105.-. 8
23 Pump Leathers	2. 3. 0
24 Padlocks & Snaps	
25 Sledge Hammers	11. 1.10
26 Bundles Miners Drill Steel	37.17. 2
Freight & H/Lading	£ 102. 4.10
Insurances &c	* 6.12. 9
	<u>108.17. 7</u>
Ex: 2/4 $\frac{3}{8}$ =	<u>£ 886. 0. 9</u>
Less Interest from 10.4.00 to 31.12.02=995 at 7 %	Tls: 7494.24
	* 1450.06
	Tls: 6924.30

Less amounts received from B.R.A. for portions of above material taken over by them and interest calculated thereon as per memo: at foot	{	
Balance due by the Imperial Chinese Railways		Tls: 5174.39

301	Memo:	
301	Amount received from B.R.A.	Tls: 356.56
	Interest for 15 months from	
302	1/12/01 to 31/12/02 at 7 %	* 27.05
	Ditto Ditto	Tls: 3216.19
	& Interest for 6 months from	
	1.5.02 to 31.12.02 at 7 %	* 150.09
		3366.26
		Taels 3749.91

E. & O. E.

Tientsin 1st November 1902.

JARDINE, MATHESON & CO.

Per *S. J. Dawson* 12 - 1961 - 23

I.C.R. Indent No.1610 London Invoice No.89 due 10th April 1900.

2 Cases Drawing Material		£ 60. 5. 1
Freight & B/Lading	£ 1.15.11	
Insurance on £ 70 & Stamp	9	
Bill Stamp	1	* 2. 3.11
		£ 62. 9.00
at Ex: 2/4 3/8 =		Taels 528.21
Plus Interest from 10.4.00 to 31.12.02		
= 995 days at 7 o/o		* 100.79
		Taels 629.00
Less amount received from B.R.A. as per memo:at foot		* 290.42
Balance due by the I.C.Railways		Tls: 338.58

1902

--- Memo. ---

April Amount received for part of above from
the B.R.A. Tls: 277.47

Add- Interest from 1.5.02
to 31.12.02 = 8 months at 7 o/o * 12.95 Tls: 290.42

E. & O. E.

Tientsin 1st November 1902.

JARDINE, MATHESON & CO.

per Sa. Cawing

13 - 1961

1902 Drayton 20/6/01 20/9/01 account period 01/10/01 Madras

Copy

Imperial Railways of North China.

to the undersigned

Dr.

For amount due on account of 60 lbs Rails delivered ex "Forestdale" and
 "Macedonia" at Sin-Ho 20/6/01 under Indent No.1576 & Contract of 21/9/99.vi
 4701 Rails = Tons 1252. 1. 3.17 at £ 7.16. 1½ = £ 9772.17. 3
 Less Sales 3600 -- 696. 8. 2. 9 * 5436.15. 5
2101 Tons 555.13. 1. 9 X £ 4337. 1.10

Plus interest at 7% per annum as follows:-

on £ 2195. 4. 4 from 20/6/01 to 20/9/01 = 3 months £ 38.8. 4

3240.11. 1	*	--	20/2/02	8	--	151.4. 6
<u>4337. 1.10</u>	*	--	20/11/02	17	--	<u>430.1.11</u> X <u>619.14. 9</u>
<u>£ 9772.17. 3</u>						<u>£ 4956.16. 7</u>

at Ex: 2/4 $\frac{7}{16}$ = Tael 41650.38

E. & O. E.

Tientsin 8th October 1902.

JARDINE, MATHESON & CO.

*S. Jardine**London*

Outstanding £ 619.14.9 =

Tael 5230.31

14-1061 Z?

No. of Pkgs	Material	Vessel	Am't of Invoice	Due date	When paid	Period	Interest
17	Rivets	" Ulysses	788.47	9/5/00	31/10/01	541 days	£ 81.81
34	"	" 1807.88	10/4/00	31/10/01	570	"	142.97
1	Belt Ends	"					
1	Sledge Hammers	de.	145.16	29/8/00	29/11/01	458	" 12.75
3	Zinc Slates	Marburg	178.94	14/6/00	11/8/02	636	" 21.83
4	Vices	Ulysses					
1	Steel Springs	"	652.63	29/8/00	11/8/02	560	" 70.09
34	Vices	Machaon					
1	Ratchet Braces	"	977.81	19/7/00	11/8/02	601	" 112.70
2	Belting	"	908.29	14/7/00	11/8/02	606	" 105.56
217	Shovels Picks etc	Ulysses	3222.40	29/8/00	11/8/02	588	" 363.38
11	Weighing Machines	Machaon	1065.80	19/7/00	11/8/02	601	" 122.79
1	Crucibles	"					
1	Saws & Cut	"	375.11	19/7/00	11/8/02	601	" 43.24
192	Sundry	Ulysses					
	Material	Phryrus	3428.07	10/4/00	12/6/02	763	" 501.63
2	Drawing Material	"	269.89	10/4/00	12/6/02	763	" 39.42
25	Rivets	Pyrrhus	1084.90	10/4/00	12/6/02	763	" 158.75
3	Domes	Ulysses	1041.06	9/4/00	12/6/02	764	" 152.54
176	Bars Iron	Stentor	622.60	25/6/00	25/6/02	730	" 87.16
2	Seap	Pyrrhus	69.81	9/5/00	12/6/02	826	" 11.06
137	Tank Plate	Ulysses	1908.17	9/4/00	12/6/02	856	" 313.25
1342	Bars Iron	Pyrrhus	8124.83	9/5/00	12/6/02	826	" 495.
1	Letter Stamps	Machaon	12.56	19/7/00	12/6/02	765	" " 1.84

Tls: 2837.77

E. & O. E.

Tientsin 1st November 1902.

JARDINE, MATHER & CO.

Ed. Jardine

15-1961 Z 3

02

Ref No. 2

Thank You Work

To interest for three years
from 1st July 1900 to 1st July 1903
on their account £3882 9. 11
@ 3½ per ann., for materials
so deducted at 1st July 1900.

✓ 402. 8. 0

(signed) *Theresa Wark*

Ref. No. 3

Fearon Daniel & Co.

To D. L. Terrell on their account
from 5th April 1901, for
67 packages Steel bars,
angle, plate and rivet
shipped at Shiro.

£175. 19. 10

Ref No 4.

John Birch & Co Lt

To Interest on their account
for £2500, for Wheels and
Axles for Ladders, stored
in England, for 3 years from
1st July 1900 to 1st July 1903
@ 5% per annum.

£ 262. 10. 0

Ref No. 5

105

Mackenzie & Co

For Boiler tubes, & boiler covering
sc. £1.10.0, on account
of 2nd Jl 1899.
899 packages.

L 756 13 4

Ref 106 106 9
Clients 4th December 1899.
Copy.
The Imperial Chinese Railways of North China.
Dr to The American Trading Co.
100 cases 16 X 12 Window glass at 4-85-- Tls:485-0
The American Trading Co.
(Signed)
Tael 485.00
£69. 5. 9.
Agent.
19X-1961 Z3

Ref No 7.

Copy
Arnold Rabbe & Co.Contract 15th May 1900.1291 tons Japanese Coal
delivered alongside Railway
Wharf Jigkow @ \$7.85

per ton \$1034.35

For period 15th June to 25th

Sept 1901, 102 days @ 6% 169.92

\$10304.27

Oct 755 Fuel 7470.60

Oct 767-2 1067.47

Ref No 8

Chin Shan Bank

To 3 years 2 months @ 7.7%
per ann from 1st July
1900 to 1st July 1903 on
Tael 15.070.-

R 3164 70
⑧ 767 £ 452 0.0

Ref No. 9.

To Fung Shing Bank

To Interest @ 7% per annum
from 1st July 1900 to 1st
July 1903 on \$8000 = \$1480 00

@ \$10 - £ 148. 0 . 0

109A
Voucher No. 126

Tientsin 80th December 1902.

Imperial Railways of North China
Dr to Bucknister & Co. Ltd. Tientsin

For the following D/c:

Charges on Railway Material shipped
from Shanghai to Tientsin per S/S.
Ningpo & Changchow.

Charges in Shanghai as per Statement No. 1	£ 26,291.06
do do - 2	. 9,572.62

Charges in Tientsin:

Telegraph & Lighter Co. Voucher No.	\$	
10	3050.96	
do 11	532.90	
Interest Dec 18th to 80th. 13 days		9.60. 95
Telegrams & Postages to Ningpo	75.00	
do to Changchow	.	
Comshaw to Captain of Ningpo	15.10	
9/8.150. @ 101/4%	. 147.78	

Commission 1 1/4%	\$ 3657.75	£ 36,026.56
.	54.87	540.39

\$ 3712.62	£ 36,666.95
------------	-------------

Rate of Exchange 7/4 - \$1 = 27 2/8.

Received payment
(S/P) Bucknister & Co. Ltd.
31. Dec 1902

294 1.6
4056.72 11
4350.14.5

23 - 1461 23

101
Dr. M. & S.

copy of all the bills

will be sent to you by express

bills etc. Dr. D. & S.

will remain at no

charge initially until we receive
2/3 of the bill of exchange
and general & agent's

Dr. D. & S.
20/11/02

1. Dr. D. & S. will remain at no
charge initially until we receive
2/3 of the bill of exchange
and general & agent's

20/11/02
09/12/02
22/12/02
03/12/02
07/12/02

2. Dr. D. & S. will remain at no
charge initially until we receive
2/3 of the bill of exchange
and general & agent's

01/12/02

24/12/02

Dr. D. & S.
20/11/02
09/12/02
22/12/02
03/12/02
07/12/02

Remainder
Dr. D. & S.
copy coll. at

Dr. D. & S.	£	Dr. D. & S.	£
20/11/02	. 18 43	20/11/02	. 18 43
22/12/02	. 18 43	22/12/02	. 18 43
03/12/02	. 18 43	03/12/02	. 18 43
07/12/02	. 18 43	07/12/02	. 18 43

copy

101

Voucher No. 126

110

Tientsin 30th December 1902

Imperial Railways of North China

Dr. to Buchalter & Co. Ltd.

For the following Viz:

Charges on Railway Material forwarded

for Ningpo to Hsinchu Wharf

Storage Shanghai Hongkew Wharf Co.

as per Voucher No. 1

£ 9147.60

. 2 273.90

. 3 845.90

. 4 6419.40

. 5 626.00

. 6 633.90

. 7 260.79

#350.17 @ 74.4125.

Freight on account from 11th Nov. to 26th.

Nov. Approximate arrival of Steamer at Taku
#10000-- at 74.4125.

Balance of freight to be settled on Completion
of discharge, Voucher No. 8.

Marine Insurance £30000--

#266666.66 @ 1/4%, Voucher No. 9
Shanghai Telegrams #52.48. @ 74.4125.

Bill Brokerage 1/8%

@ 1/8% IIIa.
26685.43.
26291.06.

Received payment
(Sd) Buchalter & Co. Ltd.

24 - 1902 - 3

Ref. No. 11
Imperial Chinese Railway

110A

Summary of Claim for Indemnity
Engineering Department

	L	p	d
do B.	45,900	0	0
do C.	144,380	0	0
do D.	7,480	0	0
do E.	61,333	0	0
do F.	11,150	0	0
	42,928	0	0
	L 313,171	0	0

(Signed) C. W. Kinder
Feb 9th 1903

Note. This claim does not include

- (1) Compensation to Staff
- (2) Expenses of Head Office, etc.
- (3) Cost of storage & shipping material from Chungtien to Shanghai
- (4) Interest payable on debts.

Indemnity claim for loss to cars
Loco. stores & a shop.

Valuation of Rolling Stock on
June 1900, see attached report
6 Mixed Cars, I.C.R type
200 German do "

£ 345.000	0 0
2.040	0 0
8.000	0 0

Total £ 355.040 0 0

Re-valuation of same in October
1902, see attached report.
59 fluid cars, I.C.R type
92 German do "

374.300	0 0
2.360	0 0
3.680	0 0

Total £ 380.340 0 0

Increased value £ 33.800.

Cost of repairing & rebuilding
500 cars used by Russians
@ £30. each.

£15,000 0 0

do rebuilding 8 locomotives
destroyed by Boche P.R. line.
Repairs and rebuilding 26
Locomotives partly destroyed
by Russians O.W.J.
Loss of materials for Car & Loco
building at Tomsk (see
attached Schedule X) (page 10)

9,600 0 0

10,400 0 0

44,700 0 0

£ 19,700 0 0

Claim amount as above £ 19,700 0 0
Less, increase in value of
stock due to building ops
during B.R.L.

33,800 0 0

Balance due £

45,900 0 0

11/3 Schedule B.

113

Indemnity claim for losses & damage to
Tientsin Peking Railway

80 Miles of 2nd track			
Rails 12,000 tons @ £1	£ 84,000 0 0		
Sleepers	30,000 0 0		
Ballast	12,000 0 0		
Plates	3,000 0 0		
Port to Crossing	1,000 0 0		
		Track	
Cost of removal & repairing of			
girders, both tracks	2,700 0 0		
Floors of same. do do	1,400 0 0		
Demolition of Tangtai Slope.	1,200 0 0		
Stores lost Peeri do	4,500 0 0		
Pl. 1 do do	4,500 0 0		
Water pipes, pipe etc	350 0 0		
3 iron boulders & tracks	3,000 0 0		
Losses of sidings & crossings	1,000 0 0		
Locomotive stores	200 0 0		
Extra length of sheds (approx)	250 0 0		
Repairs, renewals of stations	2,000 0 0		
M.C.P Station & yard	7,000 0 0		
Mechanist Hqrs Peeri	4,000 0 0		
Main line signal	2,000 0 0		
Electric tramway M.C.P.	20,000 0 0		
Main road & steam Roller.	2,000 0 0		
			£ 186,100 0 0

Value of works carried out by B.R.A.
for Capital &c etc.

Chas. Mar. Extension (value of rail included)	£ 32,000 0 0
12 Miles of Rail left in stock	9,720 0 0
	£ 41,720 0 0

Clearing & pa sheet 1.	£ 186,100 0 0
Less. Value of work done by B.R.A. on Extensions.	41,700 0 0
	£ 144,380 0 0

M. Tucker Report as a
whole \$193,658.00
Beta new iron for
roof. \$ 3,812.00

M. Marshall's statement
as to timber at Hirsch
in 1900.

1900 value \$181,200.
1902 " 16,000.
Loss. 12,200 - 12,200.00
\$ 209,770.00 \$ 209,770.00

Sale of 75 ft rail to Baling
Railway \$120,000. - \$120,000.00

Bal. due \$89,770.00

④ \$12 = £1. = £7480. -

Indemnity claim for losses incurred
Outside Wall.

Damage as per Mr Cox's Report and Estimate	\$768.530.00
Fee for Bleke Crusher	2,500.00
Rebuilding of defective piers of bridge	3,500.00
Repairing of Turntable Y.K.	1,000.00
	\$768.530.00
	\$768.530.00

Estimated value of work
done by Russians for Capital
account of Railway.

155.194.00

Balance due

\$613.336.00

② \$10 = £ 613.333.0.0

Indemnity claim for injury done
by Telegraph Spk.

Injury done Inside Wall	
See Mr. Hues report.	\$ 50,415.00
do do Outside Wall	38,905.00
Total Total.	\$ 89,310.00
@ 768 - L	\$ 111,150.00
	111,150.00

11/8

Schedule F. 118

Demolition claim for losses incurred
at Shal Laikean Bridge Works

Loss of material, tools
and damage to plant
as per Mr. Howard's Report
and Schedule

£42,928 0.0

11/9

Schedule X.

119

Approximate Value of Stock

Tangier Workshops & Loco Sheds

	Total
12 ⁿ . Moon 25 ⁿ year of King Edward (25 ⁿ January 1900).	977,044.00
Material approximately received from 25/1/00 to 7/1/00, to meet the requirements for the erection of New Rolling Stock J. W.	67,126.00
do do O.W.	283,605.00
Against Running stores debt	58,651.00
Issue. February, March, April and May 1900, £65,000 per month	1,386,426.00
	260,000.00
	1,126,426.00
	<u>291,560.00</u>
	Total 334,566.00
	@ £1,750 £44,702.0.0

See Schedule A. Sheet 2.

Ref No. 12.

120

Advanced to British
Railway Administration
by British Government for
replacement and renewal
caused by Boxer damage

\$90.000 0-0

Ref No 13

1209

IMPERIAL RAILWAYS OF NORTH CHINA.

Indemnity Account.

Losses at Head Office, Tientsin.

Light Rails and Cars.	Taels 14,000.
Cost of replacing Title Deeds	<u>20,000.</u> £4,000.
Furniture, Safes etc.	10,000.
Stationery. etc.	5,000.
Dollars and fractional coins.	18,000.
	Taels <u>47, 000.</u>
£ 70. Ex.=	<u>£ 2570</u> £6 7/4. 0. 0

36-19612 7

Miscellaneous Claims

1. Material for Road Bridge over Peiho, take at 50% of Invoice No. 435. for £635. 2. 0
£ 317. 12. 0
2. Coal delivered to Brick contractors at Kao-pai-te, the property of Railway total cost £1000. 0. 0. £1200^m @ 10% 120. 0. 0
3. Two Hsüng Chieh store &c at Jang-pur £ 357. 3^m @ . 35 14 6
4. Hsueh Ming-jui's claim for horse at Chao-chia-tun occupied by W. M. McKenzie, destroyed by Boxcars, estimated value £ 3500^m @ . 350 0. 0
5. Honorarium to Pohu, tea corps at Tung-shan £ 2000^m @ . 200 0. 0
6. Pay of men working at European in Feng-tai 1st; 8th, 75th KH 26 £ 157. 88 @ . 15 15 0
7. Pay passenger money of Shantung Station police £ 495^m @ . 49 10. 0

£ 1088 11 6

Inside Wall £ 618. 11. 6
 Outside do 470. 0. 0
£ 1088. 11. 6

37 1960 23

Ref 10-15

A
122

Compensation to Staff which was retained in the east ready
for immediate reemployment under B. R. A. Service.

Schedule C.

Names.	Particulars.	Amount	Total
Barber. J.	5 months half pay from Aug. to Dec. at \$11.8.0. per month = \$59.00	59.00	
	House allowance 9 months at \$150 -	1350.00	1909.00
Cox. A. G.	Was paid full salary.		
	House allowance 9 months at \$150 -	1350.00	
	Less \$350. drawn at Yingkow	-350.00	1000.00
Cheyne. J. J.	5 months half pay at \$10.00 - = \$50.00	50.00	
	House allowance 9 months at \$150 -	1350.00	1350.00
Engstran. H.	3 months at half pay at \$13.00 per month = \$39.00	39.00	
	3 months House allowance at \$150 -	1350.00	1749.00
Foley. J. E.	Received full pay		
	9 months House allowance at \$150 -	1350.00	1350.00
Harris. F. A.	5 months half pay at \$17.10.0. per month = \$85.50	85.50	
	9 months House allowance at \$150 -	1350.00	2225.00
	Carried Forward		19065.00

83

• 3 • 金華市地圖

Schedule C. (Continued)

A.I.

123

which gave us at best not more than 50% of possible production.

Claim for Compensation of Railway Staff.

123

Names.	Particulars.	B/F.	Amount	Total.
McAllister. J.J.	House allowance 9 months	B/F.		18665.88
Howard. W. G.	5 months pay at \$22.16.4 =		1108.88	554.44
	\$2114.11.8,		1145.82	
	House allowance 9 months at \$150.		1350.00	2495.82
Jameson. F.A.	5 half months pay		1250.00	
	at \$20.16.8 = \$10.4.3.4.		1041.62	2081.62
	House allowance 9 months at \$150.		1350.00	2301.62
Kitching. F.	2 months at half pay		775.00	
	at \$11.75 - \$22.75 -		220.00	1035.00
	House allowance 9 months at \$150.		1350.00	1575.00
Kinder. G. W.	House allowance 9 months at \$150.		1350.00	1350.00
Leitch. W. O.	5 months half pay <i>After claim #150.1.0 at \$18.15.8 = \$93.15.4.</i>		465.00	
			937.50	
	House allowance 9 months at \$150.		1350.00	1060.50
	Less \$350. drawn at Ying kou		-350.00	1037.50
Martin. J.C.	5 months half pay <i>After claim #149.13.8 = \$706.62 at \$18.15.8 = \$93.15.4.</i>		448.25	
			937.50	
	House allowance 9 months at \$150.		1350.00	1136.88
	Less \$350. drawn at Ying kou		-350.00	1037.50
Marshall. H.J.W.	5 months half pay			
	at \$20.16.8 = \$10.4.3.4.		1041.62	
	House allowance 9 months at \$150.		1350.00	
	Passage money unpaid \$50.		-500.00	2801.62
Hoffat. J.	House allowance 9 months		-725.00	
	at \$150. per month		1350.00	1350.00
Moore. J.	Do. do		1350.00	1350.00
No. 4. Hall.	5 months half pay		300.00	1500.00
	Quality laundry			
	play clothes & soap etc.		-300.00	-600.00

Section 10 (Definitions)

Costs of Construction of Residential Projects

Names.	Irvin:	Particulars.	Amount	Total
		With Feb. 1st, in date of B/F.		25005.00
Maclelland, J.		3 months House allowance months at \$150. per month	450.00	26451.00
		went on leave home		
Newmarch, I.J.		5 months half pay		
		at 225.-" 2125.-"	1250.00	
		House allowance 9 months at \$150.	1350.00	2600.00
Rathmell, W.		5 months at half pay		
		at 2250.00. 227.10.00.	-275.00	
		House allowance 9 months at \$150.	1350.00	1625.00
Reckerby, J.		On full pay drawn from military		
		House allowance 9 months at \$150.	1350.00	1350.00
Ricketts, D.P.		Paid by Military		
		House allowance 2 months at \$150.	300.00	300.00
Rugby, E.H.		Received full pay		
		House allowance 9 months at \$150.	1350.00	
		Deduct \$350. drawn at Ying kou	-350.00	1000.00
Roberts, H.		3 months full pay at \$120.	-360.00	
		2 do half pay at \$60	-120.00	
		House allowance 9 months at \$90.	-810.00	1200.00
Sheriff, A.		Received full pay		
		No House allowance		
Tuckey, T.W.T.		Fully paid		
Wheeler, A.		5 months half pay at \$20.10.		
		247.10.00.	-475.00	
		House allowance 9 months at		
		\$150.	1350.00	1625.00
V. -		Pay to Oct. 14. by		
		Military House allowance		
		Hospital or 2150.-		

Ref. 15/3

B. F.

221, 01

Messrs. Irwin.		Half pay June July 3ist 1900 to 23th Feb.'01. to date of B.R.A.	
		Landing over to E.C.R. 7 months	Amount Total
Brown, W.H.		at £20. per month £140.-/- at £10.10s. per month £70.-/- Manus Return Allowance at £100. 7 months to 1st Feb. '01 notice	1400.00 1400.00
Brown, W.H.			£25.00
			39.621.01
		Total	41105.6
Brown, W.H.		at £10.10s. per month £5.50. Inside Hall £1730.68 - £ 1730.00 Outside do 7850.33 - £ 785.00 Monthly Half pay due in lieu of notice at £10.10s. per month £5.50. Inside Hall, drawn at Hastings 5 months half pay in lieu of notice at £10.10s. per month £5.50.	£5.50 1730.00 785.00
Brown, W.H.			1845.00 1845.00
Brown, W.H.		2 months at half pay at £10.10s. per month £5.50. 3 months full-pay at £20. £75.-/-	110.00 110.00 75.00
Brown, W.H.		3 months full-pay at £20.	75.00 75.00
Borggaard, H.J.		3 months half pay at £10.10s. per month £5.50.	110.00 110.00
Palmer, R.J.		3 months half pay at £10.10s. per month £5.50.	110.00 110.00
		Total	6550.00
		Inside Hall £1730.68 - £ 1730.00 Outside do 1730.16 - £ 685.00 £685.00 - £ 685.00	1730.00 685.00

Ref. No. 18

126

Schedule C.

Staff remaining in East but not

employed by I.C.R.Y or B.R.A.

Names	Particulars.	Amount	Total
Brown, H.H.	Five months in lieu of notice at \$12.15.4 = \$62.10.0.	625.00	625.00
Clarke, J.	6 months House allowance at \$150	900.00	1525.00
Dunn, E.C.A.	Five months in lieu of notice	333.33	333.33
Harris, J.	at \$16.13.4 = \$83.6.0. Was not asked to remain.	833.33	833.33
Dixon, A.	Required by reemployed	0.00	0.00
Gibson, R.G.	Reemployed by B.R.A.	0.00	0.00
Harris, G.C.A.	5 months half pay due in lieu of notice at \$16.13.4 = \$83.6.0.	833.33	833.33
Griffeth, D.P.	Less \$350. drawn at Yingkou 5 months half pay in lieu of notice at \$20.16.8. = \$104.3.4.	-350.00	483.33
Knowles, G.S.	2 months at half pay at \$12.10.8. = \$60.0.0.	-250.00	250.00
Krier, Otto.	3 months full-pay at \$205.275.0.0.	-750.00	1000.00
Norregard, B.W.	5 months half pay	-500.00	500.00
Palmer, R.J.	5 months half pay at \$0.5.0. = \$25.5.0.	-25.50	25.50
Watlin, W.	5 months half pay at \$0.5.0. = \$25.5.0.	-25.50	25.50
Willis, J.	5 months pay in lieu of notice	Total	6683.28
			6683.28
Inside Wall	\$704.12	= £	54.0.0
Outside do	1729.16	=	128.0.0
	\$6883.28	=	£689.0.0

Ref No. 7
Schedule C.

127

For additional information this

• A short history of the magazine

Name.	Particulars	Amount	Total
Bone, T.	Five months full-pay in lieu of notice at \$18. per month = \$90.-	900.00	900.00
Clarke, C.	5 months full-pay at \$18.- = \$90.00	1350.00	1350.00
Clarke, J.	5 months full-pay at \$15.- = \$75.-	750.00	750.00
Currie, G. [initials] M. Allo. 10/10/38 8/2/39/3/4	5 months full-pay at \$45.16.0 = \$225.84. <i>6 months leave</i>	650.00 220.16.02	650.00 220.16.02
Dunn, E.C.A.	5 months full-pay in lieu of notice at \$18.- = \$90.-	900.00	
	deduct \$350. drawn at Ying-kou	-350.00	550.00
Dorrell, H.	5 months House allowance at \$18.- = \$90.-	900.00	900.00
Emmerson, A.	5 months at half pay at \$11.10.0. per month = \$55.10.0.	575.00	
	5 months House allowance at \$150.-	900.00	1125.00
	Less \$350 was retained at Yingkou for 6 months	-350.00	
Early, W.H.	5 months half pay at \$11.10.0. \$55.10.0.	575.00	625.00
Franklin, H.	5 months full pay at \$18.10.0. = \$90.10.0.	925.00	925.00
Gennill, J.	5 months pay in lieu of notice at \$58.6.0 = \$291.13.4.	2916.62	
	Less \$350. drawn at Yingkou	-350.00	2566.62
Hurst, F.	Ask to return to China refuse to do so	461.18. @ 5% 1166.63	6334.62
	5 months at half pay at \$14.11.0. = \$116.13.4.	1166.63	1166.62
	Carried forward	43	1514.07.77

	Names.	Particulars	Amount	Total
		Brought forward		17140.82
	Hancock, G.W.	5 months full pay in lieu of notice at £23.0.0 = £115.0.0.	1100.00	22570.82
10,000	Hoyle, A.B.	5 months at half pay at £18.15.0 = £93.15.0.	937.50	937.50
10,000	Jackson, J.W.	5 months full pay at £33.0.0 = £165.0.0.	1650.00	
10,000		Less £350. drawn at Yingkou	350.00	1316.62
10,000	Mackenzie, A.C.	5 months at half pay at £18.15.4 = £93.15.4.	933.33	
10,000		Less £350. paid at Yingkou	-350.00	483.33
10,000	Robke, O.	5 months half pay at £0. - £45.	450.00	
10,000		Less £350. drawn at Yingkou	-350.00	100.00
10,000	Simmon, G.	5 months half pay at £12.10/- = £60.00/-	600.00	
10,000		Less £350. drawn at Yingkou	-350.00	250.00
10,000	Statham, F.E. <i>Mr. Allens letter 7/2/18</i>	Probably 2 months pay due <i>5 months pay £166.84 at £33.0.0 = £66.15.4.</i>	666.62	1666.62
10,000	Flowman, W.	5 months full pay at £0. 2100.	1000.00	
10,000	Packham, R.	Was engaged but never served on Railway & during trouble was pd. passage. Say 2 months pay as compensation at £18 = £36.	360.00	
10,000		half months House allowance	75.00	435.00
10,000	Stafford, W.R.	5 months full pay in lieu of notice at £18.10.0 = £92.10.0.	925.00	925.00
10,000	Strong, J.	Six months pay in lieu of notice decreased relative claim at £25.0.0 = £125.0.0.	1250.00	
10,000		Carried forward		91828.948
10,000				31249.85
10,000				94.78
10,000				2

17/2

	Names.	Particulars	Amount	Total
		Brought forward		21888.04
	Symonds. T.	5 months full pay in lieu of notice at £26.0.0 = £130.0.0.	1300.00	£1249.87
	Terris. R.	5 months full pay in lieu of notice at £30.0.0 = £150.0.0.	1500.00	1500.00
	Tumber. R.S.	5 months half pay at £0.5.0 = £25.2.6.	231.25	231.25
		Total		£24030.10
				£35281.14

Inside Wall £ 27197.91 - £ 2719.16.0

Outside do 8.083.28 - 808. 6.6

£ 35281.14 - £ 3528. 2.6

Ref No 18

Wage Compensation

J-

For Wage Compensation

to coolie accompanying

Seyoun Expedition

\$4019.98 £402.0.0

o dr pipe = 18 cent shall stand
 3 d 308 - 65.880 8 to stand
 1 c 322 - 42.937 8

Ref. No. 19

W- Contingent Fund.

For this amount set
aside for Contingent
purposes.

Tabs 50,000⁰⁰ - £ 7500 0.0

Ref No 20

British Chinese
Corporation

W.

British Chinese Corporation

Claim for loss at

Napier. See Ref No 1.

£8980 1.5

IMPERIAL CHINESE RAILWAYS

TO BRITISH & CHINESE CORPORATION LTD.

Dr.

(W-1)
For amount of claim on account of losses in connection
with the Boring operations at Nampiao as presented to

H.B.M. Claims Commissioner £ 12012. 7. 4

Plus claimed for damages -

i.e interest on money, expenses of general &
head office, Administration charges
postponement of profit &c, say 15 o/o

£ 1801.17. 1

£ 13814. 4. 5

Less amount of Indemnity allowed by Claims Commissioner
under the assumption that a portion of the Plant
remained uninjured.

£ 4834. 3.

£ 8980. 1. 5

(This Claim is based upon information of the total destruction of every
thing by Native Soldiers or Brigands early in 1901 - see copies of corres-
pondence attached)

JARDINE, MATHESON & CO.

Per 

9061

4158
Jardine, Matheson & Co.

1903

IMPERIAL CHINESE RAILWAYS

TO REINFORCE & CENTER COORDINATION IN

This item of £ 4000 represents, according to Mr Kinder's estimate, the cost of a resurvey of the Railways lands.

This resurvey is made necessary by the loss or destruction of all the Title Deeds which were kept on the safes at the Head Office in Tientsin. These Deeds were the Railway's sole evidence of title as there seems to have been no official registration of the lands from time to time acquired by the Railways.

The British & Chinese Corporation as Mortgagee is interested in the
restoration of the title for the purposes of the security, and this
can be accomplished only upon the basis of a resurvey.

It is submitted therefore that the expense in question is a direct consequence of the Boxer uprising and may properly be included in the

--- 12.00 ---

4 April, 1903.

Sir,

Referring to the joint communication from Mr. Hewat and myself of 27 April last, I now beg to hand you herewith for the information of H.M. Government a copy of the Railway Claim as amended, amounting to £452,836. 13. 9.

The Administration holds of course the necessary documents and vouchers in support of the various items constituting the claim, and is prepared to produce these for verification whenever called upon, to whoever may be deputed by H.M. Government to examine and deal with same.

The Administration would however suggest the expediency of the examination being conducted in Tientsin where such further evidence as may be required is more easily accessible.

I avail myself of this opportunity to bring to your notice the fact that in consequence of the large expenditure already incurred in connection with the work of restoring the line to its ante-Boxer condition, the Railway Administration finds itself seriously hampered by want of funds.

Referring to my communication of 26th January, I venture to express the hope that H.M. Government will come to a decision as soon as possible when, and how, the final claim allowed will be made available, and in what amounts.

I understand that owing to questions at issue between the Powers and Chinese Government with regard to the interpretation of Clause 6 of the Protocol, delay has occurred in the preparation and signature of Bonds representing the British share

after Townley Esq.

A.C. A.C. A.C.

P R E V I E W.

4 - 1961 23

share of the indemnity, and it will be obvious to you that if this delay is likely to be protracted and the settlement of the Railway Claim has to await the decision of the questions referred to, serious embarrassment will be caused to the Administration., not only with regard to the claims of firm and individuals for which it is continually being pressed, but also with regard to progress of work on the line.

In view of this I have to express the hope that H.M.'s Government will see its way to arrange for the examination and settlement of the Claim as soon as possible.

I have etc.

(Sgd.) Ed. Cousins.

Representing British & Chinese Corptn. Ltd.

- 1961 - 3

2
in
H. T. Townley
No 138
9 April
1903.

~~CONFIDENTIAL.~~

SECTION 3.

[No. 1.

Mr. Townley to the Marquess of Lansdowne.—(Received May 23.)

(No. 138.)

My Lord,

Peking, April 9, 1903.

I HAVE the honour to transmit herewith to your Lordship, in original, and with reference to your despatch No. 42 of the 13th February last, a detailed Statement of the Northern Railways claim as furnished to me by Mr. Cousins.

Your Lordship will perceive that the total claim for both intra- and extra-mural lines amounts to 448,836 13s. 9d., to which will have eventually to be added a sum of 4,000*l.*, the estimated cost of the resurvey of the line which will have to be made in order to establish the titles to lands of which the deeds were lost in the destruction of the head office at Tien-tsin. The above-mentioned sum, divided, I am told, as well as may be between the two lines, is made up of:—

	£	s.	d.
Intra-mural line	319,680 18 10
Extra-mural line	121,655 14 11
Contingent fund	7,500 0 0
Total	<hr/> 448,836 13 9

I understand that a fuller and more detailed examination of the extra-mural line than Mr. Kinder had been able to make before he furnished Sir E. Satow with the information supplied in his despatch No. 320 of the 29th October last, has caused him to place the Engineering Department's claim for damage to that line at 104,010*l.* Only a small portion of this last figure covers damage to rolling-stock, and that chiefly applicable to engines, since it was not possible to make a full separate allocation of rolling-stock to the one line or the other, and where separation has not been made the rolling-stock and supplies have been attributed to the intra-mural line.

Mr. Cousins tells me that it is possible that the sum of 300,000*l.* claimed by His Majesty's Government on account of the extra-mural line will still be sufficient to cover both the present actual claim and the amount payable by the Chinese to the Russian Government which, he is given to understand, amounts to about 150,000*l.*, although, as far as he can ascertain, no accounts have as yet been submitted by the Russian Railway Administration. I am addressing a letter on this point to the Chinese Northern Railways Administration, and shall have the honour to submit their reply to your Lordship at alater date.

From the appended Statement, it would, however, appear that the sum of 500,000*l.* claimed on account of the intra-mural line would prove sufficient to cover the losses sustained on both lines, if it should be found possible to devise some means of meeting the very large discount to be encountered in negotiating Chinese Government indemnity bonds. Mr. Cousins has represented to me that the Railway Administration is in urgent need of immediate funds to enable it to carry out pressing repairs and to replenish its rolling-stock, and that the requisite funds are only forthcoming out of the Indemnity claim, which, however, in the shape of bonds, is not a negotiable factor except at a ruinous discount. He will write to me further upon the subject, and I will submit his communication to your Lordship upon its receipt.

Mr. Cousins points out that the sum of 30,000*l.* referred to in your Lordship's despatch under reply, on account of extensions to the Temple of Heaven and Peking, and from Peking to Tungchow, was specifically applied to the extensions and was drawn from loan funds, no claim on this account being made in the statement of claim, in which your Lordship will see that the sum of 90,000*l.* advanced by His Majesty's Government is duly included.

I have been carefully through the statement of claims with Mr. Cousins and Mr. Allen, and would beg to submit that I have found it, as far as I am able to judge, a reasonable one. The contingent fund of 7,500*l.* seems to be a necessary precautionary measure to take, and if the money is not required it will not be used.

References 15, 16, and 17 deal with claims on behalf of the railway staff, both those retained in China and those who have left the East. The names of all the Europeans employed on the railway at the time of the troubles figure on these lists, and where claims have been made by them the full amount of claim is given, although, in most cases, the claim is subject to reduction. The large majority of those employed by the railway who have left the East have made no claim at all, so that it is possible that the sum of close upon 9,000/- at which the amount of possible compensation has been fixed may be somewhat reduced. A Notification will be issued shortly in the London "Times" that all claimants of this class must make application for payments due to them before a certain date, probably six months after publication of the Notice. It is clear that, without such public announcement, the Railway Administration could not be sure for an indefinite time of the full amount of its liabilities.

I should be glad to be furnished with a copy of the statement of claims if the document is printed, as press of work has prevented a copy being made here.

I have, &c.

(Signed) WALTER TOWNLEY.

P.S.—Since writing the above, I have received an amended statement from Mr. Cousins, which includes the 4,000/- on account of resurvey, together with a Memorandum explaining the necessity for the same. In transmitting these documents to your Lordship, I have also the honour to inclose copy of a letter just received from Mr. Cousins, covering the railway claim, pointing out that the Railway Administration is seriously hampered by want of funds, and expressing a hope that His Majesty's Government will come to an early decision as to when, and how, the final claim allowed will be made available, and in what amounts.

W. T.

Inclosure 1 in No. 1.

Mr. R. Cousins to Mr. Townley.

Tien-tsin, April 4, 1903.

Sir,
REFERRING to the joint communication from Mr. Hewat and myself of the 27th April last, I now beg to hand you herewith, for the information of His Majesty's Government, a copy of the railway claim, as amended, amounting to 452,836/- 13s. 9d.

The Administration holds, of course, the necessary documents and vouchers in support of the various items constituting the claim, and is prepared to produce these for verification whenever called upon, to whoever may be deputed by His Majesty's Government to examine and deal with same.

The Administration would, however, suggest the expediency of the examination being conducted in Tien-tsin, where such further evidence as may be required is more easily accessible.

I avail myself of this opportunity to bring to your notice the fact that, in consequence of the large expenditure already incurred in connection with the work of restoring the line to its anti-Boxer condition, the Railway Administration finds itself seriously hampered by want of funds.

Referring to my communication of the 26th January, I venture to express the hope that His Majesty's Government will come to a decision as soon as possible when, and how, the final claim allowed will be made available, and in what amounts.

I understand that, owing to questions at issue between the Powers and Chinese Government with regard to the interpretation of Clause 6 of the Protocol, delay has occurred in the preparation and signature of bonds representing the British share of the indemnity, and it will be obvious to you that if this delay is likely to be protracted, and the settlement of the railway claim has to await the decision of the questions referred to, serious embarrassment will be caused to the Administration, not only with regard to the claims of firms and individuals, for which it is continually being pressed, but also with regard to progress of work on the line.

In view of this I have to express the hope that His Majesty's Government will see its way to arrange for the examination and settlement of the claim as soon as possible.

I have, &c.

(Signed) ED. COUSINS,

Representing British and Chinese Corporation (Limited).

Inclosure 2 in No. 1. Detailed Statement of the Northern Railway Claim.

SUMMARY of Claim.

No.					Inside Wall.	Outside Wall.	Total.
					£ s. d.	£ s. d.	£ s. d.
1	Inclusive Matheson, and Co. As per statement Avl 32,037.07 taels, at 7 taels	•••	•••	•••	£ 1,051 16 5 3,165 6 0	5,117 2 5	••• 5,117 2 5
2	Thomas Ironworks. Interest for three years, from 1st July, 1900, to 1st July, 1903, on their account, 3,832/- 9s. 11d., at 3½ per cent, per annum, for material landed at Tientsin)	•••	•••	•••	•••	402 8 0	402 8 0
3	Ferrier, Daniel and Co., Limited. Interest on their account	•••	•••	•••	175 19 10	•••	175 19 10
4	John Birch and Co., Limited. Interest on their account, for 2,600/- for wheels and axles for tenders stored in England for three years, from 1st July, 1900, to 1st July, 1903, at 3½ per cent, per annum	•••	•••	•••	262 10 0	•••	262 10 0
5	Mackenzie and Co. For boiler tubes and sanitary materials ex steamship "Clingaro".	•••	•••	•••	756 13 4	•••	756 13 4
6	American Trading Company. For 100 cases window glass.	•••	•••	•••	69 5 9	•••	69 5 9
7	Arnold, Körberg, and Co. For 1,291 tons Japanese coal delivered at Tungkow	•••	•••	•••	•••	1,067 4 7	1,067 4 7
8	Cing Shan Bank. For three years interest, at 7 per cent, per annum, from 1st July, 1900, to 1st July, 1903, on 15,070 taels = 3,164.70 taels, at 7 taels = 11.	•••	•••	•••	•••	452 0 0	452 0 0
9	Foo Fung Shueh Bank. For three years interest, at 7 per cent, per annum, from 1st July, 1900, to 1st July, 1903, on 8,000 dollars = interest, 1,480 dollars, or, at 10 dollars = 11.	•••	•••	•••	•••	148 0 0	148 0 0
10	Freight and storage, wharf charges on cargo stored in Shanghai, and brought to Hsinho in December 1902 by steamships "Nugget" and "Changchow".	•••	•••	•••	2,719 16 0	•••	2,719 16 0
11	Summary of claim of engineering departments, as per Mr. Kinder's statement.	•••	•••	•••	209,161 0 0	•••	209,161 0 0
12	Amount retained by British Government for replacement and removal caused by Boiler damage	•••	•••	•••	30,000 0 0	•••	30,000 0 0
13	Losses at head office, Tien-tsin.	•••	•••	•••	6,714 0 0	•••	6,714 0 0
14	Miscellaneous claims, as per statement.	•••	•••	•••	618 11 6	•••	618 11 6
15	Compensation to staff retained by British Railway Administration.	•••	•••	•••	3,173 0 0	•••	3,173 0 0
16	Staff remaining in the East, but not employed by British Railway Administration.	•••	•••	•••	511 0 0	•••	511 0 0
17	Staff which left the East, services not being required.	•••	•••	•••	178 0 0	•••	178 0 0
18	Wages and compensation to coolies accompanying Seymour expedition, 4,019.98 dollars.	•••	•••	•••	808 6 6	•••	808 6 6
					402 0 0	•••	402 0 0
19	Contingent Fund				£ 7,500 0 0	•••	7,500 0 0
20	British and Chinese Corporation claim for losses at Sianpoo.	•••	•••	•••	•••	8,980 1 6	8,980 1 6
21	Estimated cost of resurvey of railway lands	•••	•••	•••	•••	4,000 0 0	4,000 0 0
Total	•••	•••	•••	•••	7,500 0 0	319,680 18 10	121,655 14 11
							452,836 13 9

(Signed) H. J. W. MARSHALL, Secretary, Imperial Railways of North China.

(No. 1.)

Tien-tsin, December 17, 1902.

Dear Sir,
 Referring to our communication of the 1st ultimo, and documents attached, we now beg to hand you herewith an amended Statement of telegraphic expenditure, which please substitute for the one rendered.

Our claims are now summarized as follows:—

	Tael.
Amounts due for material lost through the Boxer troubles in 1900, together with interest thereon, as per separate Statement	20,344·80 1,712·90
Expenditure on telegrams, as per Statements attached	£ s. d. 1,951 16 5 and
British and Chinese Corporation's claim for losses at Nampiao, less amount allowed by His Britannic Majesty's Claims Commissioner	8,980 1 5
	<hr/>
	10,931 17 10 and
	22,057·70

We are, &c.
 (For Jardine, Matheson, and Co.),
 (Signed) ED. COUSINS.

M. T. Liang, Esq., Director,
 Imperial Chinese Railways.

SUMMARY of Amounts due by the Imperial Chinese Railways to the Undersigned, on account of Material imported under their Indents and lost in consequence of the Boxer troubles of 1900, also of Interest as per Statements attached.

A/c Indent	No.	Tael.
"	32	4,457·31
"	38	694·17
"	1605 (part)	1,612·27
"	1605/1608	5,174·39
"	1610	338·58
"	1676	5,230·31
"	619, 144, 9d., at 2s. 4 <i>1/2</i> d.	2,857·77
"	Various, delivered to British Railway Administration	
	Total	20,344·80

(E. and O. E.)

(For Jardine, Matheson, and Co.),
 (Signed) ED. COUSINS.

Tien-tsin, November 1, 1902.

IMPERIAL CHINESE RAILWAYS.

Indent No. 32 (part), London Invoice No. 68, due April 10, 1900.

	£ s. d.	Tael.	£ s. d.
5 cases split and copper pins	.. .. .. .. ..	.. .. .. .. ..	47 7 0
53 " iron rivets	.. .. .. .. ..	.. .. .. .. ..	232 5 0
3 " brass sheets	.. .. .. .. ..	.. .. .. .. ..	.. .. .. .. ..
3 " copper sheets	.. .. .. .. ..	.. .. .. .. ..	.. .. .. .. ..
2 " copper tubes	.. .. .. .. ..	.. .. .. .. ..	.. .. .. .. ..
1 case copper and brass tubes	.. .. .. .. ..	.. .. .. .. ..	379 12 1
Freight and bill of lading			43 5 7
Insurance on 770L, at 12s. 6d., and stamp	.. .. .. .. ..	38 0 4	702 9 6
Bill stamp	.. .. .. .. ..	4 18 3	Taels.
		0 7 0	5,941·71
At ex. 2s. 4 <i>1/2</i> d. ..			1,153·81
Plus interest from 10th April, 1900, to 31st December, 1902, = 995 days, at 7 per cent, per annum			7,075·52
Less amounts received from the British Railway Administration for portions of above material taken over by them, plus interest calculated thereon as per Memo, at foot			2,618·21
Balance due by the Imperial Chinese Railways			4,457·31
1901.	MEMO.		
October.	Amount received from British Railway Administra-	Tael.	Tael.
	Add—Interest from 1st November, 1901, to 31st		
	December, 1902, = 14 months, and 7 per cent.	109·37	
1902.	Amount received from British Railway Administra-	1,339·24	
	Add—Interest from 1st May, 1902, to 31st December,		1,448·61
	1902 = 8 months, at 7 per cent.	52·15	
	Total	1,117·45	1,169·60
		.. .. .. .. ..	2,618·21

(E. and O. E.)

(For Jardine, Matheson, and Co.),
 (Signed) ED. COUSINS.

Tien-tsin November 1, 1902.

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IMPERIAL CHINESE RAILWAYS.

Indent No. 38, London Invoice No. 99, due July 19, 1900.

	£ s. d.	£ s. d.
2 cases crucibles ..	..	32 15 3
2 " saws and gutters ..	..	56 11 0
24 " vices ..	..	..
1 case ratchet braces ..	..	64 3 5
2 cases jacks ..	..	53 16 0
1 case letter stamps ..	..	1 1 0
Bulls-eyes ..	..	0 7 8
Freight and bill of lading ..	11 17 3	
Insurance on 240L ..	1 10 6	
Bill stamp ..	0 2 0	
	13 9 9	
	222 4 1	
	Taels.	
At ex. 2s. 4d. ..	1,879·44	
Plus interest from 19th July, 1900, to 31st December, 1902, = 895 days, at 7 per cent. ..	322·42	
	2,201·86	
Less amounts received from the British Railway Administration for portions of above taken over by them and interest calculated thereon, as per Memo. at foot ..	1,507·69	
Balance due by Imperial Chinese Railways ..	694·17	
MEMO.	Taels.	Taels.
1902.		
March. Amount received from British Railway Administra-		
tion ..	1,407·20	
Add—Interest from 1st April to 31st December, 1902, = 9 months, at 7 per cent. ..	73·88	
	1,481·08	
August. Amount received from British Railway Administra-		
tion ..	26	
Add—Interest for 4 months ..	0·61	
	26·61	
	1,507·69	

(E. and O. E.)

(For Jardine, Matheson, and Co.),
(Signed) ED. COUSINS.

Tien-tsin, November 1, 1902.

IMPERIAL CHINESE RAILWAYS.

Indent No. 1605 (part), London Invoice No. 85, due April 9, 1900.

	£ s. d.	£ s. d.
6 kegs rivets ..	..	..
120 iron tubes ..	..	..
1 cask bends, &c. ..	..	..
1 cask flanges, &c. ..	..	..
137 steel tank plates ..	..	..
Bill of lading and freight ..	..	..
Insurance on 465L ..	..	..
Bill stamp ..	..	..
	54 6 0	
	21 9 5	
	4 4 6	
	57 9 11	
	425 1 5	
	Taels.	
At ex. 2s. 4d. ..	3,595·21	
Plus interest from 9th April, 1900, to 31st December, 1902, = 996 days, at 7 per cent. ..	659·33	
	4,254·54	
Less amounts received from the British Railway Administration for portions of above material taken over by them, and interest calculated as per Memo. at foot ..	2,342·27	
Balance due by the Imperial Chinese Railways ..	1,612·27	
MEMO.	Taels.	Taels.
1901.		
October. Amount received from the British Railway Adminis-		
tration ..	425·92	
Interest for 16 months from 1st September, 1901, to 31st December, 1902.. ..	39·75	
	465·67	
July. Amount received from the British Railway Adminis-		
tration ..	1,980·25	
Interest for 17 months from 1st August, 1901, to 31st December, 1902.. ..	196·37	
	2,176·60	
	2,642·57	

(E. and O. E.)

(For Jardine, Matheson, and Co.),
(Signed) ED. COUSINS.

Tien-tsin, November 1, 1902.

IMPERIAL CHINESE RAILWAYS.

Indents Nos. 1605 and 1608 (part), London Invoice No. 87, due April 10, 1900.

		£ s. d.	£ s. d.
13 cases bolt ends ..	..	..	96 10 0
16 " weighing machines ..	..	..	97 6 3
57 bars angle steel ..	..	..	27 19 1
50 cases galvanized iron ..	..	..	401 5 2
40 kegs brown paint oak ..	..	..	..
48 drums drying oil thinning ..	..	..	..
6 " turpentine ..	..	..	..
1 " ease chalk ..	..	..	105 0 8
1 " pump leathers ..	..	..	2 3 0
1 " padlocks and snaps ..	..	..	..
1 " sledge hammers ..	..	..	11 1 10
13 bundles miners drill steel ..	..	..	37 17 2
Freight and bill of lading ..	..	102 4 10	..
Insurances, &c. ..	..	6 12 9	..
		108 17 7	..
		886 0 9	..
		Taels.	..
		7,494 24	..
		1,450 06	..
		8,924 30	..
At ex. 2s. 4d. =	..	..	..
Plus interest from 10th April, 1900, to 31st December, 1902, =	..	..	..
995 days, at 7 per cent. ..	..	..	..
Less amounts received from British Railway Administration for portions of above material taken over by them, and interest calculated thereon as per Memo. at foot ..	..	..	3,749 91
Balance due by the Imperial Chinese Railways ..	..	..	5,174 39

MEMO.

	Taels.	Taels.
1901.		
November. Amount received from British Railway Administra-	356 58	
tion		
Interest for 13 months from 1st December, 1901, to		
31st December, 1902, at 7 per cent. ..	27 05	383 63
1902. Amount received from British Railway Administra-	3,216 19	
tion		
Interest for 8 months from 1st May, 1902, to		
31st December, 1902, at 7 per cent. ..	159 09	3,366 28
Total ..	..	3,749 91

(E. and O. E.)

(For Jardine, Matheson, and Co.),
(Signed) ED. COUSINS.

Tien-tsin, November 1, 1902.

IMPERIAL CHINESE RAILWAYS.

Indent No. 1610, London Invoice No. 89, due April 10, 1900.

	£ s. d.	£ s. d.
2 cases drawing material
Freight and bill of lading	1 13 11
Insurance on 70L and stamp	0 9 0
Bill stamp	0 1 0
		2 3 11
		62 9 0
At ex. 2s. 4d. =	..	Taels.
Plus interest from 10th April, 1900, to 31st December, 1902, =	..	528 21
995 days, at 7 per cent.	100 79
		689 00
Less amount received from British Railway Administration, as per Memo. at foot	290 42
Balance due by the Imperial Chinese Railways	338 58

MEMO.	Taels.	Taels.
1902.		
April. Amount received for part of above from the British Railway Administration..	277 47	
Add—Interest from 1st May, 1902, to 31st December, 1902 = 8 months, at 7 per cent. ..	12 95	290 42

(E. and O. E.)

Tien-tsin, November 1, 1902.

(For Jardine, Matheson, and Co.),
(Signed) ED. COUSINS.

IMPERIAL Railways of North China to the Undersigned debtor.

	£ s. d.
For amount due on account of 60-lb. rails delivered ex "Forestdale" and "Macedonia" at Sin-Ho 20th June, 1901, under Indent No. 1576, and contract of 21st September, 1899, viz.:—	
	Tons. cwt. qrs. lbs.
Less sales ..	4,701 rails = 1,252 1 3 17
	3,600 " 696 8 2 9
	2,101 " 555 13 1 9
	At 7l. 16s. 1½d. = { 9,772 17 3
	5,135 15 5
	4,337 1 10*
Plus interest, at 7 per cent. per annum, as follows:—	
On 2,192L 4s. 4d. from 20th May, 1901, to 20th Sep-tember, 1901 = 3 months ..	38 8 4
" 3,240L 11s. 1d. from 20th May, 1901, to 20th February, 1902 = 8 months ..	151 4 6
" 4,337L 1s. 10d. from 20th May, 1901, to 20th Novem-ber, 1902 = 17 months ..	430 1 11
	619 14 9†
9,772L 17s. 3d.	4,936 16 7
At ex. 2s. 4d. 7½ = ..	Taels.
	41,650 38

(E. and O. E.)

Tien-tsin, October 8, 1902.

(For Jardine, Matheson, and Co.),
(Signed) ED. COUSINS.* Paid in London.
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† Outstanding 619L 14s. 9d. = 5,230 31 taels.

D

STATEMENT, showing Interest due to the Undersigned on Railway Material taken over by the British Railway Administration.

10

Indent No.	Number of Packages.	Material.	Vessel.	Amount of Invoice.	Due Date.	When Paid.	Period.	Interest.
32 {	17	Rivets ..	" Ulysses "	4 " 75847	May 9, 1900	October 31,	Days.	Taels.
1605 34	1	Bolt ends ..	" " 1,357 88	April 10, "	31, "	"	541	81-81
1608 1	1	Sledge hammers ..	" " 145-16	August 29, "	November 29, "	"	570	142-97
1608 3	3	Zinc sheets ..	" " 178-94	June 14, "	March 11, 1902	"	458	12-75
38 {	4	Vane ..	" " 632-63	August 29, "	March 11, "	"	636	21-83
38 {	1	Steel springs ..	" " 637-81	July 19, "	July 19, "	"	560	70-09
24 {	1	Vane ..	" " 908-99	August 29, "	July 19, "	"	601	112-70
2 {	1	Bolt heads ..	" " 3,022-40	August 29, "	July 19, "	"	606	105-86
217 1618	3	Shovels, picks, &c.	" " 1,065-30	July 19, "	July 19, "	"	588	363-38
1617 11	1	Welding machines ..	" " 1,065-30	July 19, "	July 19, "	"	601	122-70
38 {	1	Crampons ..	" " 375-11	"	18, "	"	43-24	
1605 192	1	Saw and cut ..	" " 3,498-07	April 10, "	May 10, "	"	601	
1608 1610	1	Sandery ..	" " 202-33	"	10, "	"	763	501-63
1608 32	2	Mattress ..	" " 1,684-60	"	10, "	"	763	39-42
270 25	3	Drawing material ..	" " 1,011-96	"	10, "	"	763	158-75
1605 176	1	Rivets ..	" " 632-60	June 29, "	June 29, "	"	764	162-54
295	1	Bolt iron ..	" " 69-81	"	25, "	"	730	87-18
1605 {	137	Tom plate ..	" " 1,908-17	May 9, "	May 9, "	"	826	11-06
38 {	1,342	Bolt iron ..	" " 3,191-83	May 9, "	May 9, "	"	836	313-25
38 {	1	Letter stamps ..	" " 12-56	July 19, "	July 19, "	"	765	495-10
	Total ..	" "	" "	"	"	"	69 5 9	1-84
						"	756 13 4	
						"	2,837-77	

(E. and O. E.)

(For Jardine, Matheson, and Co.,
(Signed) ED. COUSINS.

Tien-tsin, November 1, 1902.

11

(No. 2.)
Thames Iron Works.

To interest for three years, from 1st July, 1900, to 1st July, 1903, on their account 3,832L 9s. 11d., at 3½ per cent. per annum, for materials landed at Taliwenwan

£ s. d.
402 8 0

(Signed) (Thames Iron Works.)

(No. 3.)

Fearon, Daniell and Co.

To interest on their account from 5th April, 1901, for 677 packages steel bars, angles, plates, and rivets stored at Hisho

£ s. d.
175 19 10

(No. 4.)

John Birch and Co. (Limited).

To interest on their account for 2,500L, for wheels and axles for tenders, stored in England, for three years, from 1st July, 1900, to 1st July, 1903, at 3½ per cent. per annum

£ s. d.
262 10 0

(No. 5.)

Mackenzie and Co.

For boiler tubes and boiler covering ex "Chingwo," on account of indent 1599, 599 packages

£ s. d.
756 13 4

(No. 6.)

The Imperial Chinese Railways of North China, debtor to the American Trading Company.

◇ 100 cases 16 x 12 window glass, at 4-85 taels ..

Taels.
485

485 taels =

£ s. d.

For the American Trading Company,
(Signed) , Agent.

Tien-tsin, December 4, 1899.

(No. 7.)

Arnhold, Karberg, and Co. Contract, May 19, 1900.

	Dollars.
1,291 tons Japanese coal delivered alongside railway wharf, Yingkow, at 7·85 dollars per ton..	10,134·35
Interest, 15th June to 25th September, 1901, 102 days, at 6 per cent.	169·92
	<u>10,304·27</u>
	Taels.
At 72½	7,470·60
At 7 taels	<u>1,067 4 7</u>

(No. 8.)

Chin Shan Bank.

	Taels.
To three years' interest, at 7 per cent. per annum, from 1st July, 1900, to 1st July, 1903, on 15,070 taels	3,164·70
At 7 taels	<u>452</u>

(No. 9.)

Foo Fung Sheng Bank.

	Dollars.
To interest, at 7 per cent. per annum, from 1st July, 1900, to 1st July, 1903, on 8,000 dollars	1,480
At 10 dollars	<u>148</u>

(No. 10.)

Imperial Railways of North China, debtor to Buchheister and Co. (Limited), Tien-tsin.

	Dollars.	Taels.
For the following, viz.:-		
Charges on railway material shipped from Shanghai to Tien-tsin per steam-ships "Ningpo" and "Chang Chow" -		
Charges in Shanghai, as per Statement No. 1 .. .. .. .. ..	..	26,291·06
" .. .. .. .. ..	..	9,572·62
Charges in Tien-tsin -		
Taku Tug and Lighter Co. Voucher No. 10	3,050·25	..
"	522·90	..
Interest, 18th to 30th December (13 days)	8·55	..
Telegrams and postages re "Ningpo"	75·00	..
Comshaw to Captain of "Ningpo," 150 Shanghai taels, at 101½ .. .. .. .. ..	..	15·10
Commission, 1½ per cent. .. .. .. .. ..	..	147·78
3,657·75	36,026·56	
54·87	540·39	
	<u>3,712·62</u>	<u>36,566·95</u>
	<u>£ s. d.</u>	<u>£ s. d.</u>
Rate of exchange, 71·4 taels = 1 dollar at 2s. 2½d. = .. .. .. .. ..	..	..
294 1 6	4,056 12 11	
	<u>£4,350 14s. 5d.</u>	

Tien-tsin, December 30, 1902.

Received payment:

(Signed) BUCHHEISTER AND CO. (LIMITED).

December 31, 1902.

(No. 10 (1).)

Imperial Railways of North China, debtor to Buchheister and Co. (Limited).

	Shanghai Taels.
For the following, viz.:-	
Charges on railway material forwarded per "Ningpo" to Hsinho Wharf -	
Storage, Shanghai Hongkew Wharf Company, as per -	
Voucher No. 1	9,147·60
" No. 2	273·90
" No. 3	845·90
" No. 4	6,419·40
Wharfage and night and Sunday time, as per Voucher No. 5	626·00
Stevedore, as per Voucher No. 6	633·90
359·47 dollars at 74·4125, as per Voucher No. 7 =	260·79
Freight on account from 14th to 26th November, approximate arrival of steamer at Taku, 10,400 dollars, at 74·4125	7,738·90
Balance of freight, to be settled on completion of discharge (Voucher No. 8) .. .. .. .. ..	..
Marine insurance, £30,000 = 266,666·66 taels at ¼ per cent. (Voucher No. 9)	666·67
Shanghai telegrams, 52·48 dollars, at 74·4125	39·05
	<u>26,652·11</u>
Bill brokerage, ½ per cent.	33·32
	<u>26,685·43</u>
	<u>Tien-tsin Taels.</u>
At 101½	<u>26,291·06</u>

Tien-tsin, December 30, 1902.

Received payment:

(Signed) BUCHHEISTER AND CO. (LIMITED).

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E

(No. 11.)

IMPERIAL CHINESE RAILWAYS.

Summary of Claims for Indemnity, Engineering Departments.

		£
Schedule (A).—Rolling-stock	.. .. ..	45,900
" (B).—Tien-tsin to Peking	.. .. ..	144,880
" (C).—Tien-tsin to Kuyeh	.. .. ..	7,480
" (D).—Outside Wall	.. .. ..	61,833
" (E).—Telegraph	.. .. ..	11,150
" (F).—Bridge works	.. .. ..	12,928
		213,171

(Signed) C. W. KIRDER.

February 9, 1903.

NOTE.—This claim does not include—

- (1.) Compensation to staff.
- (2.) Destruction of head office, Tien-tsin.
- (3.) Cost of storage and reshipping materials from Europe and Shanghai.
- (4.) Interest payable on debts.

(No. 11 (1).)

SCHEDULE (A).

Indemnity Claim for Losses to Cars, Locomotives, and Shops.

	£
Valuation of rolling-stock on June 1900, see attached report	345,000
76 mud cars, Imperial Chinese Railway type	3,940
200 German ditto, ditto	8,000
	355,040
Revaluation of same in October 1902, see attached report	374,300
59 mud cars, Imperial Chinese Railway type	2,360
92 German ditto, ditto	3,680
	380,340
Increased value..	33,800

(No. 11 (2).)

SCHEDULE (A).—continued.

	£
Cost of repairing and rebuilding 500 cars used by Russians, at 30 <i>l.</i> each	15,000
Cost of repairing and rebuilding 8 locomotives destroyed by Boxers, P.K. line	9,600
Repairs and rebuilding 26 locomotives partly destroyed by Russians outside Wall..	10,400
Loss of materials for car and locomotive building at Tongshan (see attached Schedule X)	44,700
	79,700
Claim amount, as above	79,700
Less—Increase in value of stock due to building of same during British Railway Administration ..	33,800
Balance due	45,900

(No. 11 (3).)

SCHEDULE (B).

Indemnity Claim for Losses and Damage to Tien-tsin-Peking Railway.

	£
80 miles of 2nd track— Rails, 12,000 tons, at 7 <i>l.</i>	84,000
Sleepers	30,000
Ballast	12,000
Platelaying	3,000
Points and crossings	1,000
Track	130,000
Cost of renewals and repairs of girders, both tracks	130,000
Floors of same, ditto, ditto	2,700
Destruction of Fenghai shops	1,400
Stores lost therein, ditto	1,200
Plant, ditto, ditto	4,500
Water cranes, pipes, &c..	4,500
3 iron godowns and tracks	350
Losses of sidings and crossings	3,000
Locomotive stores	1,000
Extra length of shed (engine)	200
Repairs and renewals of stations	250
M.C.P. station and yard	2,000
Merchant houses therein	7,000
Main line signals	4,000
Electric tramway and plant	2,000
Main road and steam-roller	20,000
	186,100

(No. 11 (4).)

SCHEDULE (B).—continued.

Value of Works carried out by British Railway Administration for Capital Account, &c.

	£
Chien-Men extension (value of rails included)	32,000
12 miles of rails left in stock	9,720
	<u>41,720</u>
Claim, as per sheet 1	186,100
Less—Value of work done by British Railway Administration on extensions	41,720
	<u>144,380</u>

(No. 11 (5).)

SCHEDULE (C).

Indemnity Claim for Damages done Inside Wall between Tien-tsin and Kuyeh.

	Dollars,
Mr. Tuckey's Report and Estimate	193,658
Extra new iron for roofs	5,812
Mr. Marshall's Statement as to timber at Hisho in 1900—	Dollars,
1900 value	181,200
1902 "	169,000
Loss	<u>12,200</u>
	12,200
Sale of 75-lb. rails to Hiling Railway	209,770
	<u>120,000</u>
Balance due	89,770
	<u>£</u>
At 12 dollars = 1L	7,480

(No. 11 (6).)

SCHEDULE (D).

Indemnity Claim for Losses incurred Outside Wall.

	Dollars,
Damage as per Mr. Cox' Report and Estimate	761,530
Extra for Blake crusher	2,500
Rebuilding of defective piers of bridges	3,500
Sinking of turntable Y. K.	1,600
	<u>768,630</u>
Estimated value of work done by Russians for capital account of railway	155,194
Balance due	613,336
	<u>£</u>
At 10 dollars = 1L	61,333

[1961 g—3]

(No. 11 (7).)

SCHEDULE (E).

Indemnity Claim for Injury done to Telegraph System.

	Taels.
Injury done Inside Wall (see Mr. Huang's Report)	50,415
Injury done Outside Wall	38,905
Total	<u>89,310</u>
At 8 taels = 1L	11,150

(No. 11 (8).)

SCHEDULE (F).

Indemnity Claim for Losses incurred at Shankaikwan Bridge Works.

	£
Loss of materials, tools, and damage to plant, as per Mr. Howard's Report and Estimate	42,928

(No. 11 (9).)

SCHEDULE (X).

Approximate Value of Stock. Tongshan Workshops and Locomotive Sheds.

	Taels.
12th moon, 25th year of Kuang-Hsu (25th January, 1900) Materials approximately received from 25th January to 17th June, 1900, to meet the requirements for the erection of new rolling stock—	977,044
Inside Wall	67,126
Outside Wall	283,605
Against running store indentants	58,651
	<u>1,386,426</u>
Issues—February, March, April, and May, 1900, 65,000 taels per month	260,000
	<u>1,126,426</u>
Less—Value of stock found on the 1st October, 1903, 1,131,371 dollars at 70 ex.	791,960
	<u>334,566</u>
At ex. 7·50 =	<u>£</u>
	44,702*

* See Schedule (A), Sheet 2.

F

(No. 12.)

	£
Advanced to British Railway Administration by British Government, for replacement and renewals caused by Boxer damage..	90,000

(No. 13.)

IMPERIAL RAILWAYS OF NORTH CHINA.

Indemnity Account. Losses at Head Office, Tien-tsin.

	Tails.
Light rails and cars	14,000
Furniture, safes, &c.	10,000
Stationery, &c.	5,000
Dollars and fractional coins	18,000
	<hr/> 47,000
At 70 ex. =	£ 6,714

(No. 14.)

Miscellaneous Taxes.

	Dollars.	£ s. d.
1. Material for road bridge over Peiho, taken at 50 per cent. of Invoice No. 435 for 635L 2s.	..	317 12 0
2. Coal delivered to brick contractor at Kaipantze, the property of Railways. Total lost, 200 tons, at 6 dollars.	1,200 00 ex. at 10 =	120 0 0
3. Kwo Hsing Chen's store account at Yang-ts'un ..	357 39 "	35 14 6
4. Suen Ming Yu's claims for houses at Chao Chia Tun, occupied by Mr. McKenzie, destroyed by Boxers. Estimated value	3,500 00 "	350 0 0
5. Honorarium to volunteer corps at Tong-anhan	2,000 00 "	200 0 0
6. Pay of men working at car repairs in Feng-tai, 1st day, 5th moon, 5th year of Kuang-Hsu, 26..	157 88 "	15 15 0
7. Pay and passage money of Shan Singh, &c., station police ..	495 00 "	49 10 0
	<hr/> 1,088 11 6	
Inside Wall	618 11 6	
Outside Wall	470 0 0	
	<hr/> 1,088 11 6	

(No. 15.)

SCHEDULE (C).

Compensation to Staff which was retained in the East ready for immediate re-employment under British Railway Administration Service.

Names.	Particulars.	Amount.	Total.
Barber, J.	Five months' half-pay from August to December, at 11L per month = 55L House allowance, nine months, at 150 dollars ..	Dollars. 550·00 1,350·00	Dollars.
Cox, A. G.	Was paid full salary. House allowance, nine months, at 150 dollars Less 350 dollars, drawn at Yingkow ..	1,350·00 350·00	1,900·00
Cheyne, J. J.	Five months' half-pay, at 10L = 50L House allowance, nine months, at 150 dollars ..	500·00 1,350·00	1,850·00
Engstrand, H.	Three months at half-pay, at 13L per month = 39L .. Nine months' house allowance, at 150 dollars ..	390·00 1,350·00	1,740·00
Foley, J. E.	Received full pay. Nine months' house allowance, at 150 dollars ..	1,350·00	1,350·00
Harris, F. A.	Five months' half-pay, at 17L 10s. per month = 87L 10s. .. Nine months' house allowance, at 150 dollars ..	875·00 1,350·00	2,225·00
Howard, W. G.	Five months' pay, at 22L 18s. 4d. = 114L 11s. 8d. House allowance, nine months, at 150 dollars ..	1,145·82 1,350·00	2,495·82
Jamieson, F. A.	Five months' half-pay, at 20L 16s. 8d. = 104L 3s. 4d. .. House allowance, nine months, at 150 dollars ..	1,041·62 1,350·00	2,391·62
Kitching, F.	Two months at half-pay, at 11L = 22L .. House allowance, nine months, at 150 dollars ..	220·00 1,350·00	1,570·00
Kinder, C. W.	House allowance, nine months, at 150 dollars ..	1,350·00	1,350·00
Leitch, W. O.	As per claim, 106L 1s., at 10 dollars ..	1,060·50	1,060·50
Martin, J. C.	As per claim, 109L 13s. 8d. And tacks, 100 at 70 ..	1,096·83 143·00	1,239·83
Marshall, H. J. W.	Five months' half-pay, at 20L 16s. 8d. = 104L 3s. 4d. .. House allowance, nine months, at 150 dollars .. Passage-money unpaid, 50L ..	1,041·62 1,350·00 500·00	2,891·62
Moffat, J.	House allowance, nine months, at 150 dollars ..	1,350·00	1,350·00
Moore, J.	House allowance, nine months, at 150 dollars ..	1,350·00	1,350·00
Moorhead, H. B.	Five months' half-pay, at 8L 6s. 8d. = 41L 13s. 4d. .. Half passage, 25L ..	416·62 250·00	666·62
Maclelland, J.	Three months' house allowance, at 150 dollars Went on leave home.	450·00	450·00

Names.	Particulars.	Amount.	Total.
Newmarch, L. J. . .	Five months' half-pay, at 25 <i>l.</i> = 125 <i>l.</i> . House allowance, nine months, at 150 dollars	Dollars. 1,250·00 1,350·00	Dollars. 2,600·00
Rathnell, W. . .	Five months at half-pay, at 5 <i>l.</i> 10 <i>s.</i> = 27 <i>l.</i> 10 <i>s.</i> . House allowance, nine months, at 150 dollars	275·00 1,350·00	1,625·00
Reekerby, J. . .	On full pay drawn from military. House allowance, nine months, at 150 dollars	1,350·00	1,350·00
Ricketts, D. P. . .	Paid by military. House allowance, two months, at 150 dollars..	300·00	300·00
Regby, E. H. . .	Received full pay. House allowance, nine months, at 150 dollars. Deduct 350 dollars, drawn at Yingkow ..	1,350·00 350·00	1,000·00
Roberts, H. . .	Three months' full pay, at 120 dollars .. Two months' half-pay, at 60 dollars .. House allowance, nine months, at 90 dollars..	360·00 120·00 810·00	1,290·00
Sheriff, A. . .	Received full pay. No house allowance.		
Tuckey, T. W. T. . .	Fully paid.		
Wheeler, A. . .	Five months' half-pay, at 9 <i>l.</i> 10 <i>s.</i> = 47 <i>l.</i> 10 <i>s.</i> . House allowance, nine months, at 150 dollars	475·00 1,350·00	1,825·00
Whitecombe . . .	Full pay to October, paid by military. House allowance, nine months, at 150 dollars	1,350·00	1,350·00
Messrs. Irwin . . .	Half-pay from 31st July, 1900, to 28th February 1901, to date of British Railway Administration handing over to Imperial Chinese Railway, 7 months at 20 <i>l.</i> per month, 140 <i>l.</i> . . .	1,400·00	1,400·00
	Total . . . . .	..	39,621·00

Inside Wall	Outside Wall	Dollars.	£
..	..	31,730·68 =	3,173
..	..	7,890·33 =	789
			3,962

(No. 16.)

SCHEDULE (C).

Staff remaining in East, but not employed by Imperial Chinese Railway or British Railway Administration.

Names.	Particulars.	Amount.	Total.
Brown, H. H. . .	Five months in lieu of notice, at 12 <i>l.</i> 10 <i>s.</i> = 62 <i>l.</i> 10 <i>s.</i> .. Six months' house allowance, at 150 dollars..	Dollars. 625·00 900·00	Dollars. 1,525·00
Dunn, E. C. A. . .	Five and a-half months in lieu of notice, at 16 <i>l.</i> 13 <i>s.</i> 4 <i>d.</i> = 83 <i>l.</i> 6 <i>s.</i> 8 <i>d.</i> .. Was not asked to remain.	833·33	833·33
Dixon, A. . .	Required by re-employed.		
Gibson, R. G. . .	Re-employed by British Railway Administra- tion. Five months' half-pay due in lieu of notice, at 16 <i>l.</i> 13 <i>s.</i> 4 <i>d.</i> = 83 <i>l.</i> 6 <i>s.</i> 8 <i>d.</i> .. Less, 350 dollars drawn at Yingkou ..	833·33 350·00	483·33
Griffith, D. P. . .	Five months' half-pay in lieu of notice, at 20 <i>l.</i> 16 <i>s.</i> 8 <i>d.</i> = 104 <i>l.</i> 3 <i>s.</i> 4 <i>d.</i> ..	1,041·62	1,041·62
Knowles, G. S. . .	Two months at half-pay, at 12 <i>l.</i> 10 <i>s.</i> = 25 <i>l.</i> .. Three months' full pay, at 25 <i>l.</i> = 75 <i>l.</i> ..	250·00 750·00	1,000·00
Krier, Otto . . .	Three months' full pay, at 200 dollars ..	600·00	600·00
Norregard, B. W. . .	Five months' half-pay, at 18 <i>l.</i> 15 <i>s.</i> = 93 <i>l.</i> 15 <i>s.</i> ..	937·50	937·50
Palmer, R. J. . .	Five months' half-pay, at 9 <i>l.</i> 5 <i>s.</i> = 46 <i>l.</i> 5 <i>s.</i> ..	462·50	462·50
	Total .. . . .	..	6,883·28

Inside Wall	Outside Wall	Dollars.	£
..	..	5,104·12 =	511
..	..	1,779·16 =	178
		6,883·28 =	689

(No. 17.)

SCHEDULE (C).

Staff which left the East, not required to Remain.

Names.	Particulars.	Amount.	Total.
	Dollars.	Dollars.	
Bone, T. ..	Five months' full pay in lieu of notice, at 18 <i>L</i> . per month = 90 <i>L</i>	900·00	900·00
Clarke, C. ..	Five months' full pay, at 19 <i>L</i> . = 135 <i>L</i> . ..	1,350·00	1,350·00
Clarke, J. ..	Five months' full pay, at 15 <i>L</i> . = 225 <i>L</i> . ..	750·00	750·00
Currie, O. ..	Six months' pay } 650 <i>L</i>	6,500·00	6,500·00
Dunn, E. C. A. ..	Five months' full pay in lieu of notice, at 18 <i>L</i> . = 90 <i>L</i> . .. Deduct 350 dollars, drawn at Ying-kou ..	900·00 350·00	550·00
Dorrell, H. ..	Five months' house allowance, at 18 <i>L</i> . = 90 <i>L</i> . ..	900·00	900·00
Emmerson, A. ..	Five months at half-pay, at 11 <i>L</i> . 10 <i>s</i> . per month = 57 <i>L</i> . 10 <i>s</i> . .. Five months' house allowance, at 150 dollars ..	575·00 900·00	1,475·00
	Less 350 dollars; was retained at Ying-kou for six months	350·00	1,125·00
Early, W. H. ..	Five months' half-pay, at 12 <i>L</i> . 10 <i>s</i> . = 62 <i>L</i> . 10 <i>s</i> . ..	625·00	625·00
Franklin, H. ..	Five months' full pay, at 18 <i>L</i> . 10 <i>s</i> . = 92 <i>L</i> . 10 <i>s</i> . ..	925·00	925·00
Gennill, J. ..	Five months' pay in lieu of notice, at 58 <i>L</i> . 6 <i>s</i> . 8 <i>d</i> . = 291 <i>L</i> . 13 <i>s</i> . 4 <i>d</i> . .. Less, 350 dollars, drawn at Ying-kou ..	2,916·62 350·00	2,566·62
Hurst, F. ..	As per Claim, 631 <i>L</i> . 18 <i>s</i> . 4 <i>d</i> . ..	6,319·16	6,319·16
Hancock, G. W. ..	Five months' full pay in lieu of notice, at 22 <i>L</i> . = 110 <i>L</i>	1,100·00	1,100·00
Hoyle, A. B. ..	Five months at half-pay, at 18 <i>L</i> . 15 <i>s</i> . = 93 <i>L</i> . 15 <i>s</i>	937·50	937·50
Jackson, J. E. ..	Five months' full pay, at 33 <i>L</i> . 6 <i>s</i> . 8 <i>d</i> . = 165 <i>L</i> . 18 <i>s</i> . 4 <i>d</i> . .. Less, 350 dollars, drawn at Ying-kou ..	1,666·62 350·00	1,316·62
Mackenzie, A. C. ..	Five months at half-pay, at 16 <i>L</i> . 13 <i>s</i> . 4 <i>d</i> . = 83 <i>L</i> . 6 <i>s</i> . 8 <i>d</i> . .. Less, 350 dollars, paid at Ying-kou ..	833·33 350·00	482·33
Robbie, O. ..	Five months' half-pay, at 9 <i>L</i> . = 45 <i>L</i> . .. Less, 350 dollars, drawn at Ying-kou ..	450·00 350·00	100·00
Simmon, G. ..	Five months' half-pay at 12 <i>L</i> . 10 <i>s</i> . = 62 <i>L</i> . 10 <i>s</i> . Less, 350 dollars, drawn at Ying-kou ..	625·00 350·00	275·00
Statham, E. E. ..	Five months' full pay, at 33 <i>L</i> . 6 <i>s</i> . 8 <i>d</i> . = 165 <i>L</i> . 13 <i>s</i> . 4 <i>d</i>	1,666·66	1,666·66

Names.	Particulars.	Amount.	Total.
	Dollars.	Dollars.	
Plowman, W. ..	Five months' full pay, at 20 <i>L</i> . = 100 <i>L</i> . ..	1,000·00	1,000·00
Packham, R. ..	Was engaged, but never served on railway, and during trouble was paid passage. Say, two months' pay as compensation, at 18 <i>L</i> . = 36 <i>L</i> . .. Half-month's house allowance	360·00 75·00	435·00
Stafford, W. R. ..	Five months' full pay in lieu of notice, at 18 <i>L</i> . 10 <i>s</i> . = 92 <i>L</i> . 10 <i>s</i> . ..	925·00	925·00
Strong, J. ..	Six months' pay in lieu of notice, decreased relative claim, at 25 <i>L</i> . = 150 <i>L</i> . ..	1,500·00	1,500·00
Symonds, T. ..	Five months' full pay in lieu of notice, at 26 <i>L</i> . = 130 <i>L</i> . ..	1,300·00	1,300·00
Terrie, R. ..	Five months' full pay in lieu of notice, at 30 <i>L</i> . = 150 <i>L</i> . ..	1,500·00	1,500·00
Tumber, R. S. ..	Five months' half-pay, at 9 <i>L</i> . 5 <i>s</i> . = 23 <i>L</i> . 2 <i>s</i> . 6 <i>d</i> . ..	231·25	231·25
	Total .. .. ..	..	35,281·14

Inside Wall ..	Outside Wall ..	Dollars.	£ s. d.
		27,197·91 = 8,083·23 =	2,719 16 0 808 6 6
		35,281·14 =	3,528 2 6

(No. 18.)

Wages and Compensation.

For wages and compensation to coolies accompanying Seymour expedition ..	Dollars.	£
..	4,019·98	402

(No. 19.)

Contingent Fund.

For this amount set aside for contingent purposes ..	Taels.	£
..	50,000·00	7,500

(No. 20.)

British and Chinese Corporation.

British and Chinese Corporation claim for losses at Nanpiac. (See Reference No. 1)	£ s. d.
..	8,980 1 3

Imperial Chinese Railways, debtor to British and Chinese Corporation (Limited).

	£ s. d.
For amount of claim on account of losses in connection with the bearing operations at Nanpiao as presented to His Britannic Majesty's Claims Commissioner	12,012 7 4
Plus claimed for damages, i.e., interest on money, expenses of general and head office, Administration charges, postponement of profit, &c., say, 15 per cent.	1,801 17 1
Less amount of indemnity allowed by Claims Commissioner under the assumption that a portion of the plant remained uninjured	13,814 4 5
	4,834 3 0
	8,980 1 5

This claim is based upon information of the total destruction of everything by native soldiers or brigands early in 1901 (see copies of correspondence attached).

(For Jardine, Matheson, and Co.).

(Signed) ED. COUSINS.

(No. 21.)

This item of 4,000*l*. represents, according to Mr. Kinder's estimate, the cost of a resurvey of the railway lands.

This resurvey is made necessary by the loss or destruction of all the title-deeds which were kept in the safes at the head office in Tien-tsin. These deeds were the railway's sole evidence of title, as there seems to have been no official registration of the lands from time to time acquired by the railways.

The British and Chinese Corporation, as mortgagee, is interested in the re-establishment of the title for the purposes of the security, and this can be accomplished only upon the basis of a resurvey.

It is submitted, therefore, that the expense in question is a direct consequence of the Boxer uprising, and may properly be included in the indemnity claim.

AFFAIRS OF CHINA.

[May 23.]

CONFIDENTIAL

SECTION 3.

[No. 1.

Mr. Townley to the Marquess of Lansdowne.—(Received May 23.)

(No. 138.)

My Lord,

Peking, April 9, 1903.

I HAVE the honour to transmit herewith to your Lordship, in original, and with reference to your despatch No. 42 of the 13th February last, a detailed Statement of the Northern Railways claim as furnished to me by Mr. Cousins.

Your Lordship will perceive that the total claim for both intra- and extra-mural lines amounts to 448,836 13*s. 9d.*, to which will have eventually to be added a sum of 4,000*l*., the estimated cost of the resurvey of the line which will have to be made in order to establish the titles to lands of which the deeds were lost in the destruction of the head office at Tien-tsin. The above-mentioned sum, divided, I am told, as well as may be between the two lines, is made up of :—

	£ s. d.
Intra-mural line	319,680 18 10
Extra-mural line	121,655 14 11
Contingent fund	7,500 0 0
Total	448,836 13 9

I understand that a fuller and more detailed examination of the extra-mural line than Mr. Kinder had been able to make before he furnished Sir E. Satow with the information supplied in his despatch No. 320 of the 29th October last, has caused him to place the Engineering Department's claim for damage to that line at 104,010*l*. Only a small portion of this last figure covers damage to rolling-stock, and that chiefly applicable to engines, since it was not possible to make a full separate allocation of rolling-stock to the one line or the other, and where separation has not been made the rolling-stock and supplies have been attributed to the intra-mural line.

Mr. Cousins tells me that it is possible that the sum of 300,000*l*. claimed by His Majesty's Government on account of the extra-mural line will still be sufficient to cover both the present actual claim and the amount payable by the Chinese to the Russian Government which, he is given to understand, amounts to about 150,000*l*., although, as far as he can ascertain, no accounts have as yet been submitted by the Russian Railway Administration. I am addressing a letter on this point to the Chinese Northern Railways Administration, and shall have the honour to submit their reply to your Lordship at a later date.

From the appended Statement, it would, however, appear that the sum of 500,000*l*. claimed on account of the intra-mural line would prove sufficient to cover the losses sustained on both lines, if it should be found possible to devise some means of meeting the very large discount to be encountered in negotiating Chinese Government indemnity bonds. Mr. Cousins has represented to me that the Railway Administration is in urgent need of immediate funds to enable it to carry out pressing repairs and to replenish its rolling-stock, and that the requisite funds are only forthcoming out of the Indemnity claim, which, however, in the shape of bonds, is not a negotiable factor except at a ruinous discount. He will write to me further upon the subject, and I will submit his communication to your Lordship upon its receipt.

Mr. Cousins points out that the sum of 30,000*l*. referred to in your Lordship's despatch under reply, on account of extensions to the Temple of Heaven and Peking, and from Peking to Tungchow, was specifically applied to the extensions and was drawn from loan funds, no claim on this account being made in the statement of claim, in which your Lordship will see that the sum of 90,000*l*. advanced by His Majesty's Government is duly included.

I have been carefully through the statement of claims with Mr. Cousins and Mr. Allen, and would beg to submit that I have found it, as far as I am able to judge, a reasonable one. The contingent fund of 7,500*l*. seems to be a necessary precautionary measure to take, and if the money is not required it will not be used.

[1961 2-3]

B

References 15, 16, and 17 deal with claims on behalf of the railway staff, both those retained in China and those who have left the East. The names of all the Europeans employed on the railway at the time of the troubles figure on these lists, and where claims have been made by them the full amount of claim is given, although, in most cases, the claim is subject to reduction. The large majority of those employed by the railway who have left the East have made no claim at all, so that it is possible that the sum of close upon 9,000*l.*, at which the amount of possible compensation has been fixed may be somewhat reduced. A Notification will be issued shortly in the London "Times" that all claimants of this class must make application for payments due to them before a certain date, probably six months after publication of the Notice. It is clear that, without such public announcement, the Railway Administration could not be sure for an indefinite time of the full amount of its liabilities.

I should be glad to be furnished with a copy of the statement of claims if the document is printed, as press of work has prevented a copy being made here.

I have, &c.
(Signed) WALTER TOWNLEY.

P.S.—Since writing the above, I have received an amended statement from Mr. Cousins, which includes the 4,000*l.* on account of resurvey, together with a Memorandum explaining the necessity for the same. In transmitting these documents to your Lordship, I have also the honour to inclose copy of a letter just received from Mr. Cousins, covering the railway claim, pointing out that the Railway Administration is seriously hampered by want of funds, and expressing a hope that His Majesty's Government will come to an early decision as to when, and how, the final claim allowed will be made available, and in what amounts.

W. T.

Inclosure 1 in No. 1.

Mr. R. Cousins to Mr. Townley.

Sir,

REFERRING to the joint communication from Mr. Hewat and myself of the 27th April last, I now beg to hand you herewith, for the information of His Majesty's Government, a copy of the railway claim, as amended, amounting to 452,836l. 13s. 9d. The Administration holds, of course, the necessary documents.

The Administration holds, of course, the necessary documents and vouchers in support of the various items constituting the claim, and is prepared to produce these for verification whenever called upon, to whoever may be deputed by His Majesty's Government to examine and deal with same.

The Administration would, however, suggest the expediency of the examination being conducted in Tien-tsin, where such further evidence as may be required is more easily accessible.

I avail myself of this opportunity to bring to your notice the fact that, in consequence of the large expenditure already incurred in connection with the work of restoring the line to its anti-Boxer condition, the Railway Administration finds itself seriously hampered by want of funds.

Referring to my communication of the 26th January, I venture to express the hope that His Majesty's Government will come to a decision as soon as possible when, and how the final claim allowed will be made available and in what amounts.

I understand [that, owing to questions at issue between the Powers and Chinese
Government] that the investigation of Chinese officials has been suspended.

Government with regard to the interpretation of Clause 6 of the Protocol, delay has occurred in the preparation and signature of bonds representing the British share of the indemnity, and it will be obvious to you that if this delay is likely to be protracted, and the settlement of the railway claim has to await the decision of the questions referred to, serious embarrassment will be caused to the Administration, not only with regard to the claims of firms and individuals, for which it is continually being pressed, but also with regard to progress of work on the line.

In view of this I have to express the hope that His Majesty's Government will see its way to arrange for the examination and settlement of the claim as soon as possible.

I have, &c.

(Signed) ED. COUSINS,
Representing British and Chinese Corporation (Limited).

DETAILED Statement of the Northern Railway Claim.
Inclosure 2 in No. 1.

SUMMARY of Claim.

		Inside Wall	Outside Wall	Total.
		£ s. d.	£ s. d.	£ s. d.
1	Judine, Matheson, and Co. And 22,067/-7d. as 7 tides	As per statement	£ 1,351 16 5 3,665 6 0	5,117 2 5
2	Thames Ironworks.	Interest for three years, from 1st July, 1900, to 1st July, 1903, on their account, 3,832/- 9s. 11d., at 7 per cent, per annum (for material landed at Tilbury)	**	**
3	Pearson, Daniel, and Co.	Interest due on their account for 9,500/- for wheels and axles for tenders stored in England for three years, from 1st July, 1900, to 1st July, 1903, at 3½ per cent, per annum	**	**
4	John Birchall & Co., Limited.	For boiler tubes and auxiliary materials ex steamship "Ching-woo"	262 10 0	262 10 0
5	Mackenzie and Co.	For 100 tons of iron	766 13 4	766 13 4
6	American Trading Company.	For 1,991 tons of iron	69 5 9	69 5 9
7	Arbaldi, Karberg, and Co.	For 1,991 tons of iron	**	**
8	Ching Shan Bank.	For three years' interest, from 1st July, 1900, to 1st July, 1903, on £15,770 taels = \$1,647.70 tads, at 7 per cent, per annum	**	1,097 4 7
9	Foung Sheng Bank.	For three years' interest, at 7 per cent, per annum, from 1st July, 1900, to 1st July, 1903, on \$1,000 dollars = interest, \$148 dollars, or at 10 dollars = $\frac{1}{12}$ tael	**	452 0 0
10	Freight and steamer charges on cargo stored in Shanghai, and brought to Hsido in December 1902 by steamships "Ningpo" and "A Changchow".	**	148 0 0	148 0 0
11	Summary of claim of engineering department, as per Mr. Kinder's statement.	**	4,350 14 5	4,350 14 5
12	Amount advanced by British Government for replacement and removal caused by Boer damage	205,161 0 0	104,010 0 0	313,171 0 0
13	Leases at head offices, Tientsin.	**	90,000 0 0	90,000 0 0
14	Miscellaneous claims, as per statement.	**	67,14 0 0	67,14 0 0
15	Compensation claim by British Railway Administration	**	618 11 6	1,088 11 6
16	Cost of repairing and repairing of railway lines	**	3,173 0 0	3,962 0 0
17	Staff remaining in the East, but not employed by British Railway Administration	**	511 0 0	639 0 0
18	Staff which left the East, services not being required	**	27,19 16 0	35,28 2 6
19	Wages and compensation to coolies accompanying Seymour expedition, 4,019·98 dollars.	**	402 0 0	402 0 0
20	Congtington Fund	**	**	**
21	British and Chinese Corporation claim for leases at Amphio	7,500 0 0	8,980 1 5	16,480 1 5
22	Estimated cost of recovery of railway lands	**	**	**
23	Total	7,600 0 0	31,060 18 10	38,660 0 0

(Signed) H. J. W. MARSHALL. Secretary, *Imperial Bank of North America*.

REVIEW OF 22 EXPERIMENTS ON THE CONTROL OF ANOTIA COTULAE

(N6. 1.)

Tien-tsin, December 17, 1902.

Dear Sir,
 Referring to our communication of the 1st ultimo, and documents attached, we now beg to hand you herewith an amended Statement of telegraphic expenditure, which please substitute for the one rendered.

Our claims are now summarized as follows:—

	Taela.
Amounts due for material lost through the Boxer troubles in 1900, together with interest thereon, as per separate Statement	20,344.80
	1,712.90
Expenditure on telegrams, as per Statements attached	£ s. d.
British and Chinese Corporation's claim for losses at Nampino, less amount allowed by His Britannic Majesty's Claims Commissioners	1,951 16 5 and
	8,880 1 5
	10,931 17 10 and
	22,057.70

We are, &c.

(For Jardine, Matheson, and Co.),
(Signed) ED. COUSINS.M. T. Liang, Esq., Director,
Imperial Chinese Railways.

SUMMARY of Amounts due by the Imperial Chinese Railways to the Undersigned, on account of Material imported under their Indents and lost in consequence of the Boxer troubles of 1900, also of Interest as per Statements attached.

A/c Indent	No.	Taela.
"	32	4,457.31
"	38	694.17
"	1603 (part)	1,612.27
"	1603/1608	5,174.39
"	1610	338.56
"	1676	5,230.31
"	6192, 14s. 9d., at 2s. 4½d.	2,837.77
	Total	20,344.80

(E. and O. E.)

(For Jardine, Matheson, and Co.),
(Signed) ED. COUSINS.

Tien-tsin, November 1, 1902.

IMPERIAL CHINESE RAILWAYS.

Indent No. 32 (part), London Invoice No. 68, due April 10, 1900.

	£ s. d.	£ s. d.
5 cases split and copper pins	.. .. .. .. ..	.. .. .. .. ..
53 " iron rivets	.. .. .. .. ..	.. .. .. .. ..
3 " brass sheets	.. .. .. .. ..	.. .. .. .. ..
3 " copper sheets	.. .. .. .. ..	.. .. .. .. ..
2 " copper tubes	.. .. .. .. ..	.. .. .. .. ..
1 case copper and brass tubes	.. .. .. .. ..	.. .. .. .. ..
Freight and bill of lading	.. .. .. .. ..	38 0 4
Insurance on 770L, st 12s. 6d., and stamp	.. .. .. .. ..	4 18 3
Bill stamp	.. .. .. .. ..	0 7 0
		43 5 7
		702 9 6
At ex. 2s. 4½d. ..		Taela.
Plus interest from 10th April, 1900, to 31st December, 1902, = 995 days, at 7 per cent. per annum		5,941.71
	
Less amounts rec'd. from the British Railway Administration for portions of above material taken over by them, plus interest calculated thereon as per Memo. at foot		1,135.81
	
Balance due by the Imperial Chinese Railways	.. .. .. .. ..	7,076.52
		2,618.21
	
		4,457.31
MEMO.		
1901. October. Amount received from British Railway Administra-	Taela.	Taela.
Add—Interest from 1st November, 1901, to 31st December, 1902, = 14 months, and 7 per cent.	1,339.24	
	109.37	1,448.61
1902. April. Amount received from British Railway Administra-		
Add—Interest from 1st May, 1902, to 31st December, 1902 = 8 months, at 7 per cent.	1,117.45	
	52.15	1,169.60
Total	.. .. .. .. ..	.. .. .. .. ..
		2,618.21

(E. and O. E.)

(For Jardine, Matheson, and Co.),
(Signed) ED. COUSINS.

Tien-tsin November 1, 1902.

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IMPERIAL CHINESE RAILWAYS.

Indent No. 38, London Invoice No. 99, due July 19, 1900.

		£	s.	d.	£	s.	d.
2 cases crucibles ..	..	..	..	..	32	15	3
2 " saws and gutters ..	..	..	..	..	56	11	0
24 "	..	..	..	..	..	..	..
1 case ratchet braces ..	..	..	..	..	64	3	5
1 case letter stamps ..	..	..	..	..	53	16	0
2 brass jacks ..	..	..	..	..	1	1	0
Balls-eyes ..	..	..	..	..	0	7	8
Freight and bill of lading ..	..	..	..	..	11	17	3
Insurance on 240L ..	..	..	..	..	1	10	6
Bill stamp ..	..	..	..	..	0	2	0
					13	9	9
					222	4	1
		Taels.					
		1,879·44					
		322·42					
		2,201·86					
At ex. 2s. 4d. ..	..	..	..	..
Plus interest from 19th July, 1900, to 31st December, 1902, = 895 days, at 7 per cent. ..	..	..	..	..
Less amounts received from the British Railway Administration for portions of above taken over by them and interest calculated thereon, as per Memo. at foot ..	..	..	..	..
Balance due by Imperial Chinese Railways ..	..	..	..	..	1,507·69		
		694·17					
MEMO.		Taels.					
1902.							
March. Amount received from British Railway Administra-							
tion ..	..	1,407·20					
Add—Interest from 1st April to 31st December, 1902, = 9 months, at 7 per cent. ..	..	73·88					
		1,481·08					
August. Amount received from British Railway Administra-							
tion ..	..	26					
Add—Interest for 4 months ..	..	0·61					
		26·61					
		1,507·69					

(E. and O. E.)

(For Jardine, Matheson, and Co.),
(Signed) ED. COUSINS.

Tien-tsin, November 1, 1902.

IMPERIAL CHINESE RAILWAYS.

Indent No. 1605 (part), London Invoice No. 85, due April 9, 1900.

	£	s.	d.	£	s.	d.
6 kegs rivets ..	..	..	..	..	..	..
120 iron tubes ..	..	..	..	..	..	..
1 cask bends, &c. ..	..	..	..	..	..	..
1 cask flanges, &c. ..	..	..	..	..	..	..
137 steel tank plates ..	..	..	..	..	..	..
Bill of lading and freight ..	..	..	..	54	6	0
Insurance on 465L ..	..	..	..	21	9	5
Bill stamp ..	..	..	..	4	4	6
				57	9	11
				425	1	5
At ex. 2s. 4d. ..	..	..	..
Plus interest from 9th April, 1900, to 31st December, 1902, = 996 days, at 7 per cent. ..	..	..	..	..	..	..
				659·33		
				4,254·54		
Less amounts received from the British Railway Administration for portions of above material taken over by them, and interest calculated as per Memo. at foot ..	..	..	..	..	..	..
				2,642·27		
Balance due by the Imperial Chinese Railways ..	..	..	..	..	..	..
				1,612·27		
MEMO.						
1901.						
October. Amount received from the British Railway Adminis-						
tration ..	..	425·92				
Interest for 16 months from 1st September, 1901, to 31st December, 1902..	..	39·75				
		465·67				
July. Amount received from the British Railway Adminis-						
tration ..	..	1,980·25				
Interest for 17 months from 1st August, 1901, to 31st December, 1902..	..	196·37				
		2,176·60				
		2,642·27				

(E. and O. E.)

(For Jardine, Matheson, and Co.),
(Signed) ED. COUSINS.

Tien-tsin, November 1, 1902.

IMPERIAL CHINESE RAILWAYS.

IMPERIAL CHINESE RAILWAYS.

Indents Nos. 1605 and 1608 (part), London Invoice No. 87, due April 10, 1900.

		£ s. d.	£ s. d.
18 cases bolt ends ..	..	..	96 10 0
16 " weighing machines ..	..	..	97 6 3
57 bars angle steel ..	..	..	27 19 1
50 cases galvanized iron ..	..	..	401 5 2
40 kegs brown paint oak ..	..	..	..
48 drums drying oil thinning ..	..	..	..
6 " turpentine ..	..	..	..
1 case chalk ..	..	105 0 8	..
1 " pump leathers ..	..	2 3 0	..
1 " padlocks and snaps ..	..	..	..
1 " sledge hammers ..	..	11 1 10	..
13 bundles miners drill steel ..	..	37 17 2	..
Freight and bill of lading ..	..	102 4 10	..
Insurances, &c., ..	..	6 12 9	..
		108 17 7	..
		886 0 9	..
		Taels.	..
		7,494 24	..
		1,450 06	..
		8,924 30	..

At ex. 2s. 4d. =
Plus interest from 10th April, 1900, to 31st December, 1902, =
995 days, at 7 per cent.

Less amounts received from British Railway Administration for portions of above material taken over by them, and interest calculated thereon as per Memo. at foot

Balance due by the Imperial Chinese Railways

Memo.	Taels.	Taels.
1901.		
November. Amount received from British Railway Administra-	356 58	
tion	27 05	
Interest for 13 months from 1st December, 1901, to 31st December, 1902, at 7 per cent.	383 63	
1902.		
Amount received from British Railway Administra-	3,216 19	
tion	150 09	
Interest for 8 months from 1st May, 1902, to 31st December, 1902, at 7 per cent.	3,366 28	
Total	3,749 91	

(E. and O. E.)

(For Jardine, Matheson, and Co.),
(Signed) ED. COUSINS.

Tien-tsin, November 1, 1902.

Indent No. 1610, London Invoice No. 89, due April 10, 1900.

	£ s. d.	£ s. d.
2 cases drawing material
Freight and bill of lading	1 13 11
Insurance on 70L and stamp	0 0 0
Bill stamp	0 1 0
		2 3 11
		62 9 0
		Taels.
		528 21
At ex. 2s. 4d. = Plus interest from 10th April, 1900, to 31st December, 1902, = 995 days, at 7 per cent.	100 79
		Less amount received from British Railway Administration, as per Memo. at foot
		290 42
		Balance due by the Imperial Chinese Railways
		338 58

MEMO.	Taels.	Taels.
1902.		
April. Amount received for part of above from the British Railway Administration	277 47	
Add—Interest from 1st May, 1902, to 31st December, 1902 = 8 months, at 7 per cent.	12 95	
		290 42

(E. and O. E.)
(For Jardine, Matheson, and Co.),
(Signed) ED. COUSINS.
Tien-tsin, November 1, 1902.

IMPERIAL Railways of North China to the Undersigned debtor.

	£ s. d.
For amount due on account of 60-lb. rails delivered ex "Forestdale" and "Macedonia" at Sin-Ho 20th June, 1901, under Indent No. 1576, and contract of 21st September, 1899, viz.—	
Tons. cwt. qrs. lbs.	
Less sales .. 4,701 rails = 1,252 1 3 17 ..	9,772 17 3
3,600 .. 696 8 2 9 ..	5,435 15 5
2,101 .. 555 13 1 9 ..	4,337 1 10*
	7L 16s. 1½d. =
Plus interest, at 7 per cent. per annum, as follows:— On 2,195L 4s. 4d. from 20th May, 1901, to 20th September, 1902 = 3 months	619 14 9†
" 3,240L 11s. 1d. from 20th May, 1902, to 20th February, 1902 = 8 months	4,956 16 7
" 4,337L 1s. 10d. from 20th May, 1901, to 20th November, 1902 = 17 months	41,650 38

(E. and O. E.)
(For Jardine, Matheson, and Co.),
(Signed) ED. COUSINS.
Tien-tsin, October 8, 1902.

* Paid in London.
† Outstanding 619L 14s. 9d. = 5,230 31 taels.
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D

STATEMENT, showing Interest due to the Undersigned on Railway Material taken over by the British Railway Administration.

Indent No.	Number of Packages,	Material,	Vessel,	Amount of Invoice	Due Date,	When Paid,	Period,	Interest,
32 {	17	Rivets ..	"	788.47	May 9, 1900	October 31, 1901	Days.	Tack.
34	1	Bolt ends ..	"	1,307.88	April 10, "	October 31, "	n	81-81
1605	1	Sledge hammers ..	"	145.16	August 29, "	November 29, "	570	142-97
1608 {	3	Zinc plates ..	"	178.94	June 14, "	March 11, 1902	636	12-75
38 {	4	Vines ..	"	652.63	August 29, "	11, n	21-83	
24	1	Steel springs ..	"	977.81	July 19, "	11, n	560	70-09
1618	2	Vines ..	"	908.29	July 14, "	11, n	601	112-70
217	11	Bolting ..	"	3,222.40	August 29, "	11, n	606	105-56
1617	11	Shovels, picks, &c.	"	1,006.35	July 19, "	11, n	688	363-38
38 {	1	Cudgels ..	"	375.11	19, "	11, n	601	122-79
1605	192	Saws and cut	"	3,428.07	April 10, "	May 12, "	43-24	501-63
1608	1	Material ..	"	262.39	10, "	12, n	763	33-43
1610	2	Driving material ..	"	1,084.90	10, "	12, n	763	138-75
1624	25	Rivets ..	"	1,011.66	10, "	12, n	763	132-34
92	3	Bolts ..	"	636.60	June 23, "	June 23, "	764	87-16
1605	176	Bars, iron ..	"	1,968.81	May 5, "	July 11, 1902	730	81-16
295	1	Spikes ..	"	3,134.83	May 5, "	August 12, "	825	111-06
1605 {	137	Tin plate ..	"	1,144.83	May 5, "	12, n	835	313-25
1,342	1	Bars, iron ..	"	12.56	July 19, "	12, n	825	49-00
38 {	1	Letter stamps ..	"			12, n	765	1-84
		McMahon ..	"			13, n		..
		McMahon ..	"					2,837-77
		Total ..	"					..

(For Jardine, Matheson, and Co.),
(Signed) ED. CONISTS

[E_x and O. E_x)

Tien-tsin, November 1, 1902.

	<i>L</i>	<i>s.</i>	<i>d.</i>
To interest for three years, from 1st July, 1900, to 1st July, 1903, on their account 3,832 <i>l.</i> 9 <i>s.</i> 1 <i>ld.</i> , at 3 <i>1/2</i> per cent. per annum, for materials landed at Taliewan	402	8	0

(Signed) (Thames Iron Works.)

(Sign)

£ s. d.

James Iron Works.

(No. 3.)	
Fearon, Daniell and Co.	
To interest on their account from 5th April, 1901, for 677 packages steel bars, angles, plates, and rivets stored at Hisho: • • • • • •	£ s. d. 175 19 10

(No. 3.)

Fearon, Daniell and Co.

£ s. d.

(No. 4.)	
John Birch and Co. (Limited).	
To interest on their account for 2,500L, for wheels and axles for tenders, stored in England, for three years, from 1st July, 1900, to 1st July, 1903, at 3½ per cent. per annum	<i>f. s. d.</i> 262 10 0

(No. 4.)

John Birch and Co. (Limited).

£ s. d.

(No. 5.)	
Mackenzie and Co.	
For boiler tubes and boiler covering or "Chingwo," on account of indent 1599, 599 packages	<i>£ s. d.</i> 756 13 4

(No. 5.)

Mackenzie and Co.

£ s. d.

(No. 6.)

The Imperial Chinese Railways of North China, debtor to the American Trading Company.

100 cases 16 x 12 window glass, at 4·85 taels	..	Taels. 485
485 taels = ..	..	£ a. d. 69 5 9

(No. 6.)

The Imperial Chinese Railways of North China, debtor to the American Trading Company.

Tien-tsin, December 4, 1899.

(No. 7.)

Arnhold, Karberg, and Co. Contract, May 19, 1900.

		Dollars.
1,291 tons Japanese coal delivered alongside railway wharf, Yingkow, at 7·65 dollars per ton..		10,134·35
Interest, 15th June to 25th September, 1901, 102 days, at 6 per cent.		169·92
		<hr/>
		10,304·27
		<hr/>
		Taela.
At 7½		7,470·60
		<hr/>
At 7 taels		£ s. d.
		1,067 4 7

(No. 8.)

Chin Shan Bank.

	Taels.
To three years' interest, at 7 per cent. per annum, from 1st July, 1900, to 1st July, 1903, on 15,070 taels	3,164.70
At 7 taels	452

(No. 9.)

Foo Fung Sheng Bank.

	Dollars.
To interest, at 7 per cent. per annum, from 1st July, 1900, to 1st July, 1903, on \$8,000 dollars	1,480
At 10 dollars..	£ 148

(No. 10.)

	Dollars.	Taels.
For the following, viz.:—		
Charges on railway material shipped from Shanghai to Tien-tsin per steam-ships "Ningpo" and "Chang Chow"—		
Charges in Shanghai, as per Statement No. 1 	26,291.06	
" No. 2 	9,572.62	
Charges in Tien-tsin—		
Taku Tag and Lighter Co. Voucher No. 10 	3,050.25	
" No. 11 	522.90	
Interest, 18th to 30th December (13 days) 		
Telegrams and portages re "Ningpo" 	8.65	
" " "Chang Chow" 	75.00	
Commission to Captain of "Ningpo," 150 Shanghai taels, at 10½ 		
	147.78	
Commission, 1½ per cent. 		
	3,657.75	36,026.56
	54.87	540.39
	3,712.62	36,566.95
Rate of exchange, 71.4 taels = 1 dollar at 2s. 2½ d. ..		
	£ s. d.	£ s. d.
	294 1 6	4,056 12 11
		£ 4,350 14s. 5d.

Tien-tsin, December 30, 1902.

Received payment :
(Signed) BÜCHHEISTER AND Co. (Limited).

(No. 19 (1).)

Imperial Railways of North China, debtor to Buckminster - 12 -

For the following, viz. ---		Shanghai Taels.
Charges on railway material forwarded per " Ningpo " to Hainho Wharf ---		
Storage, Shanghai Hongkew Wharf Company, as per---		
Voucher No. 1	9,147.69	
" No. 2	273.99	
" No. 3	845.99	
" No. 4	6,419.49	
Wharfage and night and Sunday time, as per Voucher No. 5	626.00	
Stevedore, as per Voucher No. 6	633.90	
350-47 dollars at 74,4125, as per Voucher No. 7 =	260.79	
Freight on account from 14th to 26th November, approxi- mate arrival of steamers at Taku, 10,400 dollars, at 74,4125	7,738.99	
Balance of freight, to be settled on completion of dis- charge (Voucher No. 8) .. .. .. .. ..	.. .. .. .. ..
Marine insurance, £30,000 = 266,666.66 taels at $\frac{1}{2}$ per cent. (Voucher No. 9)	666.67	
Shanghai telegrams, 52,48 dollars, at 74,4125	39.05	
Bill brokerage, $\frac{1}{2}$ per cent.	26,652.11	
	33.92	
	26,685.45	
		Tien-tsin Taels.
At 101 $\frac{1}{2}$	26,291.96	

Tien-tsin, December 30, 1902.

Received payment:
(Signed) BUCHHEISTER AND Co. (Limited).

[1961 z-3]

(No. 11.)

IMPERIAL CHINESE RAILWAYS.

Summary of Claims for Indemnity, Engineering Departments.

Schedule (A).—Rolling-stock	£
" (B)—Tien-tsin to Peking	144,380
" (C)—Tien-tsin to Kuyeh	7,480
" (D)—Outside Wall	61,333
" (E)—Telegraph	11,150
" (F)—Bridge works	42,928
	313,171

(Signed) C. W. KIRDER.

February 9, 1903.

NOTE.—This claim does not include—

- (1.) Compensation to staff.
- (2.) Destruction of head office, Tien-tsin.
- (3.) Cost of storage and reshipping materials from Europe and Shanghai.
- (4.) Interest payable on debts.

(No. 11 (1).)

SCHEDULE (A).

Indemnity Claim for Losses to Cars, Locomotives, and Shops.

	£
Valuation of rolling-stock on June 1900, see attached report	345,000
76 mud cars, Imperial Chinese Railway type	3,040
200 German ditto, ditto	8,000
	355,040
Revaluation of same in October 1902, see attached report	374,300
59 mud cars, Imperial Chinese Railway type	2,360
92 German ditto, ditto	3,680
	380,340
Increased value..	35,800

(No. 11 (2).)

SCHEDULE (A).—continued.

	£
Cost of repairing and rebuilding 500 cars used by Russians, at 30/- each	15,000
Cost of repairing and rebuilding 8 locomotives destroyed by Boxer, 12,000 £.K. lire	9,600
Repairs and rebuilding 26 locomotives partly destroyed by Russians outside Wall	10,400
Loss of materials for car and locomotive building at Tongshan (see attached Schedule X)	44,700
	79,700
Claim amount, as above	79,700
Less—Increase in value of stock due to building of same during British Railway Administration	33,800
Balance due	45,900

(No. 11 (3).)

SCHEDULE (B).

Indemnity Claim for Losses and Damage to Tien-tsin-Peking Railway.

	£	£
80 miles of 2nd track—		
Rails, 12,000 tons, at 7/-	..	84,000
Sleepers ..	..	36,000
Balast ..	..	12,000
Platelaying ..	..	3,000
Points and crossings ..	..	1,000
Track ..	..	130,000
Cost of renewals and repairs of girders, both tracks ..	..	130,000
Floors of same, ditto, ditto ..	..	2,700
Destruction of Fenghai shops ..	..	1,400
Stores lost therein, ditto ..	..	1,200
Plant, ditto, ditto ..	..	4,500
Water cranes, pipes, &c..	..	4,500
8 iron godowns and tracks ..	..	350
Losses of sidings and crossings ..	..	3,000
Extra length of steel (engine) ..	..	1,000
Repairs and renewals of stations ..	..	200
M.C.P. station and yard ..	..	250
Merchant houses therein ..	..	2,000
Main line signals ..	..	7,000
Electric tramway and plant ..	..	4,000
Main road and steam-roller ..	..	20,000
		2,000
		186,100

(No. 11 (4).)

SCHEDULE (B).—continued.

Value of Works carried out by British Railway Administration for Capital Account, &c.

	£
Chien-Men extension (value of rails included) 12 miles of rails left in stock	32,000 9,720
	<hr/>
	41,720
Claim, as per sheet 1	186,100
Less—Value of work done by British Railway Administration on extensions	41,720
	<hr/>
	144,380

(No. 11 (5).)

SCHEDULE (C).

Indemnity Claim for Damages done Inside Wall between Tien-tsin and Kuyeh.

	Dollars.	£
Mr. Tuckey's Report and Estimate ..	193,658	
Extra new iron for roofs ..	3,812	
Mr. Marshall's Statement as to timber at Hisho in 1900—		
1900 value ..	181,200	
1902 ..	169,000	
Loss ..	12,200	12,200
Sale of 75-lb. rails to Hiling Railway ..	209,770	120,000
Balance due ..	89,770	
		<hr/>
At 12 dollars = 1L ..	7,480	

(No. 11 (6).)

SCHEDULE (D).

Indemnity Claim for Losses incurred Outside Wall.

	Dollars.	£
Damage as per Mr. Cox' Report and Estimate ..	761,530	
Extra for Blake crusher ..	2,500	
Rebuilding of defective piers of bridges ..	3,500	
Sinking of turntable Y. K. ..	1,000	
	<hr/>	
Estimated value of work done by Russians for capital account of railway ..	768,530	
	<hr/>	
Balance due ..	155,194	
	<hr/>	
At 10 dollars = 1L ..	613,336	
	<hr/>	
	61,333	

(No. 11 (7).)

SCHEDULE (E).

Indemnity Claim for Injury done to Telegraph System.

	Taels.	£
Injury done Inside Wall (see Mr. Huang's Report) ..	50,415	
Injury done Outside Wall ..	38,905	
Total ..	<hr/>	89,310
At 8 taels = 1L ..	<hr/>	11,150

(No. 11 (8).)

SCHEDULE (F).

Indemnity Claim for Losses incurred at Shanhakwan Bridge Works.

	£
Loss of materials, tools, and damage to plant, as per Mr. Howard's Report and Estimate ..	\$2,928

(No. 11 (9).)

SCHEDULE (X).

Approximate Value of Stock. Tongshan Workshops and Locomotive Sheds.

	Taels.
12th moon, 25th year of Kuang-Hsu (25th January, 1900) ..	977,044
Materials approximately received from 25th January to 17th June, 1900, to meet the requirements for the erection of new rolling-stock—	
Inside Wall ..	67,126
Outside Wall ..	283,605
Against running store indent ..	58,651
Issues—February, March, April, and May, 1900, 65,000 taels per month ..	1,386,426
	<hr/>
Less—Value of stock found on the 1st October, 1903, 1,181,371 dollars at 70 c. ..	260,000
	<hr/>
..	1,126,426
..	791,960
	<hr/>
..	334,566
At ex. 7·50 = ..	<hr/>
	44,702*

* See Schedule (A), Sheet 2.

[1961 §—3]

F

(No. 12.)

	£
Advanced to British Railway Administration by British Government, for replacement and renewals caused by Boxer damage..	90,000

(No. 13.)

IMPERIAL RAILWAYS OF NORTH CHINA.

Indemnity Account. Losses at Head Office, Tien-tsin.

	Tael.	£
Light rails and cars	14,000	
Furniture, safes, &c.	10,000	
Stationery, &c.	5,000	
Dollars and fractional coins	18,000	
	<hr/> 47,000	
At 70 ex. =	£	6,714

(No. 14.)

Miscellaneous Taxes.

	Dollars.	£ s. d.
1. Material for road bridge over Peiho, taken at 50 per cent. of Invoice No. 435 for 635L 2s.	..	317 12 0
2. Coal delivered to brick contractor at Kao-pante, the property of Railways. Total lost, 200 tons, at 6 dollars ..	1,200 00	ex. at 10 = 120 0 0
3. Kwo Hsing Chen's store account at Yang-tsun ..	337.39	" 35 14 6
4. Suen Ming Yu's claim for house at Chao Chia Tun, occupied by Mr. McKenzie, destroyed by Boxers. Estimated value	3,500.00	350 0 0
5. Honorarium to volunteer corps at Tong-shan	2,000.00	200 0 0
6. Pay of men working at car repairs in Feng-tai, 1st day, 8th moon, 5th year of Kuang-Hsu, 26..	157.88	15 15 0
7. Pay and passage money of Shan Singh, &c., station police ..	495.00	49 10 0
	<hr/> 1,088 11 6	
Inside Wall	618 11 6	
Outside Wall	470 0 0	
	<hr/> 1,088 11 6	

(No. 15.)

SCHEDULE (C).

Compensation to Staff which was retained in the East ready for immediate re-employment under British Railway Administration Service.

Names.	Particulars.	Amount.	Total.
Barber, J.	Five months' half-pay from August to December, at 17L per month = 55L House allowance, nine months, at 150 dollars ..	550·00 1,350·00	
Cox, A. G.	Was paid full salary. House allowance, nine months, at 150 dollars Less 350 dollars, drawn at Yingkow ..	1,350·00 350·00	1,900·00
Cheyne, J. J.	Five months' half-pay, at 10L = 50L House allowance, nine months, at 150 dollars ..	500·00 1,350·00	
Engstram, H.	Three months at half-pay, at 13L per month = 39L .. Nine months' house allowance, at 150 dollars ..	390·00 1,350·00	1,850·00
Foley, J. E.	Received full pay. Nine months' house allowance, at 150 dollars ..	1,350·00	1,740·00
Harris, F. A.	Five months' house allowance, at 17L 10s. per month = 87L 10s. .. Nine months' house allowance, at 150 dollars ..	875·00 1,350·00	
Howard, W. G.	Five months' pay, at 22L 18s. 4d. = 114L 11s. 8d. House allowance, nine months, at 150 dollars ..	1,145·82 1,350·00	
Jamieson, F. A.	Five months' half-pay, at 20L 16s. 8d. = 104L 3s. 4d. .. House allowance, nine months, at 150 dollars ..	1,041·62 1,350·00	2,495·82
Kitching, F.	Two months at half-pay, at 11L = 22L .. House allowance, nine months, at 150 dollars ..	220·00 1,350·00	
Kinder, C. W.	House allowance, nine months, at 150 dollars ..	1,350·00	1,350·00
Lcith, W. O.	As per claim, 106L 1s., at 10 dollars ..	1,060·50	
Martin, J. C.	As per claim, 109L 13s. 8d. .. And taels, 100 at 70 ..	1,096·83 143·00	
Marshall, H. J. W.	Five months' half-pay, at 20L 16s. 8d. = 104L 3s. 4d. .. House allowance, nine months, at 150 dollars .. Passage-money unpaid, 50L ..	1,041·62 1,350·00 500·00	1,239·83
Moffat, J.	House allowance, nine months, at 150 dollars ..	1,350·00	
Moore, J.	House allowance, nine months, at 150 dollars ..	1,350·00	
Moorhead, H. B.	Five months' half-pay, at 8L 6s. 8d. = 41L 13s. 4d. .. Half passage, 25L ..	416·62 250·00	666·62
Maclelland, J.	Three months' house allowance, at 150 dollars Went on leave home ..	450·00	450·00

Names.	Particulars.	Amount.	Total.
Newmarch, L. J. . .	Five months' half-pay, at 5 <i>l.</i> 10 <i>s.</i> = 12 <i>l.</i> House allowance, nine months, at 150 dollars	Dollars. 1,250·00 1,350·00	Dollars. 2,600·00
Rathnell, W. . .	Five months at half-pay, at 5 <i>l.</i> 10 <i>s.</i> = 27 <i>l.</i> 10 <i>s.</i> House allowance, nine months, at 150 dollars	275·00 1,350·00	1,625·00
Reekerby, J. . .	On full pay drawn from military. House allowance, nine months, at 150 dollars	1,350·00	1,350·00
Bickett, D. P. . .	Paid by military. House allowance, two months, at 150 dollars..	300·00	300·00
Regby, E. H. . .	Received full pay. House allowance, nine months, at 150 dollars Deduct 350 dollars, drawn at Yingkow ..	1,350·00 350·00	1,000·00
Roberts, H. . .	Three months' full pay, at 120 dollars Two months' half-pay, at 60 dollars House allowance, nine months, at 90 dollars..	360·00 120·00 810·00	1,290·00
Sheriff, A. . .	Received full pay. No house allowance.		
Tuckey, T. W. T. . .	Fully paid.		
Wheeler, A. . .	Five months' half-pay, at 9 <i>l.</i> 10 <i>s.</i> = 47 <i>l.</i> 10 <i>s.</i> House allowance, nine months, at 150 dollars	475·00 1,350·00	1,825·00
Whitecombe . . .	Full pay to October, paid by military. House allowance, nine months, at 150 dollars	1,350·00	1,350·00
Messrs. Irwin . . .	Half-pay from 31st July, 1900, to 28th February 1901, to date of British Railway Administration handing over to Imperial Chinese Railway, 7 months at 20 <i>l.</i> per month, 140 <i>l.</i>	1,400·00	1,400·00
			39,621·00
Inside Wall . . .		Dollars. 31,730·68 =	£ 3,173
Outside Wall . . .		7,890·33 =	789
			3,962

(No. 16.)

SCHEDULE (C).

Staff remaining in East, but not employed by Imperial Chinese Railway or British Railway Administration.

Names.	Particulars.	Amount.	Total.
Brown, H. H. . .	Five months in lieu of notice, at 12 <i>l.</i> 10 <i>s.</i> = 62 <i>l.</i> 10 <i>s.</i> Six months' house allowance, at 150 dollars..	Dollars. 625·00 900·00	Dollars. 900·00
Dunn, E. C. A. . .	Five and a-half months in lieu of notice, at 16 <i>l.</i> 13 <i>s.</i> 4 <i>d.</i> = 83 <i>l.</i> 6 <i>s.</i> 8 <i>d.</i> Was not asked to remain.	833·33	1,525·00
Dixon, A. . .	Required by re-employed.		833·33
Gibson, R. G. . .	Re-employed by British Railway Administration. Five months' half-pay due in lieu of notice, at 16 <i>l.</i> 13 <i>s.</i> 4 <i>d.</i> = 83 <i>l.</i> 6 <i>s.</i> 8 <i>d.</i> Less, 350 dollars drawn at Yingkou ..	838·33 350·00	483·33
Griffeth, D. P. . .	Five months' half-pay in lieu of notice, at 20 <i>l.</i> 16 <i>s.</i> 8 <i>d.</i> = 104 <i>l.</i> 3 <i>s.</i> 4 <i>d.</i> ..	1,041·62	1,041·62
Knowles, G. S. . .	Two months at half-pay, at 12 <i>l.</i> 10 <i>s.</i> = 25 <i>l.</i> Three months' full pay, at 25 <i>l.</i> = 75 <i>l.</i> ..	250·00 750·00	1,000·00
Krier, Otto . . .	Three months' full pay, at 200 dollars ..	600·00	600·00
Norregard, B. W. . .	Five months' half-pay, at 18 <i>l.</i> 15 <i>s.</i> = 93 <i>l.</i> 15 <i>s.</i> ..	937·50	937·50
Palmer, R. J. . .	Five months' half-pay, at 9 <i>l.</i> 5 <i>s.</i> = 46 <i>l.</i> 5 <i>s.</i> ..	462·50	462·50
	Total..	6,883·28	

Names.	Particulars.	Amount.	Total.
Inside Wall . . .		Dollars. 5,104·12 =	£ 511
Outside Wall . . .		1,779·16 =	178
		6,883·28 =	689

(No. 17.)

SCHEDULE (C).

Staff which left the East, not required to Remain.

Names.	Particulars.	Amount.	Total.
		Dollars.	Dollars.
Bone, T. ..	Five months' full pay in lieu of notice, at 18 <i>L</i> . per month = 90 <i>L</i>	900·00	900·00
Clarke, C. ..	Five months' full pay, at 19 <i>L</i> . = 135 <i>L</i> . ..	1,350·00	1,350·00
Clarke, J. ..	Five months' full pay, at 15 <i>L</i> . = 225 <i>L</i> . ..	750·00	750·00
Currie, O. ..	Six months' pay } 650 <i>L</i> . .. Seven months' leave }	6,500·00	6,500·00
Dunn, E. C. A. ..	Five months' full pay in lieu of notice, at 18 <i>L</i> . = 90 <i>L</i> . .. Deduct 350 dollars, drawn at Ying-kou ..	900·00 350·00	550·00
Dorrell, H. ..	Five months' house allowance, at 18 <i>L</i> . = 90 <i>L</i> . ..	900·00	900·00
Emmerson, A. ..	Five months' half-pay, at 11 <i>L</i> . 10 <i>s</i> . per month = 57 <i>L</i> . 10 <i>s</i> . .. Five months' house allowance, at 150 dollars ..	575·00 900·00	1,475·00
	Less 350 dollars; was retained at Ying-kou for six months	350·00	1,125·00
Early, W. H. ..	Five months' half-pay, at 12 <i>L</i> . 10 <i>s</i> . = 62 <i>L</i> . 10 <i>s</i> . ..	625·00	625·00
Franklin, H. ..	Five months' full pay, at 18 <i>L</i> . 10 <i>s</i> . = 92 <i>L</i> . 10 <i>s</i> . ..	925·00	925·00
Gennill, J. ..	Five months' pay in lieu of notice, at 5 <i>L</i> . 6 <i>s</i> . 8 <i>d</i> . = 291 <i>L</i> . 13 <i>s</i> . 4 <i>d</i> . .. Less, 350 dollars, drawn at Ying-kou ..	2,916·62 350·00	2,566·62
Hurst, F. ..	As per Claim, 63 <i>L</i> . 18 <i>s</i> . 4 <i>d</i> . ..	6,319·16	6,319·16
Hancock, G. W. ..	Five months' full pay in lieu of notice, at 22 <i>L</i> . = 110 <i>L</i>	1,100·00	1,100·00
Hoyle, A. B. ..	Five months' half-pay, at 18 <i>L</i> . 15 <i>s</i> . = 93 <i>L</i> . 15 <i>s</i>	937·50	937·50
Jackson, J. E. ..	Five months' full pay, at 33 <i>L</i> . 6 <i>s</i> . 8 <i>d</i> . = 166 <i>L</i> . 13 <i>s</i> . 4 <i>d</i> . .. Less, 350 dollars, drawn at Ying-kou ..	1,666·62 350·00	1,316·62
Mackenzie, A. C. ..	Five months' at half-pay, at 16 <i>L</i> . 13 <i>s</i> . 4 <i>d</i> . = 83 <i>L</i> . 6 <i>s</i> . 8 <i>d</i> . .. Less, 350 dollars, paid at Ying-kou ..	833·33 350·00	483·33
Robke, O. ..	Five months' half-pay, at 9 <i>L</i> . = 45 <i>L</i> . .. Less, 350 dollars, drawn at Ying-kou ..	450·00 350·00	100·00
Simmon, G. ..	Five months' half-pay at 12 <i>L</i> . 10 <i>s</i> . = 62 <i>L</i> . 10 <i>s</i> . .. Less, 350 dollars, drawn at Ying-kou ..	625·00 350·00	275·00
Statham, E. E. ..	Five months' full pay, at 33 <i>L</i> . 6 <i>s</i> . 8 <i>d</i> . = 166 <i>L</i> . 13 <i>s</i> . 4 <i>d</i>	1,666·66	1,666·66

Names.	Particulars.	Amount.	Total.
Plowman, W. ..	Five months' full pay, at 20 <i>L</i> . = 100 <i>L</i> . ..	Dollars. 1,000·00	Dollars. 1,000·00
Packham, R. ..	Was engaged, but never served on railway, and during trouble was paid passage. Say, two months' pay as compensation, at 18 <i>L</i> . = 36 <i>L</i> . ..	360·00 75·00	435·00
Stafford, W. R. ..	Half-month's house allowance .. ..	..	..
Strong, J. ..	Five months' full pay in lieu of notice, at 18 <i>L</i> . 10 <i>s</i> . = 92 <i>L</i> . 10 <i>s</i>	925·00	925·00
Symonds, T. ..	Six months' pay in lieu of notices, decreased relative claim, at 25 <i>L</i> . = 150 <i>L</i>	1,500·00	1,500·00
Terris, R. ..	Five months' full pay in lieu of notice, at 30 <i>L</i> . = 150 <i>L</i>	1,500·00	1,500·00
Tumber, R. S. ..	Five months' half-pay, at 9 <i>L</i> . 5 <i>s</i> . = 23 <i>L</i> . 2 <i>s</i> . 6 <i>d</i> . ..	231·25	231·25
	Total .. .. ..	..	35,281·14

Inside Wall ..	Outside Wall ..	Dollars. 27,197·91 = 8,083·23 =	£ s. d. 2,719 16 0 808 6 6
		35,281·14 =	3,528 2 6

(No. 18.)

Wages and Compensation.

For wages and compensation to coolies accompanying Seymour expedition ..	Dollars.	£
	4,019·98	402

(No. 19.)

Contingent Fund.

For this amount set aside for contingent purposes ..	Tails.	£
	50,000·00	7,500

(No. 20.)

British and Chinese Corporation.

British and Chinese Corporation claim for losses at Nanpiao. (See Reference No. 1) ..	£ s. d.
	8,980 1 5

	£ s. d.
For amount of claim on account of losses in connection with the buring operations at Nanpiao as presented to His Britannic Majesty's Claims Commissioner	12,012 7 4
Plus claimed for damages, i.e., interest on money, expenses of general and head office, Administration charges, postponement of profit, &c., say, 15 per cent.	1,501 17 1
	<hr/>
13,814 4 5	
Less amount of indemnity allowed by Claims Commissioner under the assumption that a portion of the plant remained uninjured	4,834 3 0
	<hr/>
	8,980 1 5

This claim is based upon information of the total destruction of everything by native soldiers or brigands early in 1901 (see copies of correspondence attached).

(For Jardine, Matheson, and Co.),
(Signed) ED. COUSINS.

(No. 21.)

This item of 4,000*L*. represents, according to Mr. Kinder's estimate, the cost of a resurvey of the railway lands.

This resurvey is made necessary by the loss or destruction of all the title-deeds which were kept in the safes at the head office in Tien-tsin. These deeds were the railway's sole evidence of title, as there seems to have been no official registration of the lands from time to time acquired by the railways.

The British and Chinese Corporation, as mortgagee, is interested in the re-establishment of the title for the purposes of the security, and this can be accomplished only upon the basis of a resurvey.

It is submitted, therefore, that the expense in question is a direct consequence of the Boxer uprising, and may properly be included in the indemnity claim.

DRAFT OF PROPOSED SCHEME FOR FINANCING THE CHINESE

NORTHERN RAILWAYS INDEMNITY.

For the purposes of this Draft Proposal it has been assumed that the Northern Railways Indemnity has been allowed at £450,000, which sum includes the claim of H.M.G. £90,000.

1. The sum of £800,000 of the British General Indemnity originally set apart to cover the Northern Railways Indemnity Claim shall be retained by the British Government as security for a loan to be raised to provide in cash the amount of the claim allowed, namely £450,000. The bonds representing the sum of £800,000 above mentioned are hereinafter referred to as Indemnity Security Bonds.

2. Only the yearly interest, namely £32,000 accruing on the Indemnity Security Bonds will be applied to the interest and amortization of the proposed loan. On the extinction of that loan the Indemnity Security Bonds will be free of all charge on that account, and will then be at the discretionary disposal of H.M.G. and H.M.G. if it so desires may arrange beforehand to return them to the Chinese Govt. for cancellation, in which case the said Indemnity Security Bonds need not participate in

the

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11214
Nanpiao
Land
Survey
Report
for
Northern
Railways
Indemnity
Claim : In defaced statement
Petung
Nan
Laundry
M.O. 138
(Ref. P.O. 11047, Feb. 13)
Recd 3/23/94
Gen. 3/23/94
Enclosed
Enclosed

the annual drawings for redemption of the British General Indemnity. 162

3. H.M.G. will not by any act of its own allow the provisions of Art.6 of the Protocol of 7 Sept, 1901, so far as the same are applicable to the Indemnity Security Bonds, to be modified to their prejudice as such security.

4. The interest on the Indemnity Security Bonds as it accrues shall be handed to the Hongkong & Shanghai Banking Corpn. in London for the credit of the B. and C. Corpns. in trust, to be applied to the service of the proposed loan.

5. The proposed loan shall be evidenced by bonds of the Imp. Railways of N. China for the aggregate sum of £450,000, bearing interest at five per cent per annum, and shall be further secured by a second mortgage on the properties, rents and profits of the Imp. Railways of N. China, subordinated however to the existing first mortgage on the same.

6. It is believed that a loan thus doubly secured may be placed on the market at a price which would yield about eighty eight per cent net. The difference between this price and the par value of the bonds, namely twelve per cent, would be approximately covered by the interest which at the date of the issue of the new bonds would have already accrued on the

6-2020 1/5 Indemnity

Indemnity Security Bonds. By the 30 June, 1903, the interest accrued on the Indemnity Security Bonds will amount to £48,000 and this is not including £16,000, interest due on 31 December 1901, payment of which is deferred in terms of the Protocol.

Stated differently, after applying the accrued interest, say £48,000 to payment on account of the claim allowed, it is proposed to raise the balance of said claim, namely £402,000 by a loan of a sum nominal sufficient to yield that amount net, or say, in round figures a loan of £450,000,

7. The interest on the Indemnity Security Bonds, namely £32,000 per annum, applied to the service of the second mortgage bonds will on a sinking fund basis extinguish the latter in about twenty five years; that is to say, by means of a half-yearly payment of 3.52581% which, calculated on £450,000 is equal to £15,866, or a total yearly payment of £31,732. The small surplus of say £268 may be applied to defray expenses of the management of the Loan Service.

8. The second mortgage and second mortgage bonds shall contain, among other conditions, a guarantee by the Imp. Railways of N. China to make good any deficiency that may arise in the funds provided as above for the service of the proposed loan.

9.

> - 2020 15

9. The proceeds of the proposed loan shall be paid to the Hongkong & Shanghai Banking Corpn. at London for the credit of the British and Chinese Corpns. in trust, to be applied to the purposes of the Northern Rys. Indemnity, and disbursements of the same shall be made only on the joint warrant of the representative of the British & Chinese Corpns. on the Railway Directorate in Tientsin, and of the Chief Engineer of the Imp. Railways of N. China, whose appointment is provided for by the first mortgage agreement.

(Sd). E.G.Hillier.

Peking. 21 April, 1903.

8-2020 45

Peking.

22 April, 1903.

No 159.

My Lord,

I had the honour to inform Your Lordship in my despatch No 138 of the 9th inst. forwarding a statement of the Northern Railways indemnity claim, that I learned from Mr Cousins, the representative at Tientsin of the British Bondholders, that the railway was in urgent need of funds to carry out necessary repairs, and that it was hoped that His Majesty's Government would come to an early decision as to the method in which the claim should be met.

I would now beg to submit to Your Lordship a scheme for the payment of the Railway Indemnity claim which has been drawn up by Mr E.G. Hillier, the Manager of the Peking branch of the Hongkong and Shanghai Bank, at the request of Mr Cousins.

Mr Hillier

The Marquess of Lansdowne K.G.

etc. etc. etc.

2 - 2020 h 6-

Jan 15/9
Q19
S/22.23.24/13
5,290,000

Mr Hillier points out in the preamble to his scheme that the Railway claim was excluded from the method adopted for the payment of other private claims at the special instruction of His Majesty's Government, which reserved it for later consideration, and that if the British and Chinese Corporation was not in a position to offer a definite estimate of the damage done at the time of the adoption of the general scheme for the payment of British private claims, it was due to natural circumstances, and through no fault of their own.

Mr Hillier further demonstrates that, as far as his information goes, whereas other private claimants were given their choice of an early payment without interest or Chinese Government 4% bonds, the British and Chinese Corporation were given no option on account of the Railway claim, but were allotted bonds, which under any circumstances would be hard to place on the market, but which under the peculiar conditions prevailing may be considered as unmarketable. He also shows that about £30,000 on account of private claims have been included in the Northern Railway indemnity claim by the Claims Commissioner; that if the claimants are made to accept

bonds

3-2020 h.s

and credits will be acknowledged at the earliest possible time
not before both our right holders and their claimants
are called before the arbitration court to determine
what if any amount of damage should be paid.
Mr Hillier has based his scheme upon the assumption
that His Majesty's Government may feel itself in a position
to retain the whole of the £800,000 set aside to cover the
Railways indemnity claim, and he expresses a hope that
should His Majesty's Government feel bound to return to
the Chinese Government any portion of the above mentioned
sum, such portion should not be more than sufficient to
cover the Russian claim on account of the extra-mural
line, and then only in such amount as would represent the
total expenditure of the Russian military authorities on
capital account, after deducting net earnings during their
administration of the line. A full statement of the
negotiations connected with the Russian railway claim
will form the subject of an accompanying despatch.

bonds on account of their claims, they will be hardly
treated as compared with other British private claimants,
whereas if they are to be paid in cash the Railway
Administration will be a heavy sufferer.

For the purposes of his scheme, Mr Hillier has
taken the sum of £450,000, the total amount of the claim
submitted

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submitted to Your Lordship in my despatch No 138 of the 9th inst. on the hypothesis that such claim has been allowed, and he suggests that the £800,000 originally set apart to cover the Railway claim should be retained by His Majesty's Government as security for a loan to raise the cash amount of the claim allowed, hypothetically, and that the yearly interest on the Indemnity bonds, namely £32,000, be applied to the interest and amortization of the proposed loan. Mr Hillier calculates that with the sum of £48,000 which will have accrued as interest on the Railway Indemnity Bonds by the 3rd of June next, the total amount of the loan will have been paid off in about 25 years, and that His Majesty's Government will be free to do what it judges suitable with the £800,000 of bonds earmarked for the Railway claim. Mr Hillier calls attention, in the concluding paragraph of his preamble to the fact that by his scheme the £800,000 of Indemnity bonds on the Railway's account will be excluded from the drawings for redemption of the British Indemnity, while the Chinese Government will benefit by the eventual cancellation of the principal indebtedness represented by them.

In

In submitting the scheme to Your Lordship's consideration, I would beg to remark that £450,000 of Chinese Indemnity bonds would in no way represent that sum in actual fact, and that should the Railway claim be liquidated by an allotment of the mere sum claimed in bonds, the compensation granted, in that it will not be negotiable, will fall far short of the damage done, whilst the Administration will be seriously hampered in the execution of necessary repairs and works by its inability to raise the funds immediately required for the carrying out of the same.

I have the honour to be,

With the highest respect,

My Lord,

Your Lordship's most obedient,

humble Servant,

Walter Townley

Mr. Tawley No. 138 April 9, 03.

" No. 159 April 22, 03.

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We have had to wait until No. 138 could be got into print. The papers contain:

A. The detailed claim of the Northern Railways for the losses incurred inside and outside the wall.

B. Mr. Miller's scheme for the payment of the claim.

A. The total claim is £ 452, 836. 13. 9 but this does not include the claim of the Russian Railway Administration which has not yet been submitted but which is believed to amount to £ 150, 000.

Peking, 22 April 1903.
Mr. Tawley
No. 138, 9 Apr. 03
and No. 159, 22 Apr. 03
are enclosed.
Enclosed also is a copy of the
Northern Railways' Scheme for the payment of the
claim.

See Mr. Miller's scheme
for payment of:-

Peking (China)
Tawley, No. 138.
J.W.L.

492.11.6

of the Headings of the Claim (see page 3). the following seem to me to require consideration.

1. As to which on page 4: an item of £1951 .. 16 .. 5 for telegrams which is not explained.

11. and 12. pages 14 to 18. It is not clear whether the figures under 11. show the gross amount spent to repair the damage done to the line or what was spent in addition to the £90,000 advanced by the British Railways Administration. If they represent the former the £90,000 in 12. is a double charge.

20. This item is as debet to the B. and C. Corporation "for amount of claim on account of losses

losses in connection with the forming operations at Rangipo as presented to H. B. & M. Claims Commr." The claim is for £13,814 . 4 . 5 ~~less 170~~
£4,834 . 3 . 0 allowed by Claims Commr under the assumption that a portion of the plant remained unimpaired.

If the claim has already been before the Claims Commr we should ascertain why it is brought forward again before it is allowed.

It is possible that the B. and C. Corporation may be able to explain these three items. If not we must get the explanations from Mr. Tonley.

B. We must consult the Treasury about Mr. Miller's scheme. There is a good deal of force in Mr. Miller's arguments when he compares the treatment

treatment of the Railway Claim as compared with other
Private Claims. It may be mentioned that Mr. Steller
and Mr. Beris have treated the R.R. Claim, although
it forms part of the Govt. claim, as a private
claim in so far that they have set aside
out of each half yearly payment money for
the payment of the interest on bonds for £800,000
for the R.R. This appears to be fair as the claim
is really private. It is however to be noted
that although Mr. Steller anticipates the payment
out of the £800,000 of the claim of the Russian
Railway Administration he leaves that claim
out of account in his financial calculations.

I conclude that in all contracts have been made,
the account given is in Tientsin.

Q.M. Write to the B. & C. Corporation asking
whether they can give explanations on the three points
mentioned

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mentioned under A and send the
papers to the Treasury for opinion as to B.
telling them what we are doing about the
accounts, & mentioning point as to Russian R.R. Claim.
When we have got the explanations if we
have a further opinion about the figures we
can get the War Office to submit them to the
Officer who was in charge of the British R.R.
Administration in China - if he is available
We shall also have to check the figures
and - I conclude - have the vouchers
examined at Tientsin.

42.

It seems to me very unlikely that the
£90,000 advanced by Kaulow has been charged
twice over, but we can ask. A more serious
question, I think, is how much of the £120,000 is
charged

AFFAIRS OF CHINA.

[June 8.]

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CONFIDENTIAL.

SECTION 5.

No. 1.

Mr. Townley to the Marquess of Lansdowne.—(Received June 8.)

(No. 159.)

Peking, April 22, 1903.

My Lord,
I HAD the honour to inform our Lordship in my despatch No. 138 of the 9th instant, forwarding a statement of the Northern Railways Indemnity Claim, that I learned from Mr. Cousins, the representative at Tien-tsin of the British Bondholders, that the Railway was in urgent need of funds to carry out necessary repairs, and that it was hoped that His Majesty's Government would come to an early decision as to the method in which the claim should be met.

I would now beg to submit to your Lordship a scheme for the payment of the Railway Indemnity Claim, which has been drawn up by Mr. E. G. Hillier, the manager of the Peking branch of the Hong Kong and Shanghai Bank, at the request of Mr. Cousins.

Mr. Hillier points out in the preamble to his scheme that the Railway Claim was excluded from the method adopted for the payment of other private claims at the special instruction of His Majesty's Government, which reserved it for later consideration, and that if the British and Chinese Corporation was not in a position to offer a definite estimate of the damage done at the time of the adoption of the general scheme for the payment of British private claims, it was due to natural circumstances, and through no fault of their own.

Mr. Hillier further demonstrates that, as far as his information goes, whereas other private claimants were given their choice of an early payment without interest or Chinese Government 4 per cent bonds, the British and Chinese Corporation were given no option on account of the Railway Claim, but were allotted bonds, which under any circumstances would be hard to place on the market, but which under the peculiar conditions prevailing may be considered as unmarketable. He also shows that about 30,000*l.* on account of private claims have been included in the Northern Railway Indemnity Claim by the Claims Commissioner; that if the claimants are made to accept bonds on account of their claims, they will be hardly treated as compared with other British private claimants, whereas if they are to be paid in cash the Railway Administration will be a heavy sufferer.

Mr. Hillier has based his scheme upon the assumption that His Majesty's Government may feel itself in a position to retain the whole of the 800,000*l.* set aside to cover the Railways Indemnity Claim, and he expresses a hope that should His Majesty's Government feel bound to return to the Chinese Government any portion of the above-mentioned sum, such portion should not be more than sufficient to cover the Russian claim on account of the extra-mural line, and then only in such amount as would represent the total expenditure of the Russian military authorities on capital account, after deducting net earnings during their administration of the line. A full statement of the negotiations connected with the Russian Railway Claim will form the subject of an accompanying despatch.

For the purposes of his scheme Mr. Hillier has taken the sum of 450,000*l.*, the total amount of the claim submitted to your Lordship in my despatch No. 138 of the 9th instant, on the hypothesis that such claim has been allowed, and he suggests that the 800,000*l.* originally set apart to cover the Railway Claim should be retained by His Majesty's Government as security for a loan to raise the each amount of the claim allowed, hypothetically, and that the yearly interest on the Indemnity Bonds, namely, 32,000*l.*, be applied to the interest and amortization of the proposed loan. Mr. Hillier calculates that with the sum of 48,000*l.*, which will have accrued as interest on the Railway Indemnity Bonds by the 30th June next, the total amount of the loan will have been paid off in about twenty-five years, and that His Majesty's Government will be free to do what it judges suitable with the 800,000*l.* of bonds earmarked for the Railway Claim. Mr. Hillier calls attention, in the concluding paragraph of his preamble, to the fact that by his scheme the 800,000*l.* of Indemnity Bonds on the Railways account will be excluded from the drawings for redemption of the British Indemnity, while the Chinese Government will benefit by the eventual cancellation of the principal indebtedness represented by them.

[2020 h-5]

charges for the extra-mural line
is, or would be, covered by the
£150,000 demanded by the Russians
for the repair of that line; & the B & C
Corporation should be asked whether
they are in a position to supply any
uniform action as to this.

Printed copies of his despatch
will be sent to Mr. Townley.
and will be sent to Mr. Townley.

Drafts

June 30.

Feb 6 June 22/03

In submitting the scheme to your Lordship's consideration I would beg to remark that 450,000*l.* of Chinese Indemnity Bonds would in no way represent that sum in actual fact, and that should the Railway Claim be liquidated by an allotment of the mere sum claimed in bonds, the compensation granted, in that it will not be negotiable, will fall far short of the cost of the damage done, whilst the Administration will be seriously hampered in the execution of necessary repairs and works by its inability to raise the funds immediately required for the carrying out of the same.

I have, &c.
(Signed) WALTER TOWNLEY.

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2. Only the yearly interest—namely, 32,000*l.*—accruing on the Indemnity Security Bonds will be applied to the interest and amortization of the proposed loan. On the extinction of that loan the Indemnity Security Bonds will be free of all charge on that account, and will then be at the discretionary disposal of His Majesty's Government, and His Majesty's Government if it so desires may arrange beforehand to return them to the Chinese Government for cancellation, in which case the said Indemnity Security Bonds need not participate in the annual drawings for redemption of the British General Indemnity.

3. His Majesty's Government will not by any act of its own allow the provisions of Article 6 of the Protocol of the 7th September, 1901, so far as the same are applicable to the Indemnity Security Bonds, to be modified to their prejudice as such security.

4. The interest on the Indemnity Security Bonds as it accrues shall be handed to the Hong Kong and Shanghai Banking Corporation in London for the credit of the British and Chinese Corporation in trust, to be applied to the service of the proposed loan.

5. The proposed loan shall be evidenced by bonds of the Imperial Railways of North China for the aggregate sum of 450,000*l.*, bearing interest at 5 per cent. per annum, and shall be further secured by a second mortgage on the properties, rents, and profits of the Imperial Railways of North China, subordinated, however, to the existing first mortgage on the same.

6. It is believed that a loan thus doubly secured may be placed on the market at a price which would yield about 88 per cent. net. The difference between this price and the par value of the bonds, namely, 12 per cent., would be approximately covered by the interest which at the date of the issue of the new bonds would have already accrued on the Indemnity Security Bonds. By the 30th June, 1903, the interest accrued on the Indemnity Security Bonds will amount to 48,000*l.*, and this is not including 16,000*l.* interest due on the 31st December, 1901, payment of which is deferred in terms of the Protocol.

Stated differently, after applying the accrued interest, say 48,000*l.* to payment on account of the claim allowed, it is proposed to raise the balance of said claim—namely, 402,000*l.*—by a loan of a sum nominal sufficient to yield that amount net, or say, in round figures, a loan of 450,000*l.*

7. The interest on the Indemnity Security Bonds, namely, 32,000*l.* per annum, applied to the service of the second mortgage bonds will, on a sinking fund basis, extinguish the latter in about twenty-five years—that is to say, by means of a half-yearly payment of 3,525*l.* per cent. which, calculated on 450,000*l.* is equal to 15,866*l.*, or a total yearly payment of 31,732*l.* The small surplus of, say, 26*l.* may be applied to defray expenses of the management of the loan service.

8. The second mortgage and second mortgage bonds shall contain, among other conditions, a guarantee by the Imperial Railways of North China to make good any deficiency that may arise in the funds provided as above for the service of the proposed loan.

9. The proceeds of the proposed loan shall be paid to the Hong Kong and Shanghai Banking Corporation at London for the credit of the British and Chinese Corporation in trust, to be applied to the purposes of the Northern Railways Indemnity, and disbursements of the same shall be made only on the joint warrant of the Representative of the British and Chinese Corporation on the Railway Directorate in Tien-tsin, and of the Chief Engineer of the Imperial Railways of North China, whose appointment is provided for by the first mortgage agreement.

(Signed) E. G. HILLIER.

Peking, April 21, 1903.

AFFAIRS OF CHINA.

[June 8.]

171

CONFIDENTIAL

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[2020 4-5]

*Mr. Townley
No. 159*

April 22. 1903

Rec'd June 8

[Desp. No. 138 of Apr. 9. 1903]

*Northern Railways
Indemnity claims
Mr. Hillier's Scheme
for payment of:*

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(Signed) E. G. HILLIER.

Peking, April 21, 1903.

the circumstances mentioned above, from which it appears that
you have a right to expect that I will do my best to forward your
copy of the letter to Mr. Keller, and I do so now, without any
further delay.

requested that in any further communication
on this subject the under-mentioned letter and
number may be quoted, and the reply addressed to—
The Under Secretary of State for India,
India Office,
Whitehall,
London, S.W.
M. 4405.

176
India Office,
Whitehall,
London, S.W.

22nd April 1903.

Mr. Keller's name
for forward-
ing
Northern Railways
China.

Dated 17th
Feb. '03
and encs.

Sir,

In continuation of my letter of 30th January 1903
No. M. 1425, I am directed to forward a copy of a letter
from the General Officer Commanding, Tientsin,
enclosing maps and plans of a section of the Imperial
Railways of North China.

I have the honour to be, Sir,

Your obedient Servant,


Major-General,

Military Secretary.

The Under Secretary of State,
Foreign Office.

HEADQUARTERS STAFF OFFICE,
INTELLIGENCE BRANCH,
CHINA FORCE.

Confidential. No. 73/I.C. Tientsin, 17th February 1903.

From the General Officer Commanding,
China Force.

To the Right Honourable
The Secretary of State for India,
India Office,
London, S.W.

My Lord,

In continuation of my No. 795/I.C. dated 20th

November 1902, I have the honour to submit for Your

Military Report on the
Imperial Railways of
North China, Shan-Hai-
Kuan to Ying-K'ou (Niu-
Chuang) Section compiled
in the Intelligence
Branch, China Force -
February 1903.

Lordship's information a copy of
the marginally noted report with
maps and plans.

2. The only available copies of
the photographs accompanying the Report have been
forwarded to the Director-General of Military
Intelligence, War Office.

I have the honor to,
(Sd.) O'M. Creagh, Major-General,

Commanding China Force.

Enclosed
2/2/03.
P. April 23, 1903.
[Enc. I.O. Jan 30, 03].
Imperial Railways in North
China.

The present part of
this Report is offered
without delay, but
see pages 26, 28, & 32
for parts
39. and they attach
to the Report. I do not
find in the Plans and other
maps 'Luchuan & Fung
Tse' districts or 'Hui
Ch. Dom'.

~~Imperial Railways of North China~~

R { April 23, 1903

[ref I.O. Jan 30, 03]

Imperial Railways of North China

Enclosed copy of letter from
G.O.C. Tientsin enclosing maps
of plan of a section of.

Enc. in I.O.

April 23, 03

LIST OF MAPS AND PLANS.

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MAPS.

- I. SKETCH MAP to illustrate Present and Future Railways in NORTH CHINA - scale - 78 miles = 1 inch.
- II. General Map of SHENG-KING PROVINCE and Neighbouring Country - scale - 10 miles = 1 inch.
- III. Map of the Line from SHAN-HAI-KUAN to YING-K'OU (NIU-CHUANG) and HSIN-MIN-T'UN. scale - 2 miles = 1 inch.

PLANS.

1. SHAN-HAI-KUAN STATION.
2. CH'IEN-SO STATION. (Note - "so" means a place).
3. CH'IEN-WEI STATION.
4. CH'UNG-HOU-SO STATION.
5. SHA-HO-SO STATION.
6. NING-YUAN CHOU STATION.
7. LIEN-SHAN STATION.
8. KAO-CH'IAO STATION.
9. CHIN-CHOU FU STATION.
10. TA-LING HO STATION.
11. SHIH-SHAN-CHAN STATION.
12. KOU-PANG-TZU STATION.
13. SHUANG-T'AI-TZU STATION.
14. TIEN-CHUANG-T'AI STATION.
15. YING-K'OU (NIU-CHUANG) STATION.
16. Plan of CHIN-CHOU FU.
17. Rough plan of country North of CHIN-CHOU Railway Station to illustrate points from which photographs were taken.
18. Plan of TA-LING HO Bridge.
19. Types of Cirders manufactured in SHAN-HAI-KUAN Bridge works.

Note - Map No: II has been compiled from Map No: III, from original work of members of the China Force and from Japanese and Russian Maps. The Longitude and Latitude of SHAN-HAI-KUAN and NIU-CHUANG is from Admiralty Charts.

Map No: III is, with the exception of the railway from SHAN-HAI-KUAN to CH'UNG-HOU-SO, original work. The country between NING-YUAN CHOU and the NU-ERH-HO was triangulated so as to join up the coast surveys with those of the railway roads.







IMPERIAL RY^S OF NORTH CHINA

SECTION

SHAN-HAI-KUAN TO YING-KOU (Nan-chung)

Scale 2 Miles to 1 inch O.P. (1:125,000)

Furlongs

Miles

Conventional Signs

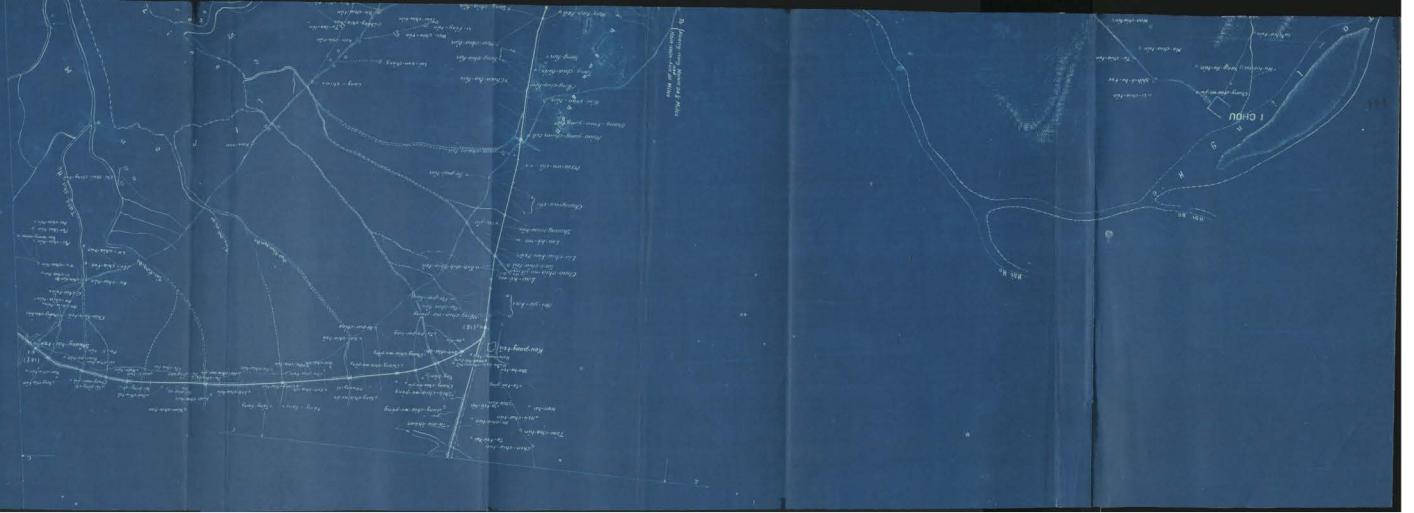
Main cart roads
Other cart tracks
Railways

Shaded areas
Distance in miles between — (1/12)

stations and coast cities from 1865 (1875)

G

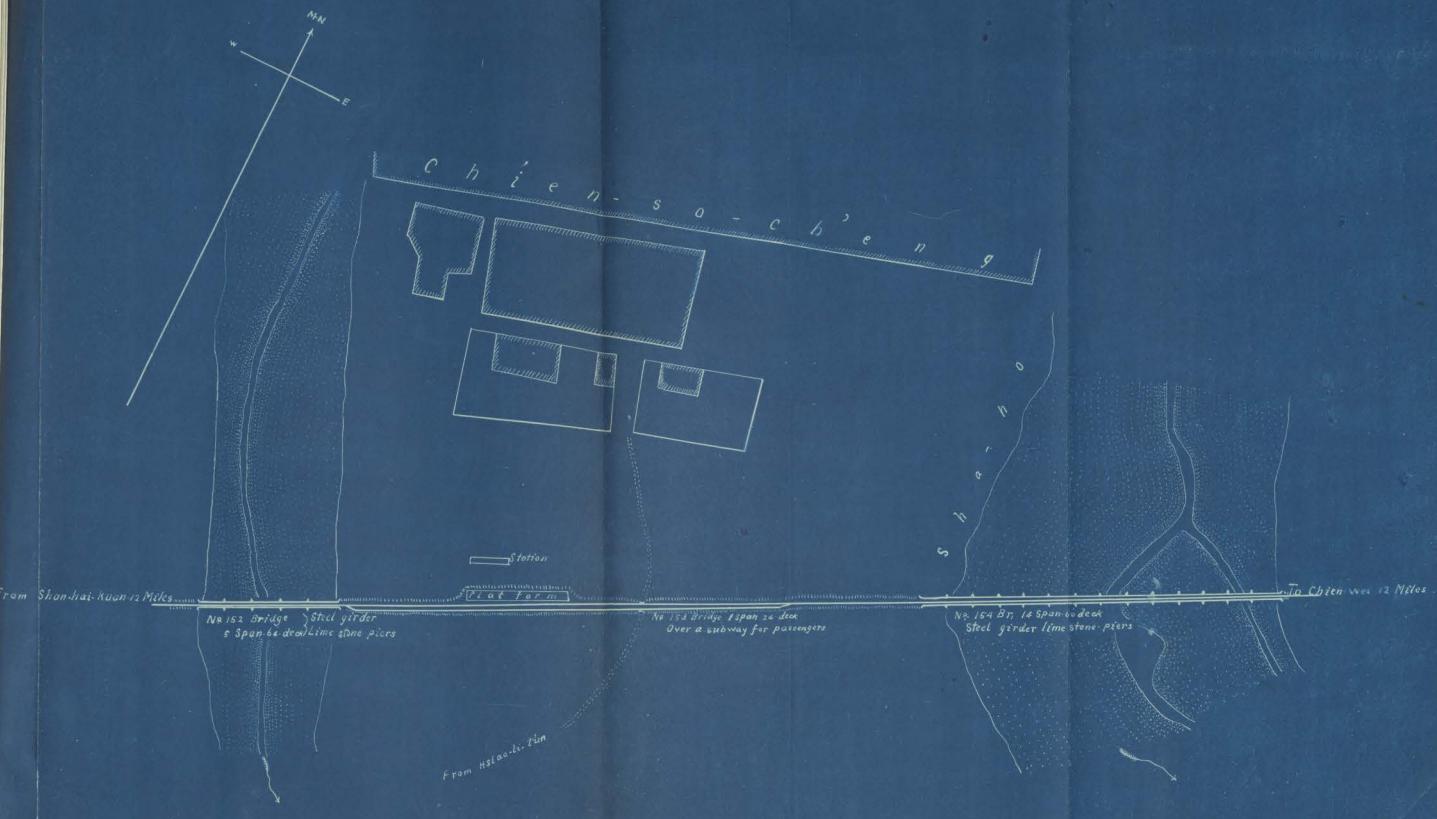
HAN-PEI



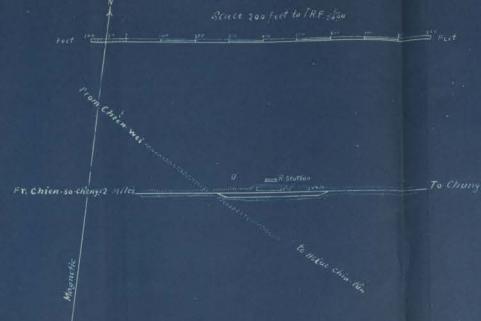
PLAN OF CHIEN SO CHENG RAILWAY STATION

Scale 200 feet to 1 inch A.P. 24⁰

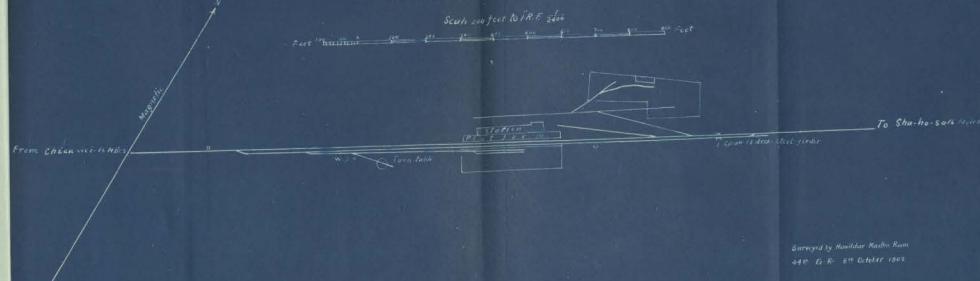
Feet 0 1 2 3 4 5 6 7 8 9 10 11 12 13 14 15 16 17 18 19 20 21 22 23 24 25 26 27 28 29 30 31 32 33 34 35 36 37 38 39 39 40 41 42 43 44 45 46 47 48 49 50 51 52 53 54 55 56 57 58 59 59 60 61 62 63 64 65 66 67 68 69 69 70 71 72 73 74 75 76 77 78 79 79 80 81 82 83 84 85 86 87 88 89 89 90 91 92 93 94 95 96 97 98 99 100



PLAN OF CHIEN-WEI STATION



PLAN OF CHUNG-HOU-SO STATION



(2)

3

PLAN OF SHA-HO-SO STATION

Scale 200 feet off 25s.
Perch

From Chang-ho-sse & Nitch

To Ning-yuan chow & Mila

Surveyed by Houlding Madha Ram
447 G.R. 10 October 1884

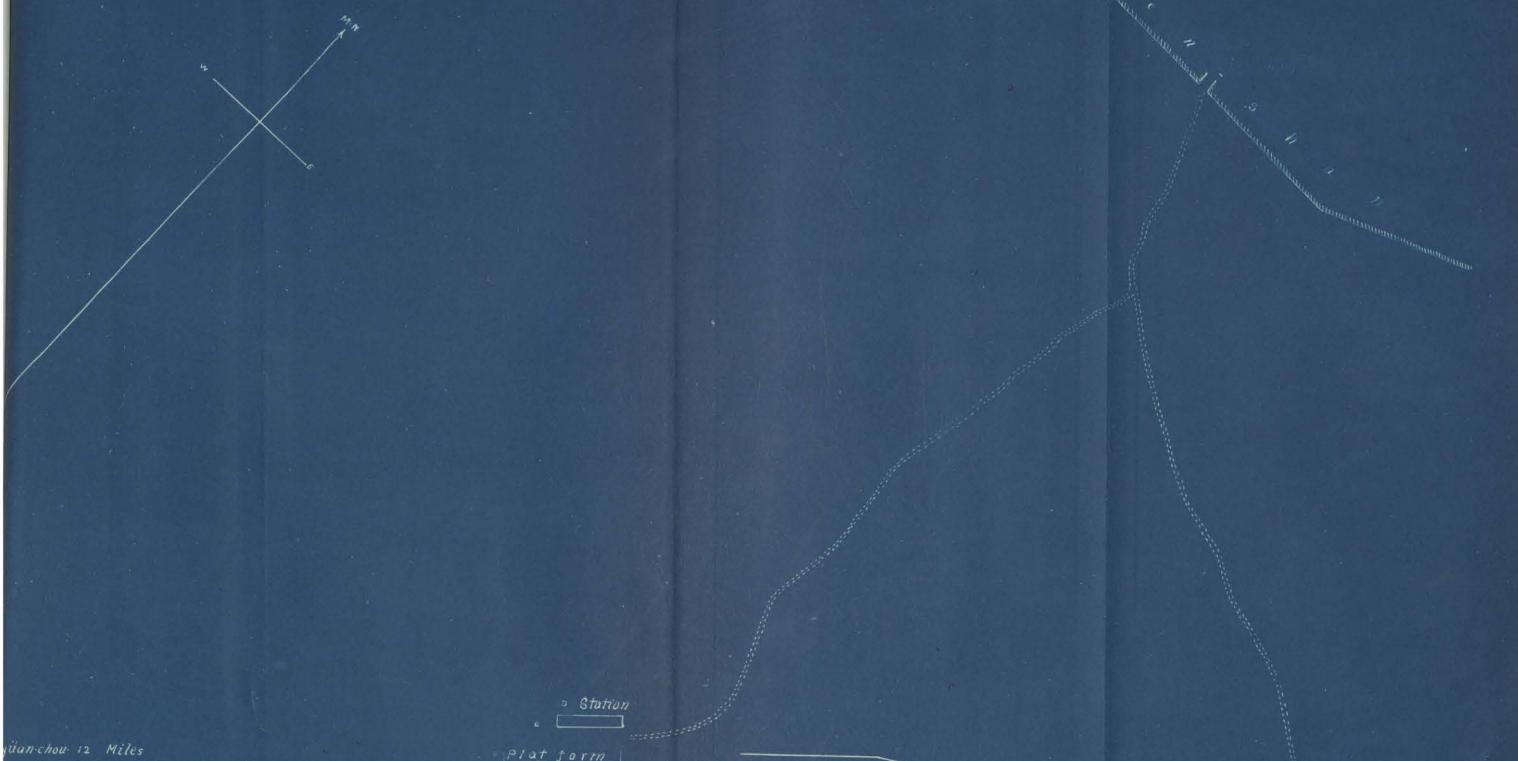
4

PLAN OF NING-YÜAN CHOU STATION

PLAN OF LIEN SHAN RAILWAY STATION

Scale 200 feet to 1 inch R.F. 1:600

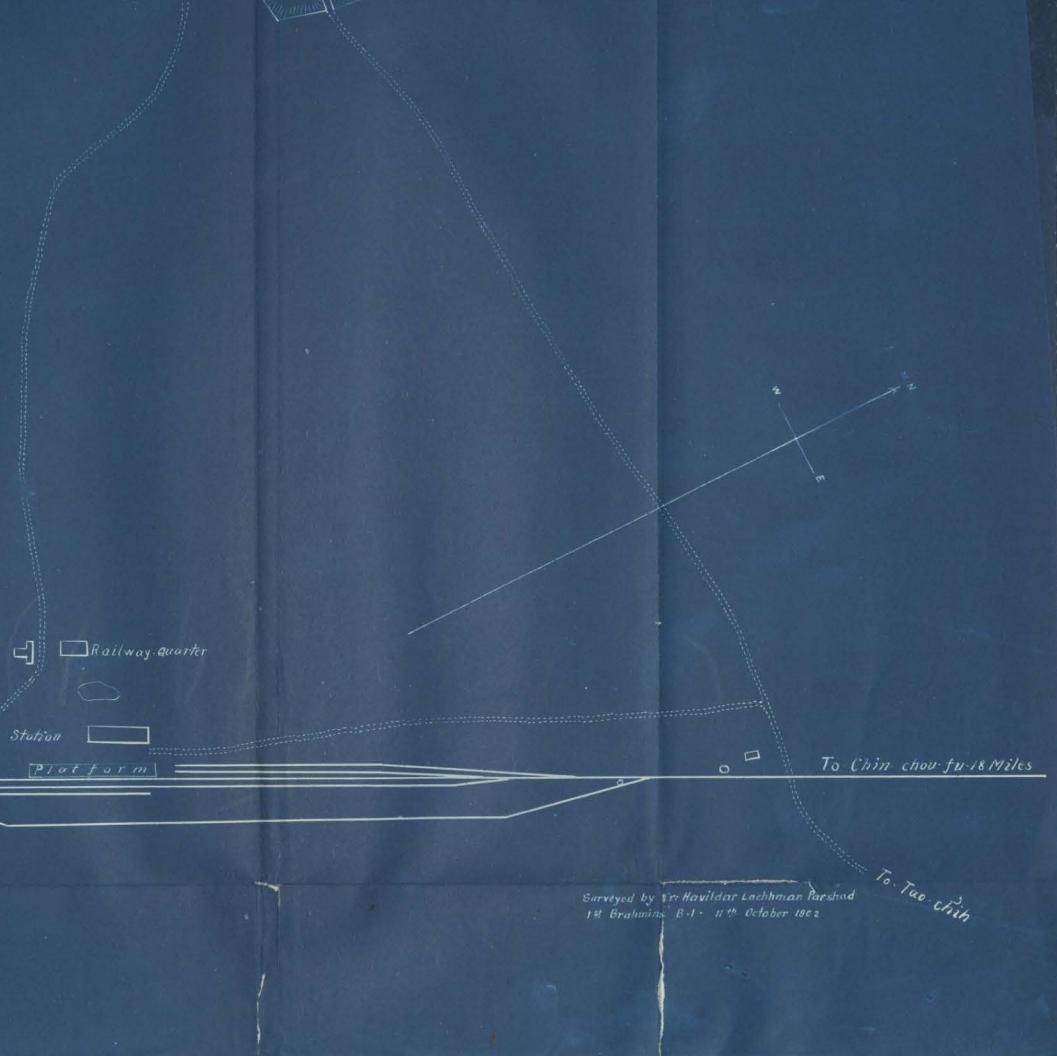
Feet 0 200 400 600 800 1000 1200 1400 1600 1800 2000 2200 2400 2600 2800 3000 3200 Feet

Surveyed by Mr. Havildar Lachman Parchas
M. Brahmins S.I. 10th October 1882.

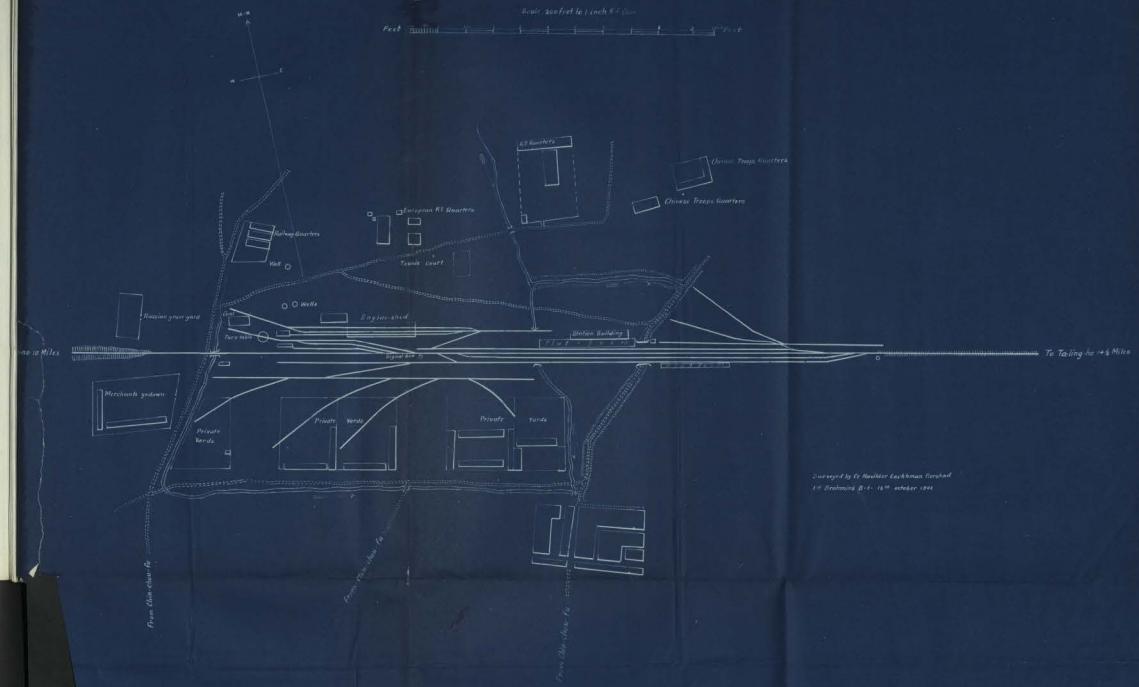
PLAN OF KAO CHIAO RAILWAY STATION

Scale 200 feet to 1 inch R.F. 1420

Feet 100 115 130 145 160 175 190 205 220 235 250 265 280 295 310 325 340 355 370 385 395 410 425 440 455 470 485 495 510 525 540 555 570 585 595 610 625 640 655 670 685 695 710 725 740 755 770 785 795 810 825 840 855 870 885 895 905 920 935 950 965 980 995 1000 Feet



PLAN OF CHIN CHOU FU RAILWAYSTATION

Scale: 200 feet to 1 inch 8¹/₂ miles

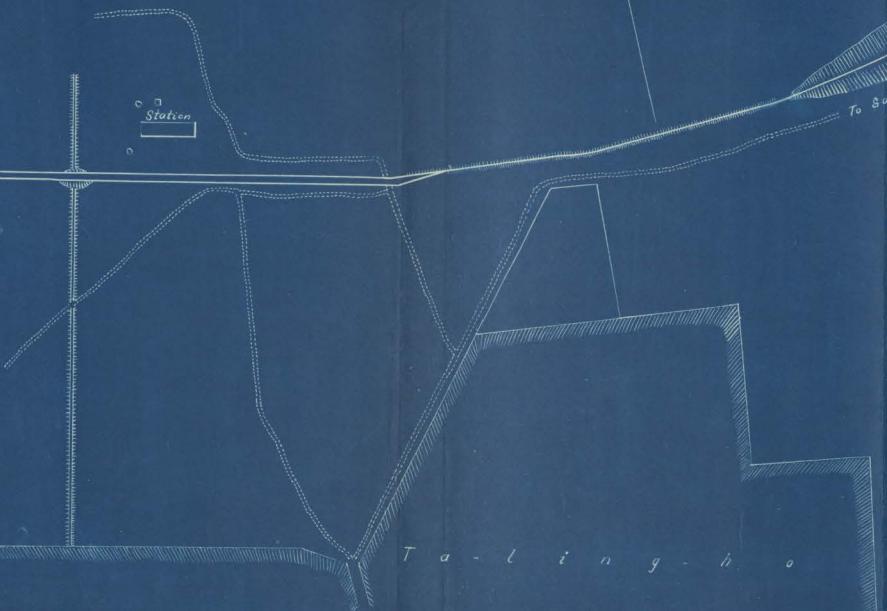
PLAN OF TA LING HO STATION

Scale 200 feet to 1 RF 2400

Feet 100 200 300 400 500 600 700 800 900 1000 Feet

N
E
W
S

chou fu 14½ Miles



Surveyed by C. Havildar Lakshman Parasad
Brahmins B.I. 17 October 1882

PLAN OF SHIH SHAN CHAN STATION

Scale 1 in. to 1 mile
Feet 100 200 300 400 500 600 700 800 900 feet

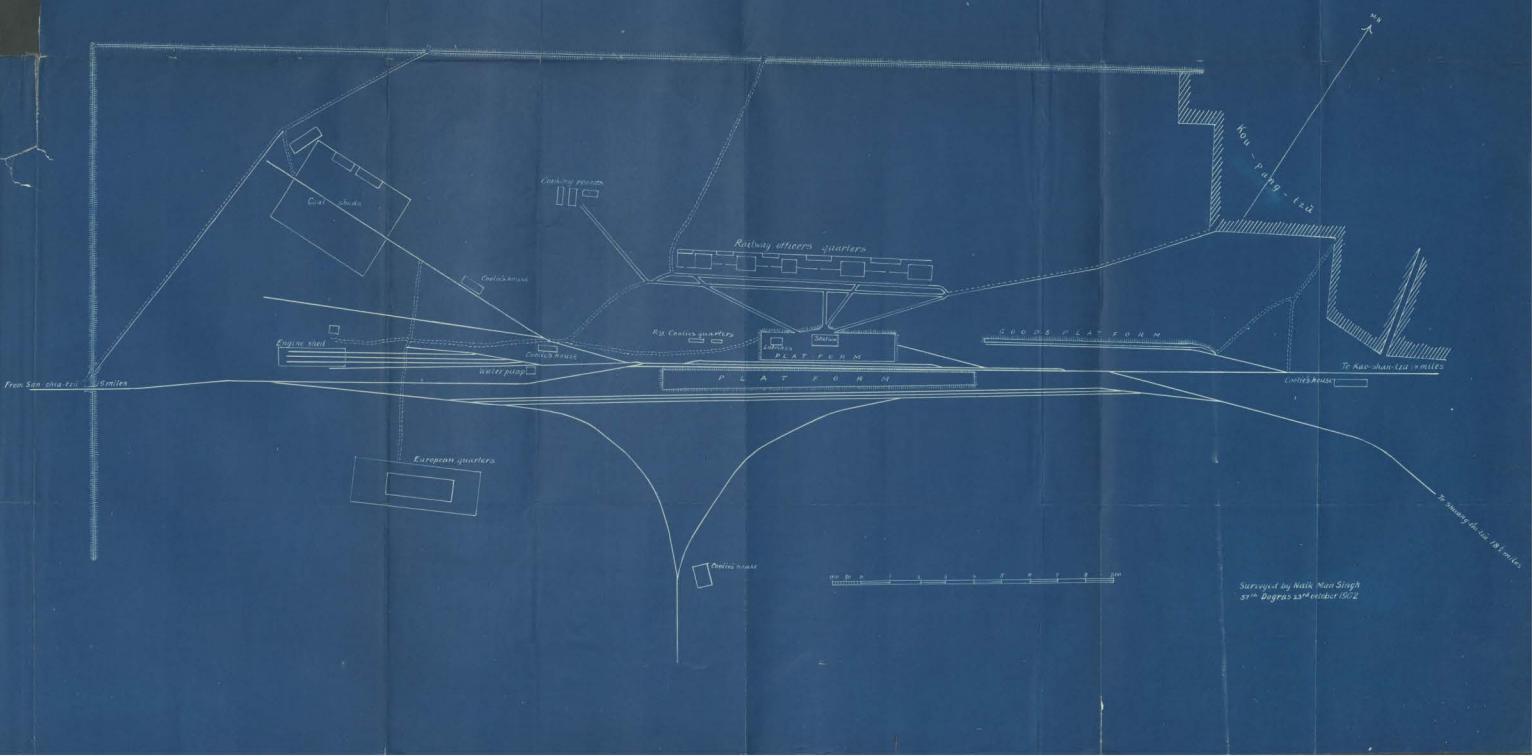
from Tailing-ha 9½ miles Single PLATE FORM
Station Water pump To Kao-ping 10½ miles

Surveyed by Nark Mun Singh
37° Degrees 21st October 1902

To San Chia-tsu
To San-chia-tsu
From Ping-chen-tzu
Magentic

PLAN OF KOU-PANG - TZÜ STATION

Scale 1000 feet to the R.F. miles

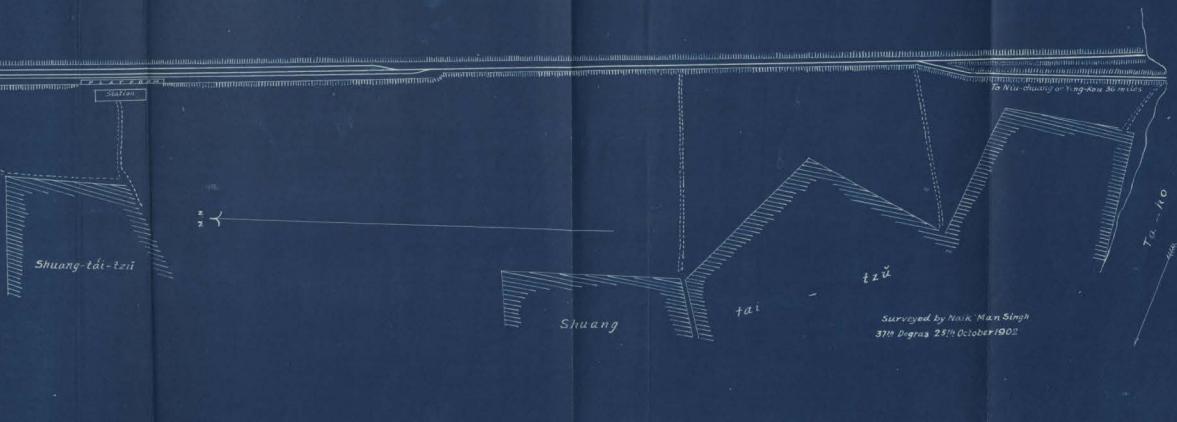


PLAN OF SHUANG TAI TZÜ STATION

Scale 200 FT to Inch

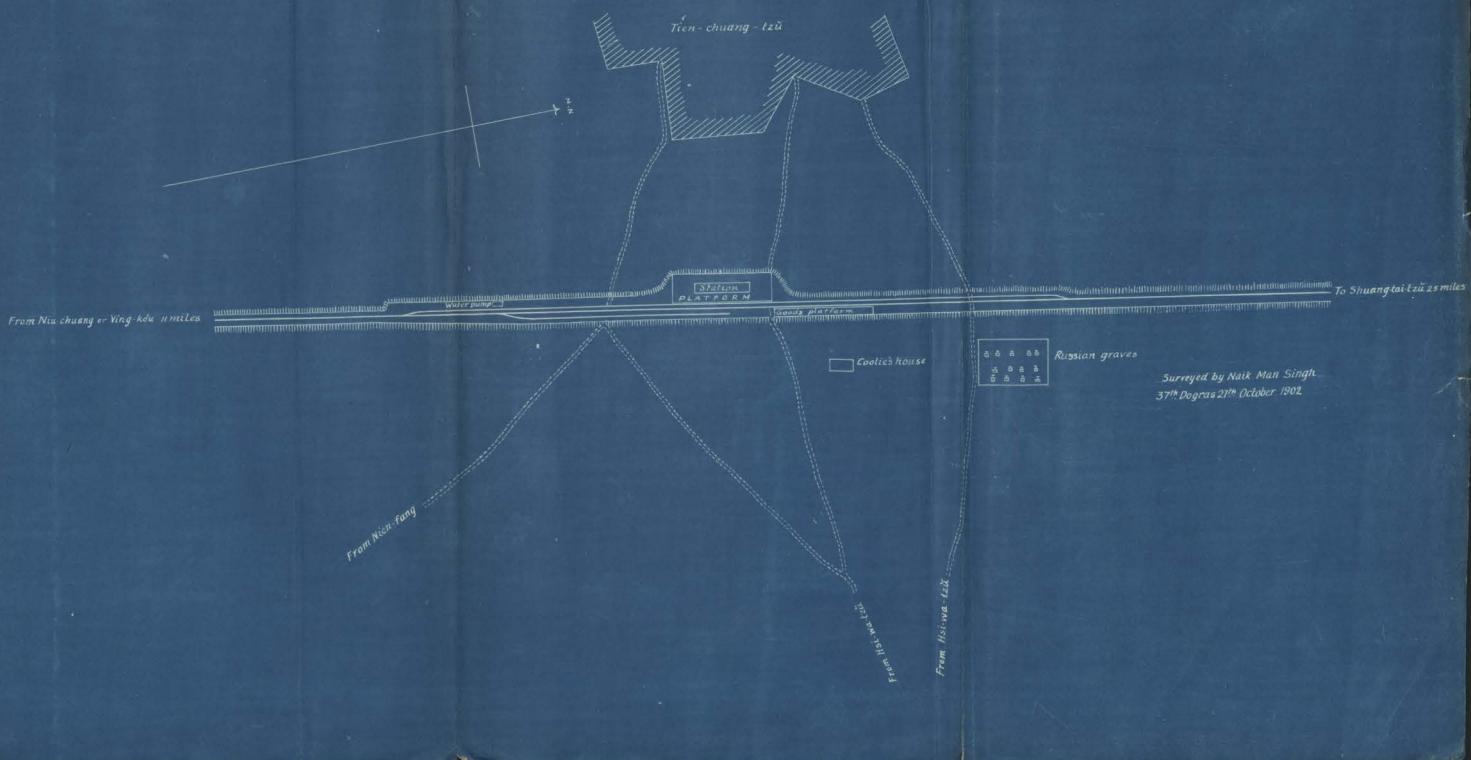
Feet 100 80 60 40 20 0

400 320 240 160 80 0 feet



PLAN OF TIEN CHUANG TAI STATION

Scale 2000 ft to 1 Inch

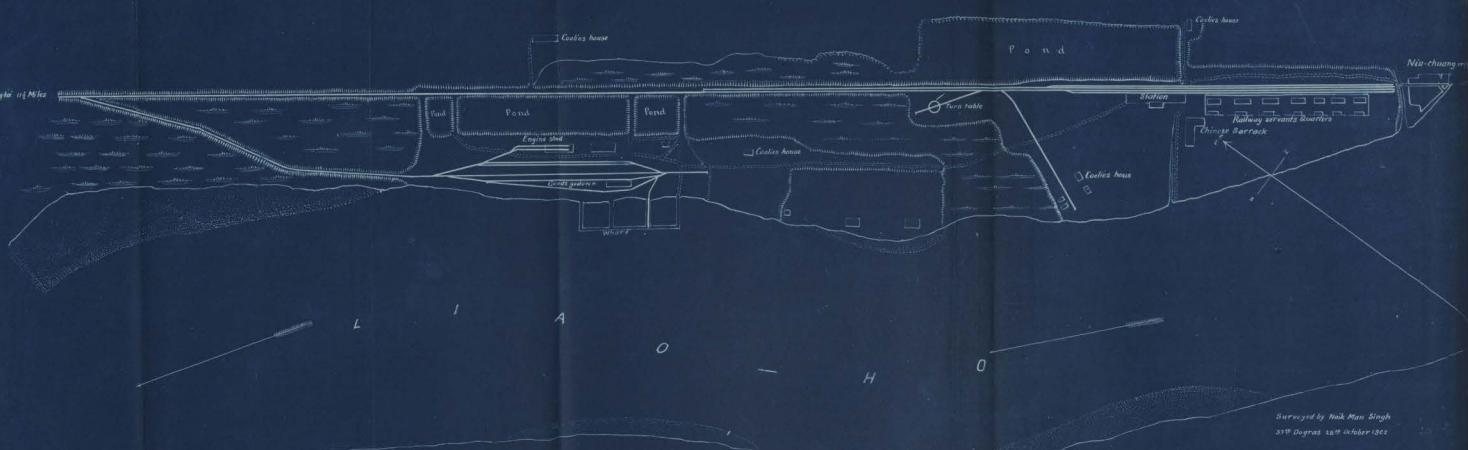


PLAN OF NIU CHUANG (YING KOU) RAILWAY STATION

Scale 400 feet to 1 inch & also

Feet 0 100 200 300 400 500 600 700 800 900 1000 1100 1200 1300 1400 1500 1600 1700 1800 1900 2000 2100 2200 2300 2400 2500 2600 2700 2800 2900 3000 3100 3200 3300 3400 3500 3600 3700 3800 3900 4000 4100 4200 4300 4400 4500 4600 4700 4800 4900 5000 5100 5200 5300 5400 5500 5600 5700 5800 5900 6000 6100 6200 6300 6400 6500 6600 6700 6800 6900 7000 7100 7200 7300 7400 7500 7600 7700 7800 7900 8000 8100 8200 8300 8400 8500 8600 8700 8800 8900 9000 9100 9200 9300 9400 9500 9600 9700 9800 9900 10000 10100 10200 10300 10400 10500 10600 10700 10800 10900 11000 11100 11200 11300 11400 11500 11600 11700 11800 11900 12000 12100 12200 12300 12400 12500 12600 12700 12800 12900 13000 13100 13200 13300 13400 13500 13600 13700 13800 13900 14000 14100 14200 14300 14400 14500 14600 14700 14800 14900 15000 15100 15200 15300 15400 15500 15600 15700 15800 15900 16000 16100 16200 16300 16400 16500 16600 16700 16800 16900 17000 17100 17200 17300 17400 17500 17600 17700 17800 17900 18000 18100 18200 18300 18400 18500 18600 18700 18800 18900 19000 19100 19200 19300 19400 19500 19600 19700 19800 19900 20000 20100 20200 20300 20400 20500 20600 20700 20800 20900 21000 21100 21200 21300 21400 21500 21600 21700 21800 21900 22000 22100 22200 22300 22400 22500 22600 22700 22800 22900 23000 23100 23200 23300 23400 23500 23600 23700 23800 23900 24000 24100 24200 24300 24400 24500 24600 24700 24800 24900 25000 25100 25200 25300 25400 25500 25600 25700 25800 25900 26000 26100 26200 26300 26400 26500 26600 26700 26800 26900 27000 27100 27200 27300 27400 27500 27600 27700 27800 27900 28000 28100 28200 28300 28400 28500 28600 28700 28800 28900 29000 29100 29200 29300 29400 29500 29600 29700 29800 29900 30000 30100 30200 30300 30400 30500 30600 30700 30800 30900 31000 31100 31200 31300 31400 31500 31600 31700 31800 31900 32000 32100 32200 32300 32400 32500 32600 32700 32800 32900 33000 33100 33200 33300 33400 33500 33600 33700 33800 33900 34000 34100 34200 34300 34400 34500 34600 34700 34800 34900 35000 35100 35200 35300 35400 35500 35600 35700 35800 35900 36000 36100 36200 36300 36400 36500 36600 36700 36800 36900 37000 37100 37200 37300 37400 37500 37600 37700 37800 37900 38000 38100 38200 38300 38400 38500 38600 38700 38800 38900 39000 39100 39200 39300 39400 39500 39600 39700 39800 39900 39999 40000

From Tien-chuang in Miles



PLAN OF CHIN-CHOU FU

Scale 8 to 1 Mile R.F. 1:62500

Yards 100 200 300 400 500 600 700 800 900 1000 1100 1200 Yards

To Taiding 40 miles

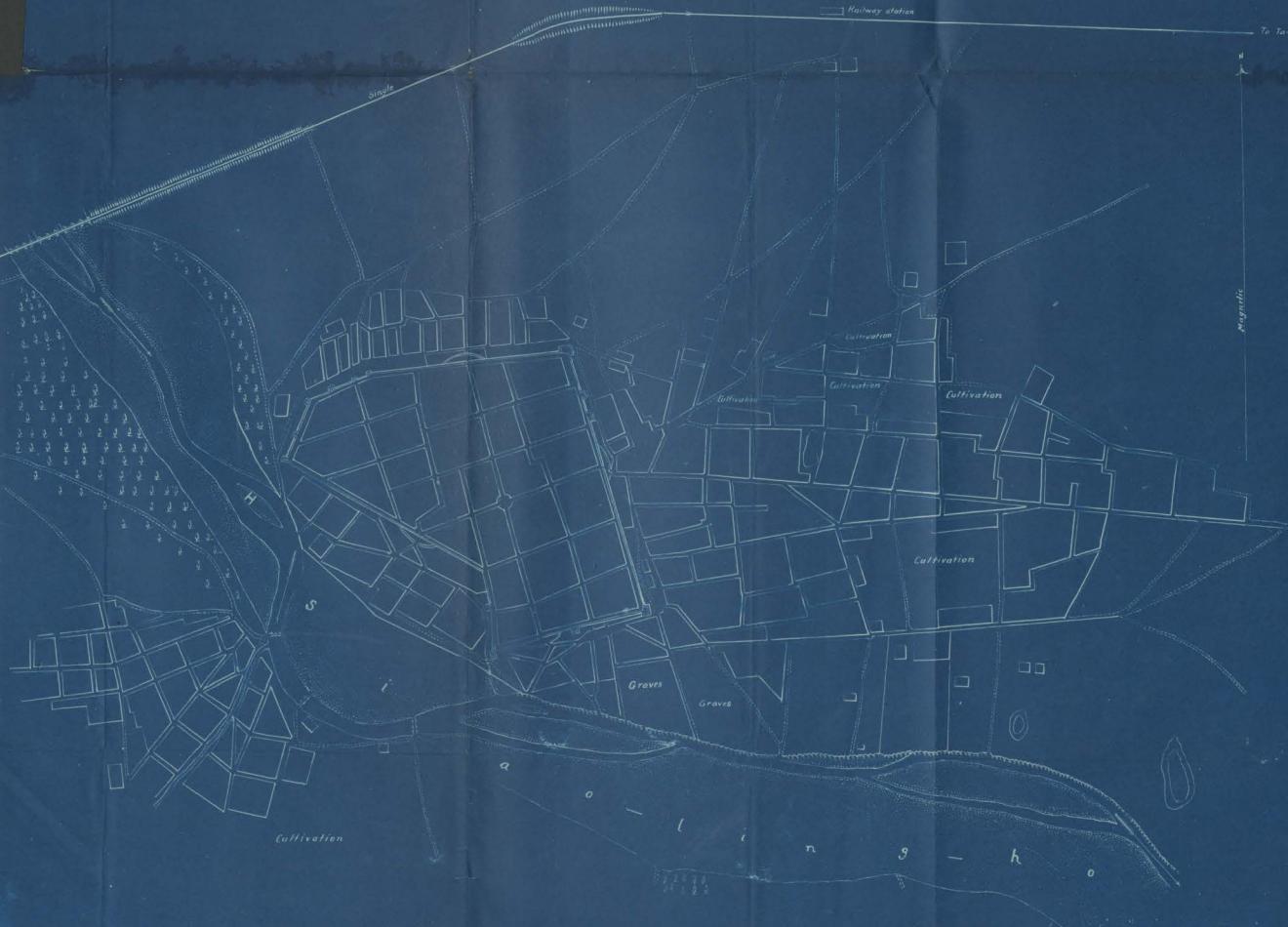
N

Neighburhood

Songs

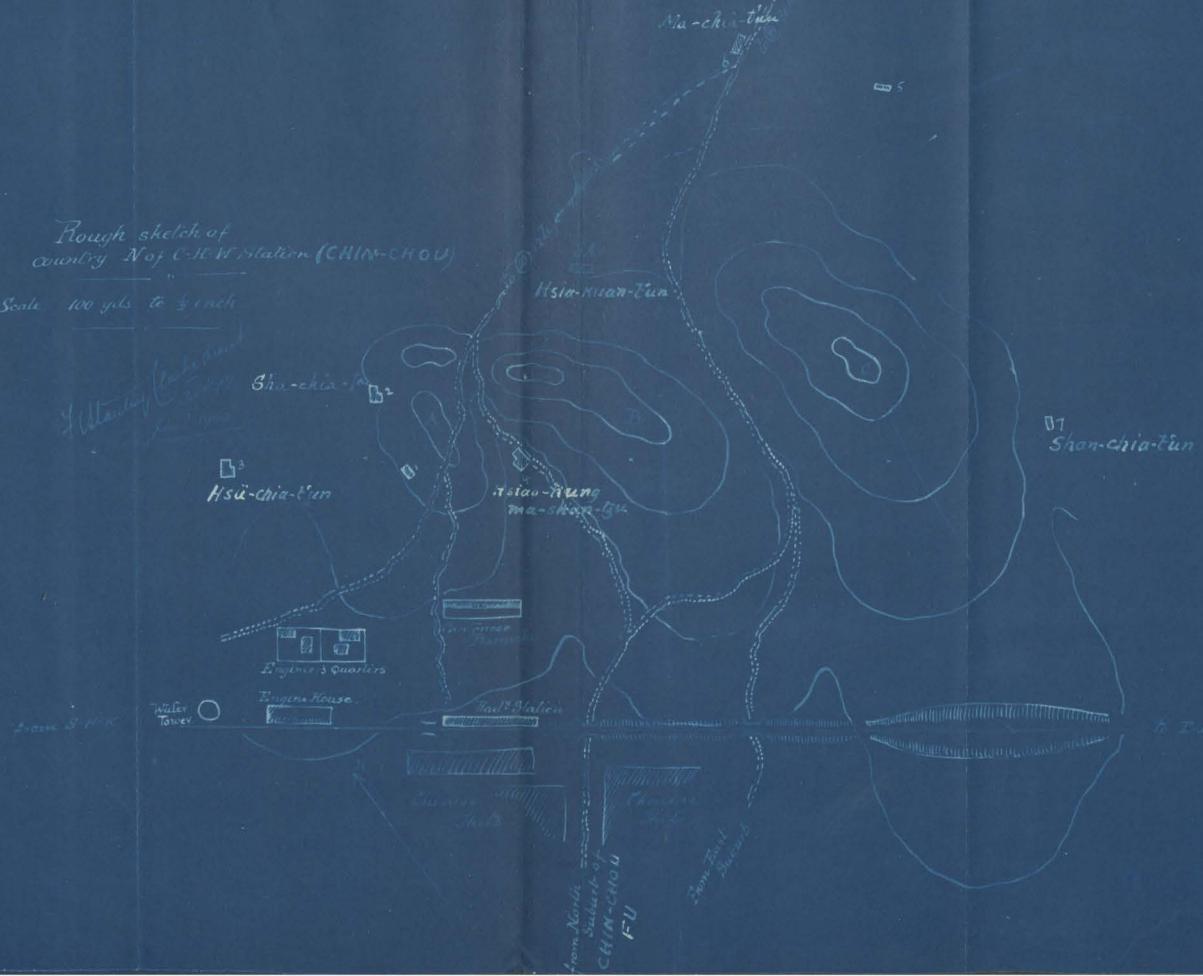
Railway station

From Kao-chou 4½ Miles



Rough sketch of
country N of C-H W Station (CHIN-CHOU)

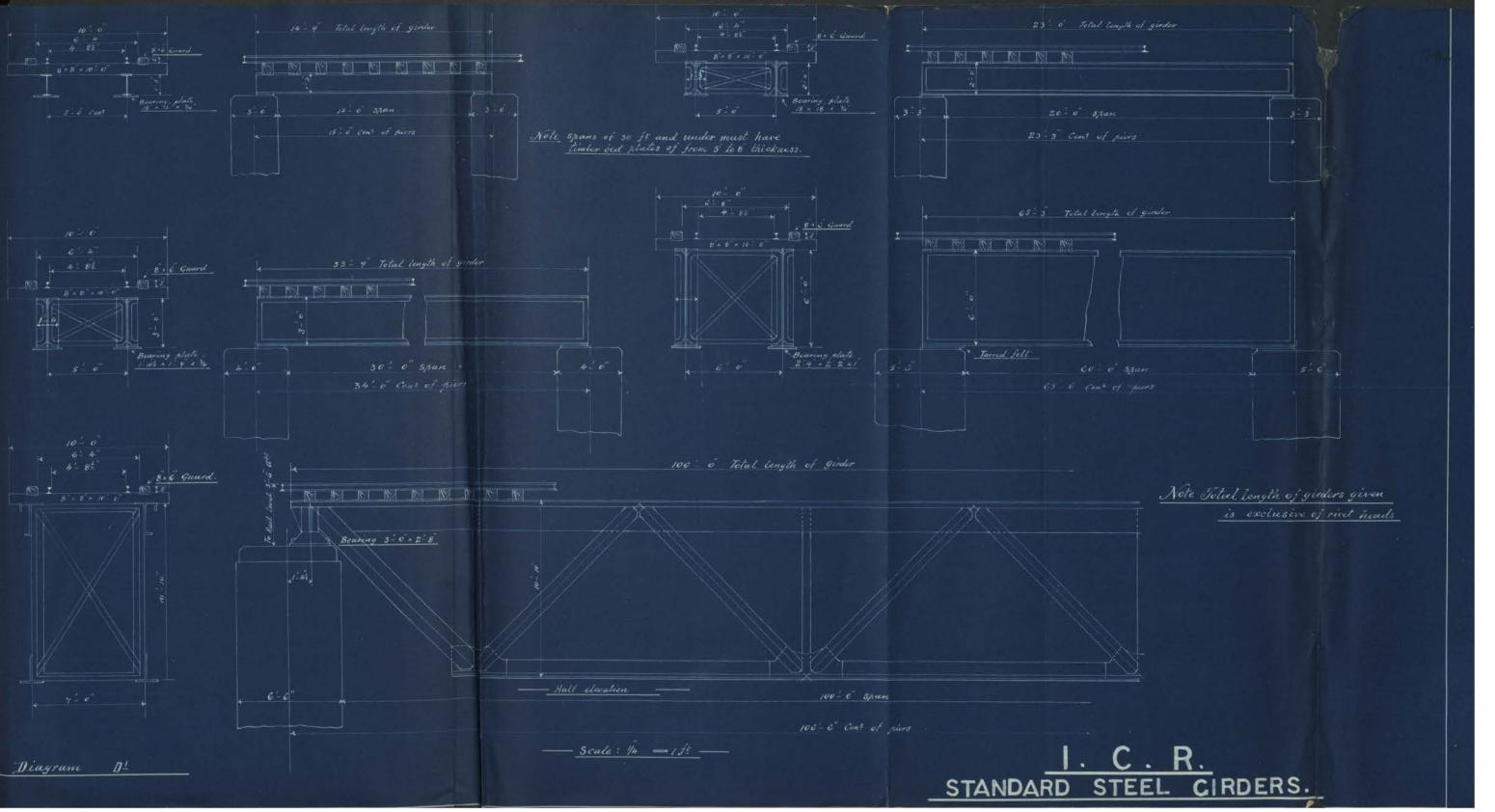
Scale 100 yds to $\frac{1}{2}$ inch



PLAN OF TA LING HO BRIDGE

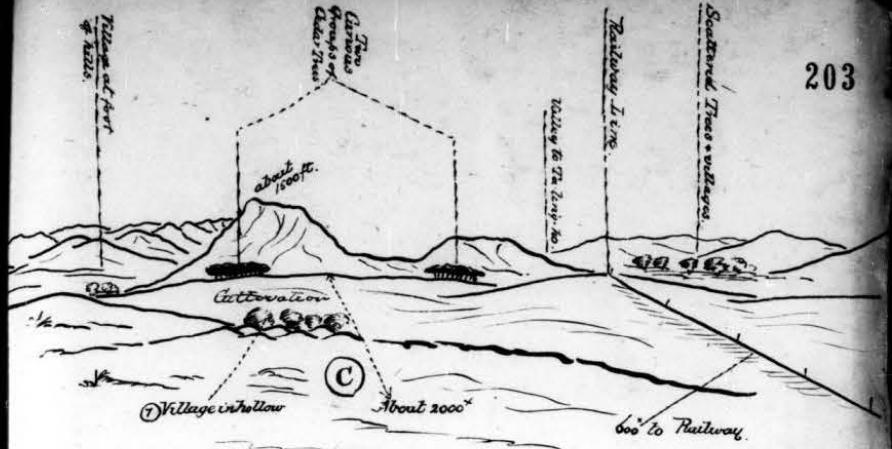
(Survey performed 1898 in course of construction)





I. C. R.
STANDARD STEEL GIRDERS.

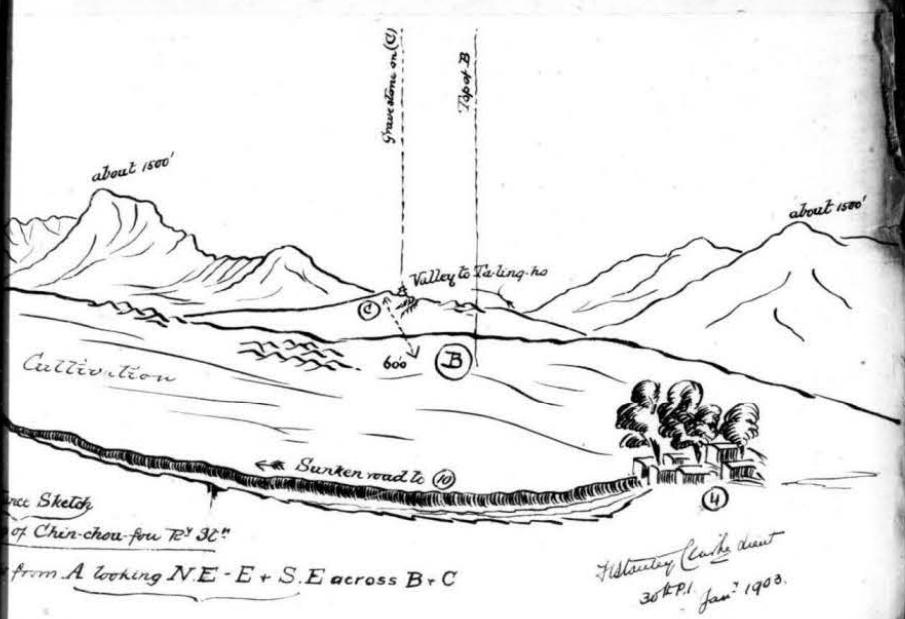
Scale: 1/8 - 1/2



Sketch See map of Chin-chou-fu R.Y. 31°
View from C looking East + S.E.

This sketch is taken from the East side
of the remarkable grave stone
shewn in the other two sketches.

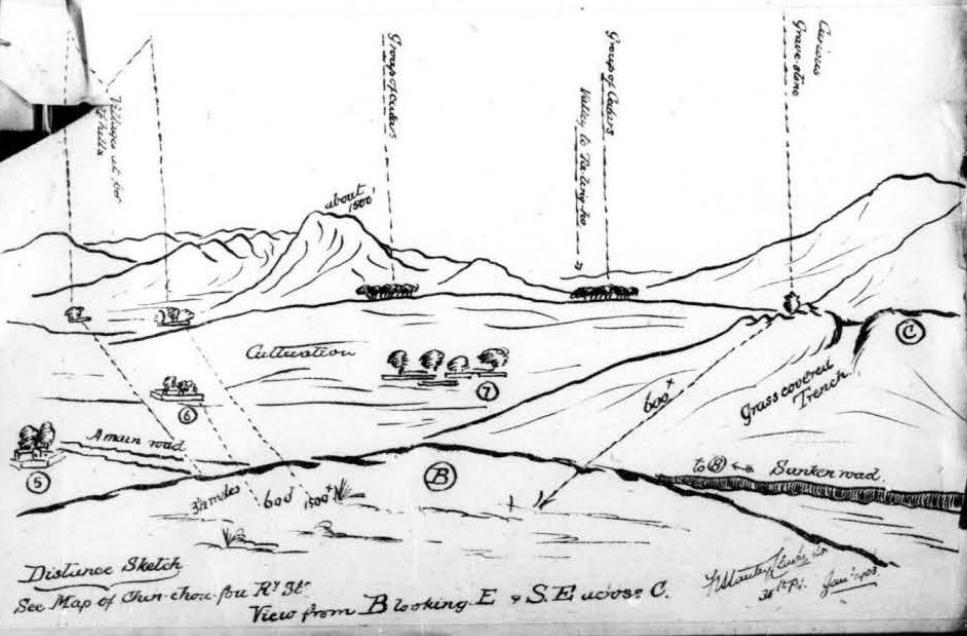
Hillman
Jan. 30, 1903.



Sketch
of Chin-chou-fu R.Y. 31°

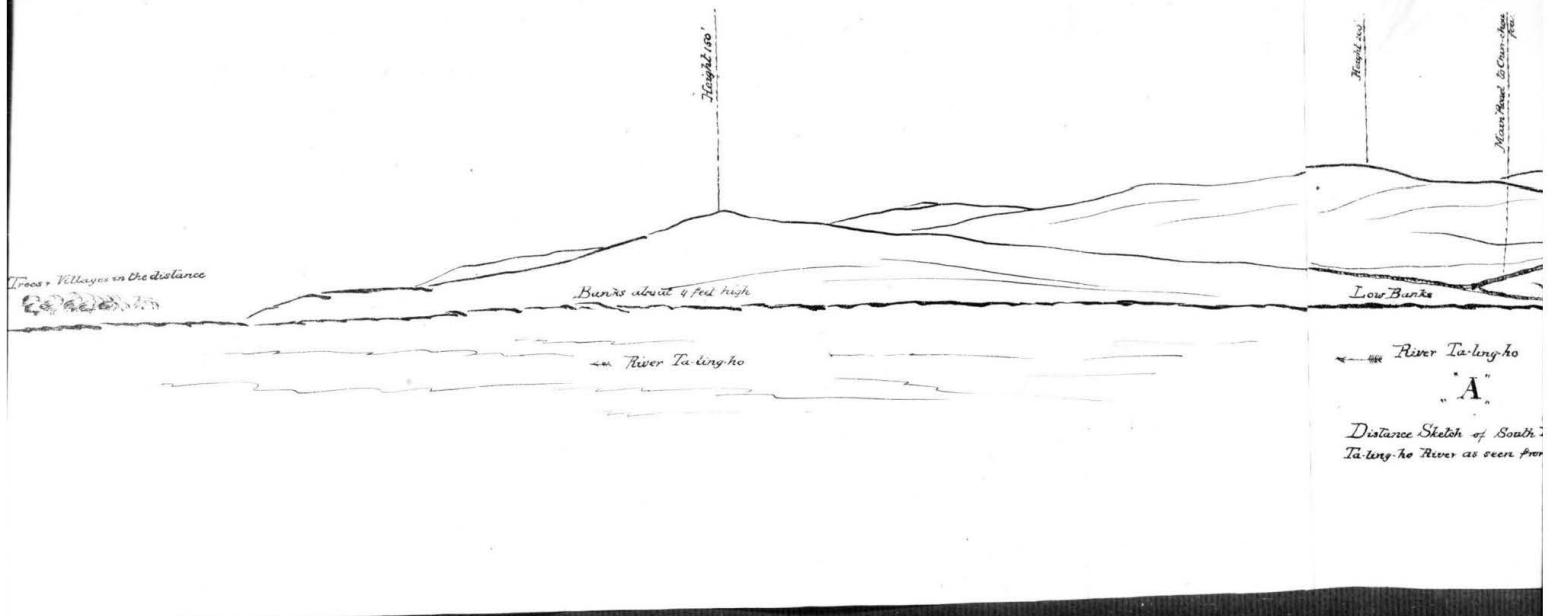
from A looking NE - E + S.E across B + C

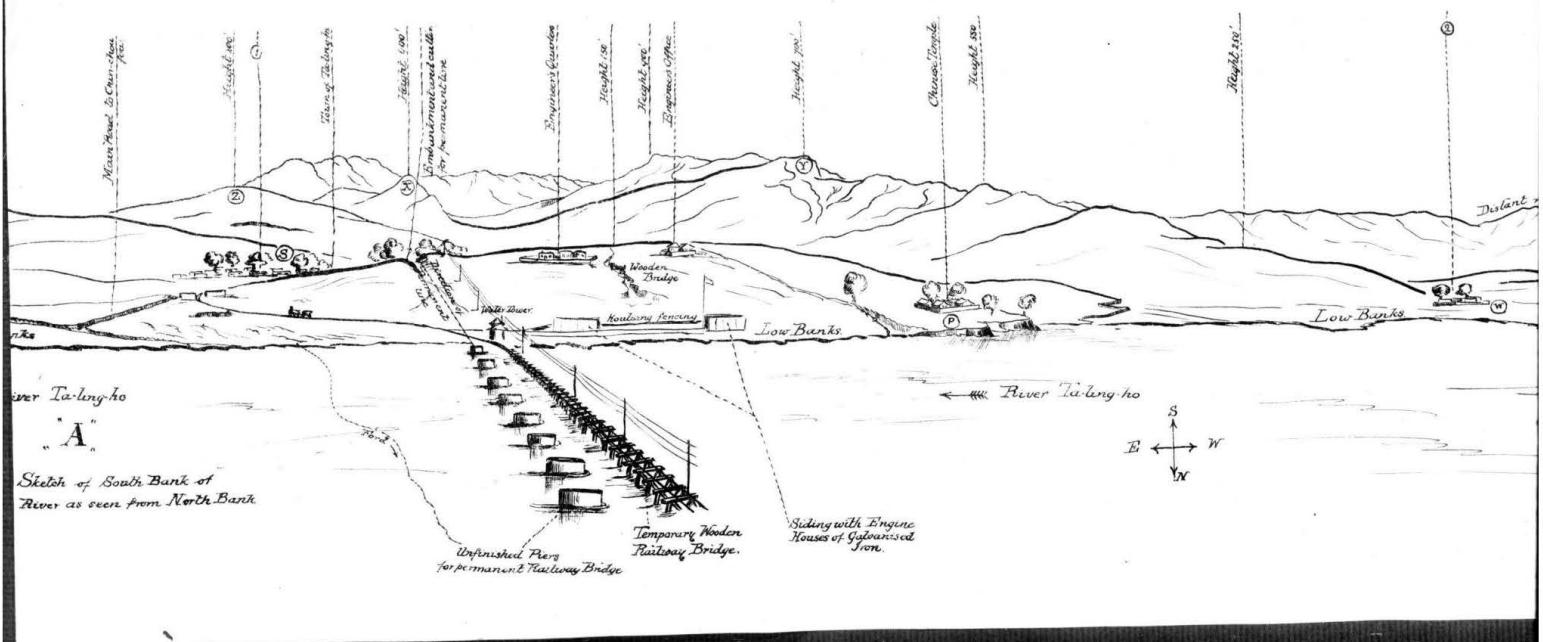
Hillman
Jan. 30, 1903.

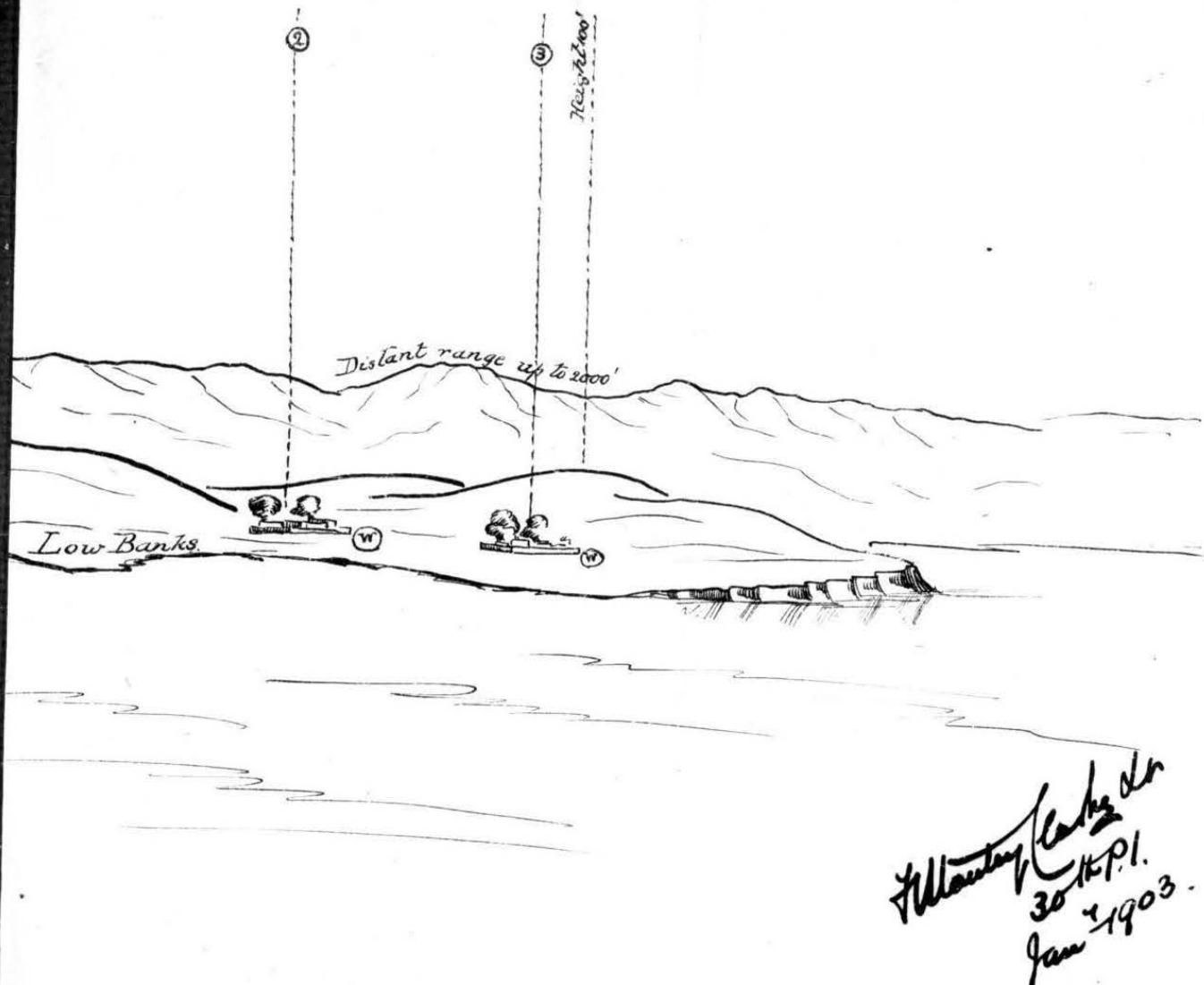


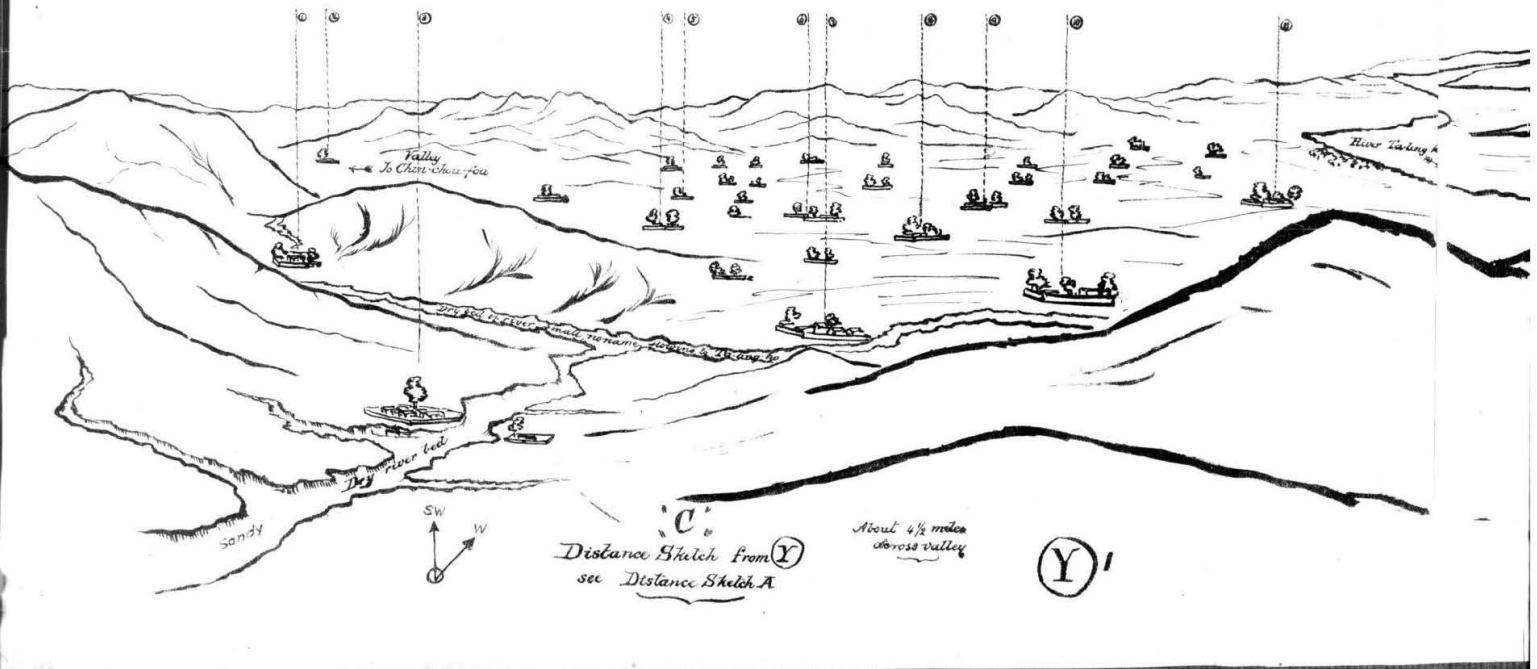
Distance Sketch
See Map of Chin-chou-fu R.Y. 31°
View from B looking E + S.E across C.

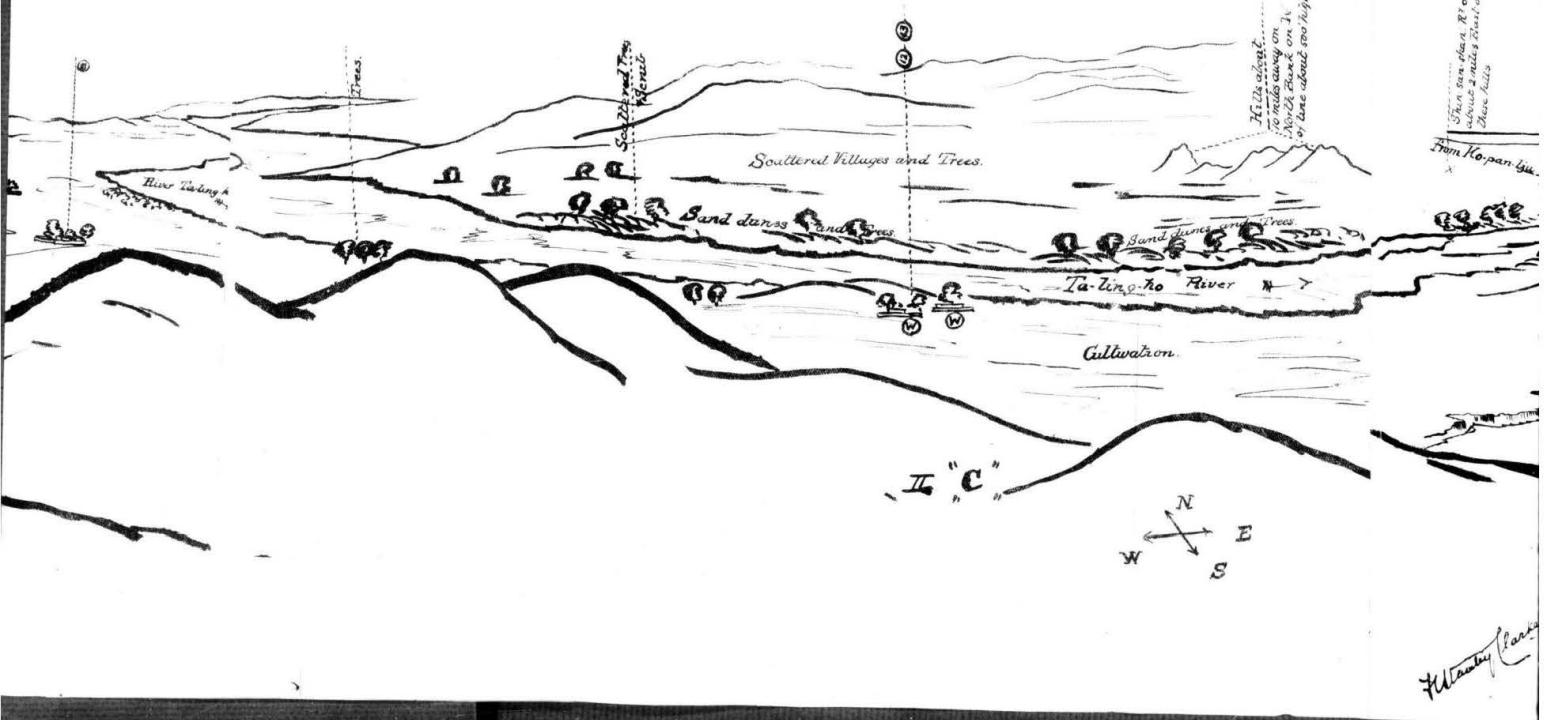
Hillman
Jan. 30, 1903.

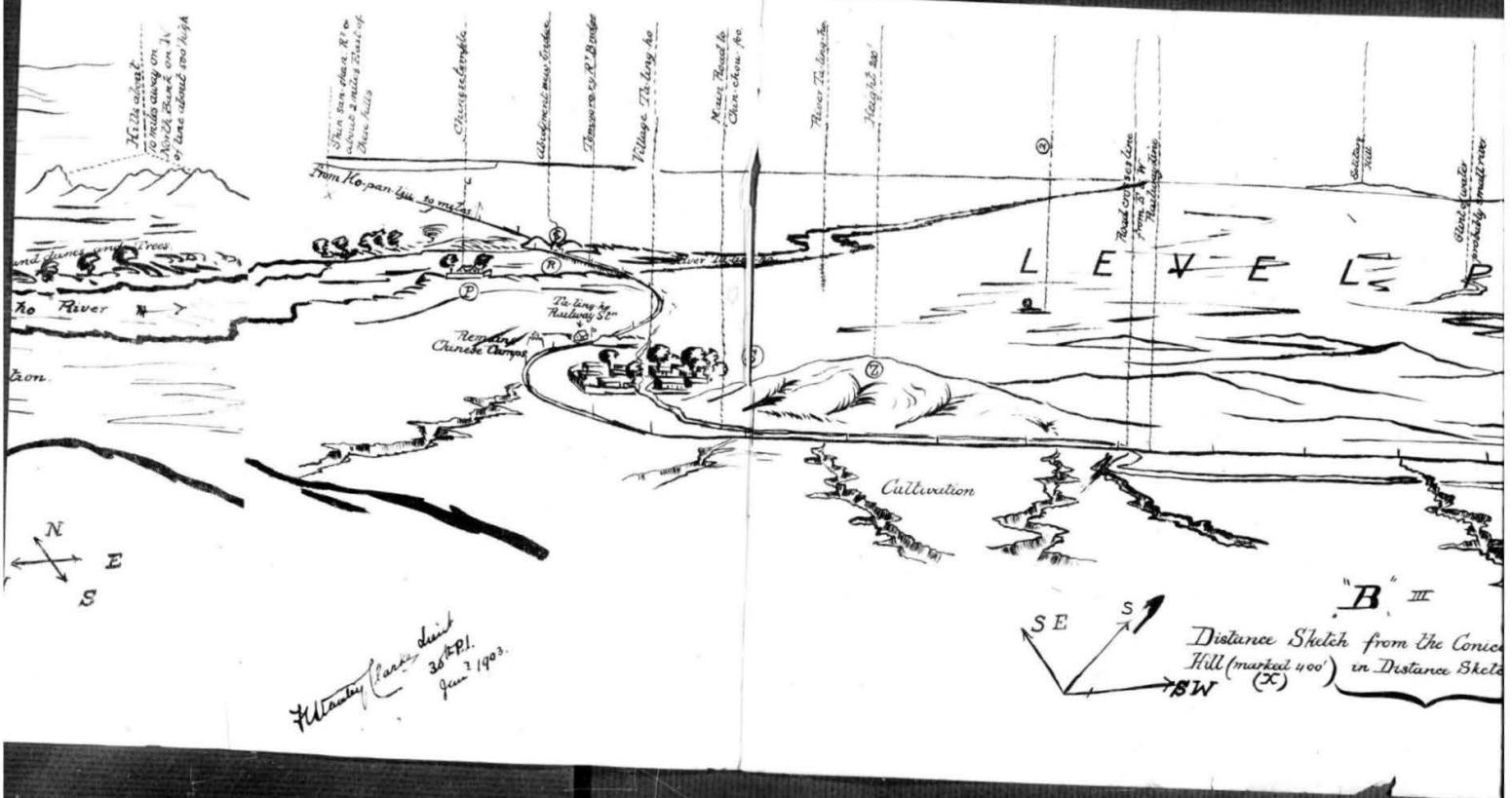


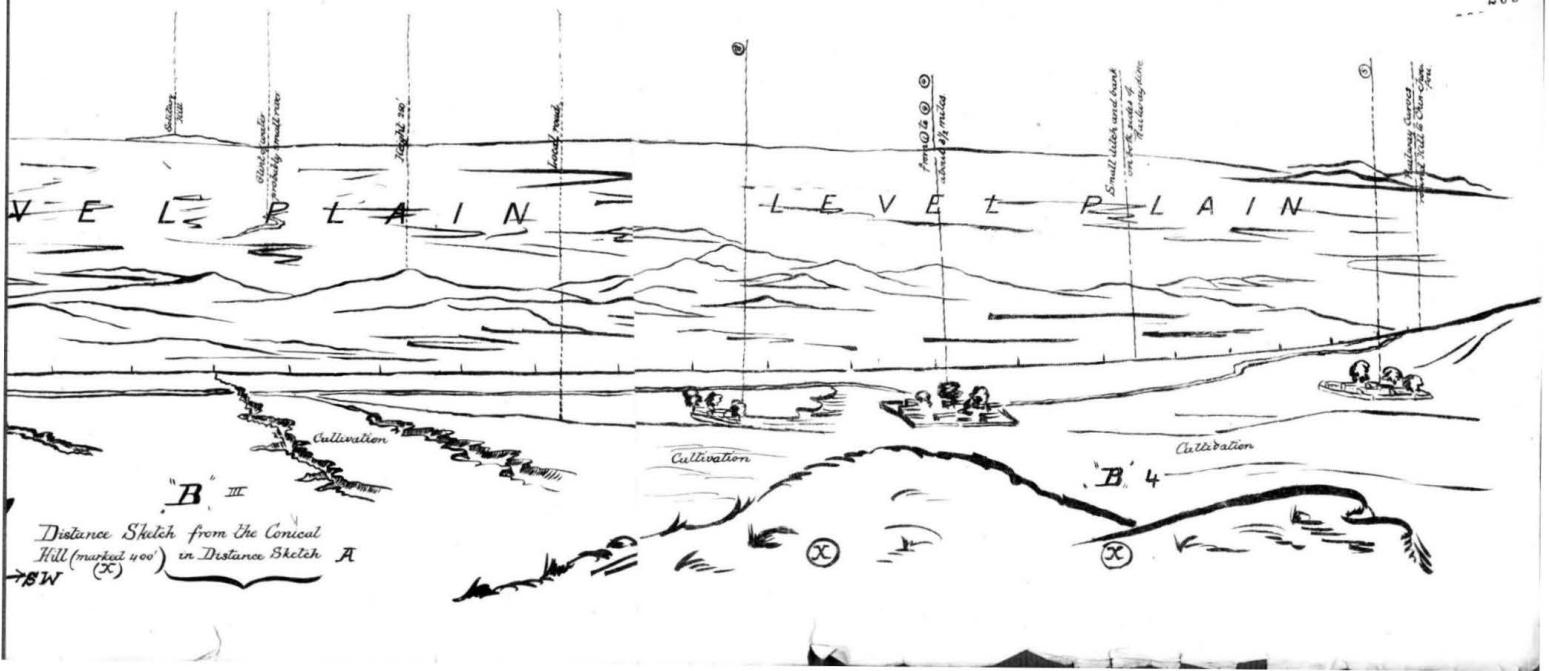








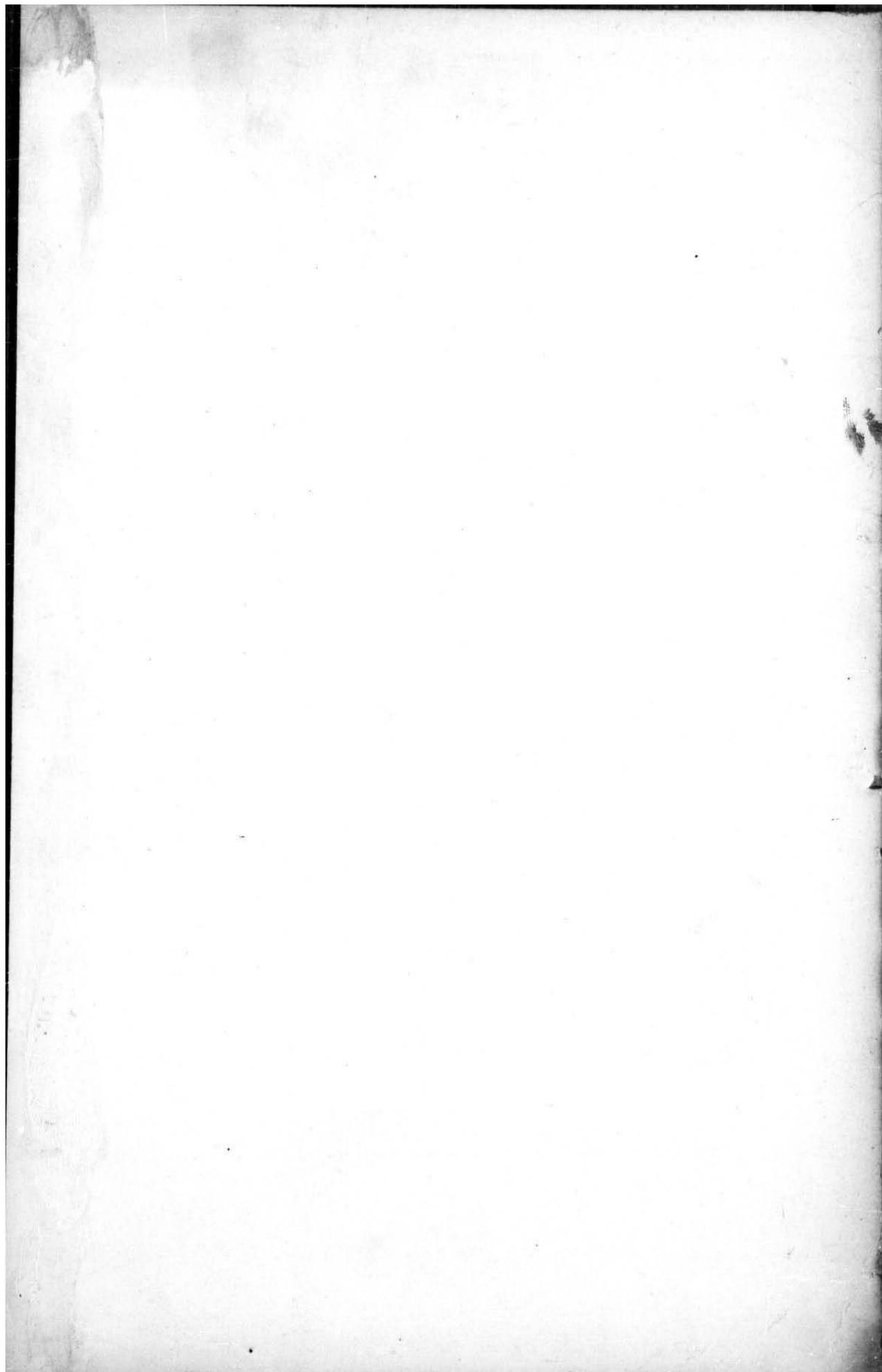




- (1) SHAN-HAI-KUAN in winter.
 - (i) View from Shore looking seawards.
 - (ii) View looking East showing site of No:1 Fort now demolished and lighthouse occupying its place.
 - (iii) The mouth of the HAI HO (formerly called PEI-HO) at TA-KU. Remains of South fort after demolition.
- (2) (i) Photograph of boundary stone of CHIH-LI and FENG-TIEN Provinces erected on the South side of the main road from LIN-YU HSIEH (i.e.SHAN-HAI-KUAN) to FENG-TIEN FU (i.e.MUKDEN)
- (ii) Photograph of boundary stone similar to (i) erected by the side of the railway where the boundary crosses the line.
- (iii) Blue print copy of sketch of stone referred to in (i) above, with translation of inscription on stone.
- (3) Types of Bridges.
 - (i) 40' span - Near KAO-CH'IAO HO.
 - (ii) 80' span - First bridge West of NING-YUAN.
 - (iii) Temporary pile bridge over TA-LING HO built by Russians.
 - (iv) Permanent bridge in course of construction over TA-LING HO.
- (4) View looking North West from high ground overlooking the village of T'ER-CHIA-T'UN. Typical of country in CHIN-CHOU Prefecture.
- (5) Typical of country south of railway between TA-LING HO and YING-K'OU.
- (6) (i) View looking South from SHENG-CHIA-T'AI on the SHENG-KOU HO.
- (ii) View looking North West from small pass 3 miles South of CH'EN-CHIA-T'UN, showing where the two rivers SHENG-KOU HO and HSIAC-LING HO unite.
- (7) (i) View looking North West from a point South East of PA-TAI-YING-TZU.
- (ii) View looking South East from a point North West of PA-TAI-YING-TZU.
- (8) (i) View looking North from point 2 miles East of CH'AO-YANG HSIEH showing hill 700 feet above the level of the valley - in the bend of the TA-LING HO on the right bank.
- (ii) View from point C on sketch plan of CHIN-CHOU Railway Station looking East. Typical of country between NING-YUAN and TA-LING HO.

SKETCHES.

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- (i) Distance Sketch of railway crossing over TA-LING HO from East or left bank.
 - (ii) Distance Sketch of railway crossing over TA-LING HO from West or right Bank.
 - (iii) Views from points A, B, and C, on Plan 17.



South gate of the city

PLAN OF RAILWAY PROPERTY
AT
SHAN-HAI-KUAN

Scale 1 inch to 400 Feet

Feet 100 200 300 400 500 600 700 800 900 1000 1100 1200 1300 1400 1500

A = Locomotives Water House
B = R.S.O.s' Quarters
From: B.R.A. plans

